



Fingal County Council
Comhairle Contae Fhine Gall

SWORDS

MASTERPLAN



JANUARY 2009

SWORDS

MASTERPLAN

ACKNOWLEDGEMENTS

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CONTENTS

	Page		Page		Page
Introduction and Vision	3	6.0 Pavilions Development Area	55	10.0 Swords Town Centre Heights Strategy	103
1.0 Statutory Context	5	6.1 Redevelopment and Expansion	55	10.1 Introduction	103
1.1 Non-Statutory Guidance Documents	7	6.2 Constraints and Opportunities	55	10.2 Overview	103
1.2 Integrated Transportation Strategy	7	6.3 Movement Strategy	56	10.3 Town Centre Key Areas	104
2.0 Masterplan Overview	11	6.4 Density and Mix	58	10.4 Landmark Buildings	104
2.1 Urban Design	11	6.5 Heights and Massing	60	10.5 Tall Building Design	104
2.2 Role of the Masterplan	11	6.6 Urban Form – Streets	65	10.6 Heights Strategy Diagram	105
2.3 Urban Design Criteria and Assessment Method	11	6.7 Open Space	73	10.7 Summary	105
2.4 Masterplan Development Areas	13	7.0 Metro Quarter	79	11.0 Sustainability Agenda	107
2.5 Public Realm Strategy	15	7.1 Characteristics of Metro Quarter	79	11.1 Sustainability & Transport	107
2.6 Strategic Objectives	15	7.2 R132 Characteristics	79	11.2 Sustainability & Building Design	107
3.0 Traditional Town Centre	17	7.3 Movement Strategy	80	12.0 Phasing and Implementation	109
3.1 Vision Statement	17	7.4 Public Realm	82	12.1 Introduction	109
3.2 Analysis of Historic Town Centre	18	7.5 Land Use	83	12.2 Historic Town Centre	110
3.3 Development of Activity Nodes	18	7.6 Heights and Massing	84	12.3 Town Centre Expansion Areas	110
3.4 Urban Design Guidelines	20	7.7 Landscape Strategy	84	12.4 Indicative Phasing for Town Centre Expansion Areas	110
3.5 Architectural Features	33	8.0 Barrysparks Town Centre Expansion	85	Appendices:	
3.6 Public Realm Strategy	38	8.1 Barrysparks Development Area: Constraints and Opportunities	85	Appendix 1: Urban Design Analysis	111
3.7 Riverside Walk	39	8.2 Movement Strategy	86	Appendix 2: Summary of Checklists	121
3.8 Opportunity Sites: Retail/Commercial Core	41	8.3 Density and Mix	89	Appendix 3: Glossary	123
4.0 Civic Quarter	49	8.4 Heights and Massing	90		
4.1 Character	49	8.5 Urban Form: Streets	92		
4.2 Connectivity	49	8.6 Open Space	95		
4.3 Design	49	9.0 Public Realm and Landscaping Strategy	99		
5.0 Cultural Quarter	51	9.1 Introduction	99		
5.1 Introduction	51	9.2 Characteristics of the Public Realm	99		
5.2 Movement Strategy	51	9.3 Street Furniture	101		
5.3 Public Realm	51	9.4 Signage	101		
5.4 Land Use	53	9.5 Lighting	101		
5.5 Heights and Massing	53	9.6 Public Art	102		
5.6 Landscape Strategy	54	9.7 Conclusion and Implementation	102		

Introduction

Introduction

Swords is identified as a Metropolitan Consolidation Town within the Regional Planning Guidelines for the Greater Dublin Area and has an important role as a developing town within the Dublin City Region. In 2006, the Government launched 'Transport 21', whereby commitment was given for the development of a high quality public transport system, 'Metro North' connecting Dublin City Centre to Swords via Dublin Airport. Swords will therefore be supported by a high quality public transport network by 2013, facilitating its economic growth and expansion in line with the Regional Planning Guidelines.

The Fingal Development Plan 2005-2011 identified the need for a Masterplan for Swords Town Centre to guide its future growth and development. The Masterplan will address issues relating to the historical core and role of the town, which is evolving and changing against the background of a growing population, economic expansion and opportunities arising from the arrival of Metro North. It is key to the future success of Swords as a multi-functional high quality Major Town Centre within the Dublin-City region, that the traditional Town Centre extend in a compact manner to the south and south west, within the Pavilions¹ and Barrysparks² development areas respectively, and integrate fully with Metro North.

The expansion of the Town Centre, in conjunction with the regeneration of the historical Main Street and North Street areas, presents a unique opportunity to bring a new dynamic and cohesiveness to the Town Centre and to sustain its vibrancy and attractiveness through the sustainable provision of an appropriate mix of uses, including modern retailing formats, and quality urban environment appropriate to the function of a successful Major Town Centre within the Dublin-City Region.

Masterplan Area

The focus of the Swords Masterplan area relates to the MC (Major Town Centre), zoned lands within the Main Street and North Street areas, Pavilions development area, and Barrysparks development area. The boundary for the Masterplan extends beyond these areas and guidance and proposals are considered within this context. The GI and ST1 zoned lands within the Masterplan boundary are considered strategic in nature, given their proximity to Metro North, and have been considered in more detail in the documents 'Your Swords, An Emerging City, Strategic Vision 2035'. These lands may be considered for rezoning as MEC/MEC1 and further guidance will be developed as part of the proposed Swords LAP.

The Swords Masterplan, which is in compliance with Fingal Development Plan Objective U01, is set within the timeframe 2008-2020, and shall be reviewed after 6 years, 2014, post arrival of Metro North.

¹ 'Pavilions Development Area' refers to the area incorporating the Pavilions Shopping Centre, Penneys/ Residential Development and link mall, and the existing Garda Station. This area extends to and is bound by the R132, Malahide Road, Dublin Street/Road and Carlton Court Residential Development.

² 'Barrysparks Development Area' refers to the MC zoned lands to the east of the R132. The County Development Plan refers to this area as 'Crowcastle/Barrysparks', but for ease of reference will be hereafter referred to as Barrysparks development area.

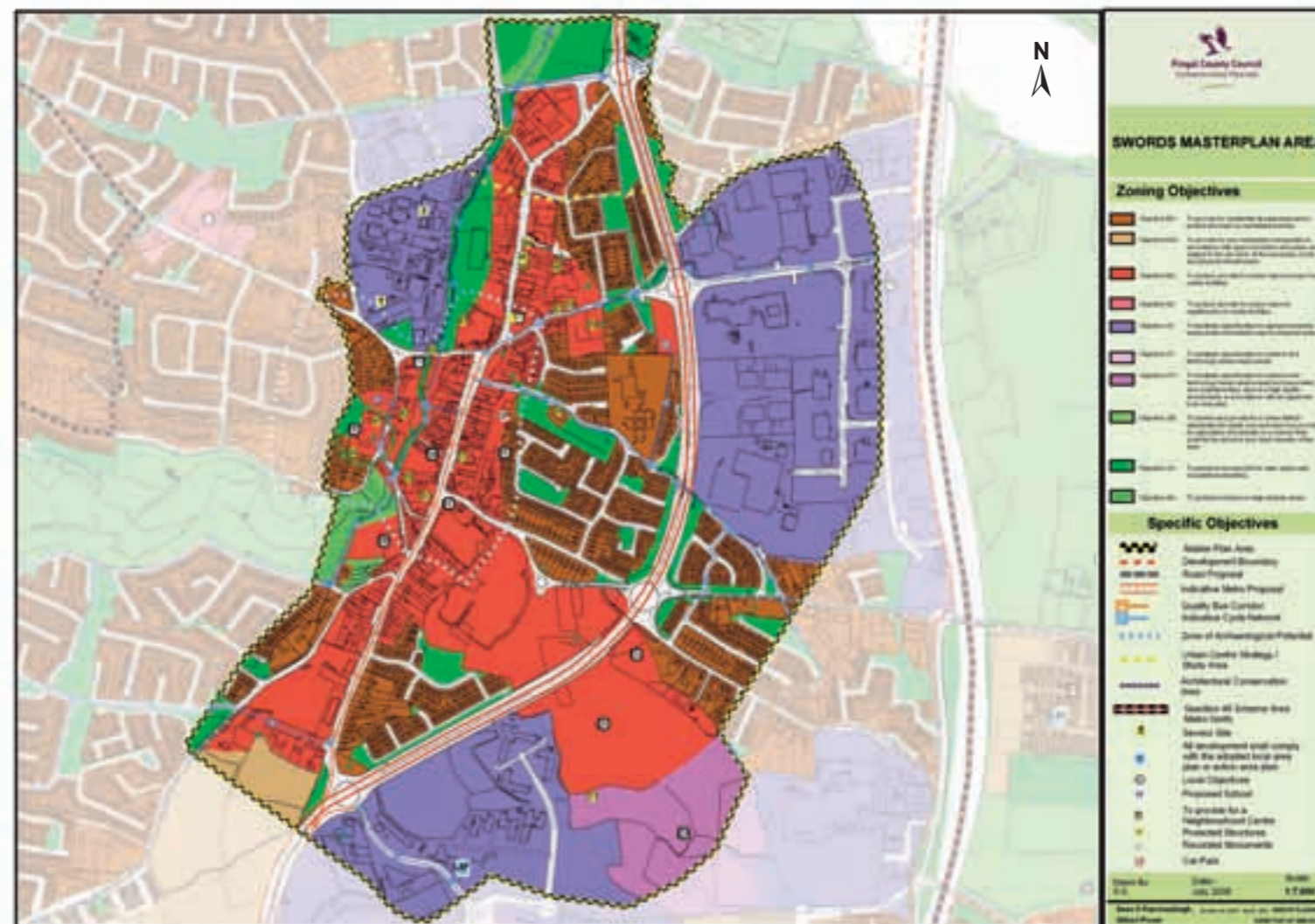
Vision

The consolidation and strengthening of Swords historic Town Centre is required to enable its continued development as a high quality, vibrant and multi-functional sustainable urban centre. This consolidation will be encouraged through the densification and growth of appropriate retail, commercial, residential, civic, cultural and leisure developments, taking into account the historical character of the town and its urban fabric.

The creation of public nodes within the new and expanding Town Centre will be encouraged resulting in the creation of high quality well-designed, well-connected, focal public spaces which will attract people between the different

character areas within the existing and expanding Town Centre and aid in the enlivening of different areas within the Town Centre. These nodes are the Civic Quarter, Cultural Quarter and Metro Quarter.

In association with the strengthening of the traditional Town Centre, Swords must also grow and extend its retail, cultural, residential and commercial offer within the Pavilions and Barrysparks development areas. These development areas must be successfully integrated with the traditional core area and public nodes through the creation of streets, a pedestrian priority environment, public urban spaces, and integration with Metro North and the public transport system, aiding in the delivery of a quality urban environment which will enhance the quality of life for residents, visitors and workers alike.



Swords Masterplan Area

1.0 Statutory Context

1.0.1 LOCAL CONTEXT: FINGAL COUNTY DEVELOPMENT PLAN 2005-2011

The Town Centre is primarily zoned MC – ‘Major Town Centre’.

The Vision for MC lands is to:

...consolidate....and...to further develop these centres by densification of appropriate commercial and residential developments ensuring a mix of commercial, recreational, civic, cultural, leisure, residential uses, and urban streets, while delivering a quality urban environment which will enhance the quality of life of residential, visitor and workers alike. The zone will strengthen retail provision in accordance with the County Retail Strategy, emphasise urban conservation, ensure priority for public transport, pedestrians and cyclists while minimising the impact of private car based traffic and enhance and develop the existing urban fabric. In order to deliver this vision and to provide a framework for sustainable development, masterplans will be prepared for each centre in accordance with the Urban Section policy objectives.’

Objective U01:

‘To produce masterplans for the major Town Centres of Swords, Blanchardstown and Balbriggan’.

The CDP indicates that the Masterplan should be drafted within the framework of an integrated land use and transportation plan. The Masterplan will include a vision, development proposals and an implementation strategy. Whilst the Masterplan will provide a vision for the Town Centre, design is only one component of what must be a multi-disciplinary approach that combines innovation and realism. It will include the following principles:

- Creation of an urban design framework for the Town Centre which will seek to enhance the Town’s character,
- Establishment of the nature and mix of the uses, the existing density of retail/other uses and intensification of uses and proposals to ensure effective Town Centres,
- Integration of land use and transportation with particular emphasis on effective traffic management, public transportation and reduction of surface level car parking; there should be a reduction in the dominance of roads and car parks,
- A better mix of Town Centre uses and the permeability of space to create a living heart to the Town Centre, including:
 - introduction of residential uses, sport and recreation,
 - creation of focal points in the public realm including squares and recreation areas,
 - encouragement of retail development which expands on what is already in place, for example, specialist traders and country markets,

- Exploitation of natural amenity and heritage assets,
- Where appropriate the designation of Architectural Conservation Areas and architectural enhancement areas,
- Application of the Council’s findings and objectives from the County Retail Strategy in order to progress the centre to achieve the characteristics of a Major Town Centre and to secure appropriate retail floorspace,
- Partnership arrangements and consultation dialogue with local community, other private sector partners and public agencies, and the voluntary sector stakeholders to align plans and proposals for the centres and facilitate agreement on key infrastructural requirements and provision,
- Establishment of a monitoring and implementation programme by the undertaking of biannual health checks.

It should include consideration of:

- Existing density of retail/other uses and intensification of uses,
- The nature of the uses,
- Lessons learned from comparable international master plans as appropriate.

The Fingal Development Plan Development Strategy promotes the planned and sustainable development of the County Town of Swords by:

- Consolidating existing and future development within well-defined town boundaries separated from the agricultural hinterland by designated greenbelt areas,
- Providing for a much-expanded civic, cultural, retail, commercial and employment base at a level appropriate for a vibrant major County town,
- Developing high quality public transport links to Dublin City, Dublin Airport and the Greater Dublin area,
- Promoting the development of high quality living and working environments.

The County Development plan identifies the following **Development Objectives**:

Objective SWORDS 1

- To encourage a range and quality of retail, commercial, civic, cultural, leisure and other services commensurate with the role of Swords Town Centre as a Major Centre.

Objective SWORDS 2

- To promote the development of Swords as a multi-modal transport hub.

Objective SWORDS 3

- To actively promote the early development of the Metro linking the town of Swords with Dublin Airport and Dublin City Centre.

Objective SWORDS 4

- To develop an Integrated Traffic Management Strategy for the town.

Objective SWORDS 5

- To maintain the operational capacity of Swords By-Pass.

Objective SWORDS 6

- To promote the early development of a local bus service within the town linking residential areas to each other and to the industrial/employment areas and the Town Centre.

Objective SWORDS 7

- To retain the Main Street as the core of the Town Centre, protect and enhance its character and ensure that any future new commercial and retail development reinforces the role of the main street as the core area of the Town Centre, by increasing the existing level of retail use, prohibiting the expansion of non-retail or inactive street frontages [including financial institutions, betting offices and public houses] and strictly limiting the development of fast food outlets.

Objective SWORDS 8

- To prepare a Masterplan for the Town Centre.

Objective SWORDS 9

- To prepare an Urban Design Framework Plan for the integration of the Metro and Swords/N1 By Pass in association with the Town Centre Masterplan.

Objective SWORDS 10

- To prepare a Local Area Plan for ‘MC’ zoned land to the east of the Swords By Pass for the integration of major Town Centre lands east of Swords By-Pass with the existing Town Centre, for a transportation hub, retail and employment uses and the Swords Masterplan.

Objective SWORDS 11

- To protect and conserve the historic core of Swords including the Zone of Archaeological Potential in the centre of the town.

Objective SWORDS 12

- To promote high quality commercial signage in the Town Centre area and initiate enforcement action where appropriate.

Objective SWORDS 13

- To promote and facilitate environmental improvements in the town core area.

Objective SWORDS 14

- To provide for a comprehensive network of pedestrian and cycle ways, linking housing and commercial areas to one another and to the Town Centre and linking the Ward River Valley, the Broadmeadow River Valley and the Estuary.

Objective SWORDS 15

- To prepare a development and design strategy to improve the appearance of the N1 from the City/County boundary to the southern development boundary for Swords Town.

Objective SWORDS 16

- To promote the intensification of use and upgrading of the existing neighbourhood centres at Rathingle, Brackenstown and Rathbeale, on a scale that respects and is compatible with the surrounding established residential communities.

1.0.2 VARIATION 29 TO THE FINGAL COUNTY DEVELOPMENT PLAN 2005-2011:

The following elements of Variation 29 have been adopted by Fingal County Council and are incorporated into the written statement. Associated specific local objectives have been added to maps SW1, SW2, SW3 and SW4. (See Swords Variation 29 report and associated map).

Objectives SWORDS SV1 and SV2 (in addition to Objectives SWORDS 1-16, Part X, 'Your Fingal' Written Statement)

Objective SWORDS SV1

- 'To develop Swords as an 'Emerging Green City'; to promote and facilitate the sustainable development of Swords as a vibrant consolidated major town with a thriving economy, an integrated public transport network, an attractive and highly accessible built environment, with the highest standards of housing, employment, services, recreational amenities and community facilities'.

Objective SWORDS SV2

- 'To target and facilitate the development of high tech, high intensity employment generating and service providing uses, in particular developments which include the provision of a hospital, 3rd/4th level educational facilities and major integrated sports facility.

Objectives SWORDS 17-24 (in addition to Objectives SWORDS 1-16, Part X, 'Your Fingal' Written Statement):

Objective SWORDS 17

- To develop a Regional Park (of circa 65 ha) immediately west of Oldtown and adjoining and ultimately extending into the Broadmeadow River Valley Linear Park. The Park shall serve Swords and its hinterland, and shall comprise active recreational and passive activities, commensurate with the Council's vision for the emerging city of Swords.

Objective SWORDS 18

- To develop a 'green necklace' of open spaces which are linked to each other and to the existing Town Centre of Swords, as well as to new development areas; thus promoting enhanced physical and visual connections to the Ward River Valley Park and the Broadmeadow River Valley Park from within the development boundary of Swords.

Objective SWORDS 19

- To facilitate and permit off-site provision of Class 1 open space on strategic sites within Swords, where it maximizes the opportunities of Metro North and advances the Council objective to develop a Regional Park (LO A Swords Objective 17).

Class 1 open space associated with the intensive development of specific strategic sites along the Metro North line shall be provided off site, by way of (a) direct acquisition of lands within the regional park itself and transfer of same to the Council, (b) payment of a financial contribution (to be identified through a review of the County s.48 Contribution Scheme), or (c) a combination of (a) and (b). Where Class 1 open space is provided off-site, there shall be a requirement on developers to provide a meaningful increase in Class 2 open space above the minimum level of Class 2 required in the County Development Plan. Class 2 open space will be provided, located, designed and constructed, to ensure that notwithstanding the introduction of higher density schemes, high standards of residential amenity shall be maintained within individual schemes (e.g. on-site small children's playgrounds, street furniture such as seating, etc).

Objective SWORDS 20

- To develop a series of new Neighbourhood Centres, on strategic development areas and undeveloped zoned lands within Oldtown, Fosterstown, Seatown Development Area, Estuary Development Area and Lissenhall Strategic Development Area, in accordance with an approved Local Area Plan for Swords.

Objective SWORDS 21

- To develop a series of District Centres, of appropriate scale and at suitable locations, to be determined and approved within the context of the proposed Swords Local Area Plan, albeit broadly identified as being located on sites within the general land banks of (i) Lissenhall Strategic Development Area, (ii) Oldtown-Mooretown LAP area (at Mooretown) and (iii) within the South East Quadrant of Swords.

Objective SWORDS 22

- 'To develop and augment the local road network to facilitate the optimum and sustainable development of Swords'.

Local Road Augmentations Proposed include:

- Oldtown-Mooretown Western Distributor Link Road;
- Oldtown-Lissenhall Link Road(s);
- Highfield Link Road;
- Seatown Development Area Link Road (Swords Business Park to Mountgorry Roundabout)
- Fosterstown Link Road;

In addition to local improvements already approved within the Development Plan:

- Forest Road Improvement Scheme (ref. County Development Plan Written Statement, Table 6.1, no. 16)
- Airside-Drynam Link Road (ref. County Development Plan Written Statement, Table 6.1, no. 17)

Objective SWORDS 23

- Within the context of the Planning & Development (Strategic Infrastructure) Act [2006], the Council shall seek to ensure that the Railway Procurement Agency (RPA) design, construct, operate and maintain Metro North, to the highest standards and ensure that all environmental impacts are minimised.

In particular, the Council shall seek to deliver the following:

- Along the R132, Metro North shall be provided in a landscaped corridor punctuated only by proposed Metro North stops which will introduce a series of urban nodes/gateways to Swords Town. Any existing landscaping which has to be removed along the R132 to facilitate the construction of the Metro shall be fully replaced by the RPA as part of the Metro North scheme.

- All elevated sections of the Metro shall be of high aesthetic design quality.
- Track construction shall comprise grass-bed track through Lissenhall, along median on R132 and through Balheary Park; embedded track at stops, and ballast elsewhere along route.
- All new footbridges (including those replacing existing bridges) along the R132 shall be of high aesthetic design quality and shall be sufficiently wide such that they provide for the comfort and safety of shared pedestrian/cycle use.
- All Metro stops, shall be well designed, and shall positively contribute to the architecture and urban design of the public realm in Swords and shall provide for pedestrian and cycle access; bus interchange facilities and adequate shelter.
- Construction interface locations shall not adversely impact on or impinge the Council's objectives as they may pertain to the delivery of significant elements of physical infrastructure.
- RPA proposals, including construction timeframe, shall have regard to the Council's proposals to delivery a pedestrian and public transport link/podium above the Swords Stop and a vehicular conduit below (linking MC zoned lands east and west of the R132);

Objective SWORDS 24

- Fingal County Council will target high density, high intensity, environmentally sound new development in the High-Tech, Science & Technology, Research & Development Sectors, encouraging their location to Swords, along the Metro North Economic Corridor (MEC).

Insert new Metro line: Show altered current Railway Procurement Agency (RPA) alignment on Fingal Development Plan 2005-2011 map, noting revised Metro route, six proposed Metro stops (namely Fosterstown Stop, Swords Stop, Seatown Stop, Estuary Stop, Lissenhall Stop and Belinstown), the proposed Metro North depot, and the proposed Park & Ride sites (at Belinstown for c.2,000 spaces and Fosterstown for 300 spaces).

Delete existing Local Objective LO 172 from CDP

LO 172 – to prepare a strategy for the Metro terminus which will provide for the optimum mix of uses.

Vary CDP Swords Map, SW1-SW14 to:

- 'Rezone from 'GB' to 'OS' of approximately 65ha of agricultural land west of Oldtown and adjoining and ultimately extending into the Broadmeadow River Valley Linear Park'.
- To show potential development boundaries associated with the identified 'strategic development areas' of 'Estuary Development Area', and Seatown Development Area (LO D) and the proposed study area for Lissenhall (LO H).
- Rezone GB to RS1, to provide for the proposed rezoning of approximately 17 ha of land from GB to RS1 at Oldtown/Mooretown, thereby extending the development boundary of Swords westwards to provide adequate additional lands to support an improved road and public transport network in west Swords and to support the provision of a mix of uses at this location necessary for the creation of a strong, vibrant and well serviced community, including the proposed Regional Park (Objective SWORDS 17).

Local Objectives to CDP Swords Maps, SW1, SW2, SW3 and SW4:

LO H To create a sustainable mixed-use urban environment to the north of Swords at Lissenhall, which will be fully integrated with the Metro North line traversing the development lands and its associated Stops and Park and Ride facility (Lissenhall Strategic Development Area).

Vary CDP Written Statement to:

- include proposed new Metro related Land-use Zoning Objectives, "MEC" and "MEC1" (as per proposed Tables No.4.15 and 4.16)

1.0.3 COMPLIANCE WITH U01

In respect of the Swords Masterplan a comprehensive and multi-disciplinary assessment of the existing and expanding Swords Town Centre (MC zoned lands) was undertaken; with internal and external expertise sought in respect of traffic, retail, heritage/conservation, landscape architecture, architecture, urban design and planning.

In addition to following and incorporating the Masterplan criteria as outlined in the County Development Plan significant consultation was undertaken with the public, stakeholders, and local councillors and TD's. Prior to publication of the draft Masterplan public meetings/workshops were held as part of the comprehensive planning for Swords. Meetings were also held with key stakeholders and elected representatives to gage their views on the specifics of planning and designing Swords and its integration with Metro North.

Following initial pre-draft consultation, the draft Masterplan was placed on public display from Friday 19 September, 2008 until Friday 17 October 2008 during which time submissions and observations were sought. A Manager's report was prepared in respect of the submissions received and amendments to the draft proposed. The Manager's Report was presented to the Balbriggan/Swords Area Committee on 8.01.09, where the report was noted and the Swords Masterplan, as proposed, unanimously supported. The Area Committee recommended the Manager's Report and Master Plan be brought to the Full Council on 12.01.09. The Manager's Report and Masterplan was accepted by the full Council. The Council is fully satisfied that the Swords Masterplan has been prepared in accordance with U01.

1.1 Non-Statutory Guidance Documents

1.1.1 'YOUR SWORDS, AN EMERGING CITY, STRATEGIC VISION 2035'

Fingal County Council is currently preparing a Local Area Plan for Swords, with the primary focus being the incorporation and optimisation of the opportunities arising from the arrival of Metro North in 2014. Swords needs a new Town Plan (LAP) to ensure it can properly and sustainably plan for, and deliver future economic prosperity and improve the quality of life for its residents following the arrival of the Metro. The document 'Your Swords, An Emerging City, Strategic Vision 2035' provides the background and an assessment of how Fingal County Council plan to support the growth and development of Swords into the future in terms of physical, social and economic growth. This document, which is a non-statutory consultation document, was taken into account in the compilation of the Swords Masterplan.

1.1.2 SWORDS RETAIL STRATEGY ASSESSMENT

As a background to the Swords Masterplan, a review of the County Retail Strategy as it relates to Swords was undertaken. This assessment examined changes in the comparison and convenience retail sector since 2004.

The assessment has shown that Swords currently underperforms as a designated Major Town Centre. Although the Pavilions Shopping Centre and the Penneys scheme have significantly enhanced the retail offer and attraction of the Town, there is very limited representation of the key international comparison retailers and no opportunity within the historical urban grain of Main Street/North Street area to accommodate a modern retailing and commercial format. In this regard, it is necessary to extend the traditional Town Centre core to meet the needs of the growing population of Swords and to develop Swords' role and function as a designated Level 2 Major Town Centre through encouragement of mix of uses. This urban extension will focus on existing MC zoned lands at the Pavilions³ and Barrysparks⁴ site. Facilitation of the growth and interaction of the multi-functional uses of the town, in conjunction with the creation of an accessible quality urban environment, are key to the success of Swords.

The results of the retail assessment have been utilised in understanding and assessing the need for Swords to grow its existing retail offer to meet the needs of its population and to sustain Swords Town Centre in an efficient, equitable and sustainable manner. This assessment will continue to be used as a background aid to the planning authority to assist in development management/pre-planning in relation to retail/commercial applications in the Town Centre.

1.1.3 SWORDS TRANSPORTATION STRATEGY

A traffic base model has been developed by Fingal County Council to give up to date information on existing traffic levels on roads and at junctions within Swords. This model was utilised in the assessment of the adopted Variation 29 to the Fingal Development Plan 2005-2011 for Swords, to determine enhancements required to the local road network, and public transport network improvements necessary to support Metro North. This model will continue to be used as an aid in the assessment of development management/pre-planning proposals, particularly where large scale residential, retail and office developments are proposed. The Masterplan has considered the issue of the wider transport network in developing an urban design framework for the Town Centre and impact on the accessibility and attractiveness of Swords Town Centre.

1.2 Integrated Transportation Strategy

Objective U01, 'To produce masterplans for the major Town Centres of Swords, Blanchardstown and Balbriggan', requires Masterplans to address (among other elements) the integration of land use and transportation with particular emphasis on effective traffic management, public transportation and reduction of surface level car parking.

³ 'Pavilions Development Area' refers to the area incorporating the Pavilions Shopping Centre, Penneys/Residential Development and link mall, and the existing Garda Station. This area extends to and is bound by the R132, Malahide Road, Dublin Street/Road and Carlton Court Residential Development.

⁴ 'Barrysparks Development Area' refers to the MC zoned lands to the east of the R132. The County Development Plan refers to this area as 'Crowcastle/Barrysparks', but for ease of reference will be hereafter referred to as Barrysparks development area.

As a background to this requirement, Fingal County Council non-statutory consultation document in relation to Swords, 'Your Swords, An Emerging City Strategic Vision Document 2035', identifies a number of deficiencies within the existing transportation network. These deficiencies will be addressed via the proposed Integrated Transportation Strategy, which will be prepared as part of the proposed Swords LAP, and will also be supported via a number of variations to the Fingal County Development Plan, in particular relating to Metro North.

The Masterplan has been compiled taking into account the proposed arrival of Metro North by 2014, required improvements to the bus network, provision of multi-modal interchange facilities, traffic management (including parking standards) and improved pedestrian/cyclist routes and connectivity. Hereunder, the proposed transportation developments, which will impact on the Town Centre, are highlighted and where appropriate are incorporated within the urban design framework for the Town Centre.

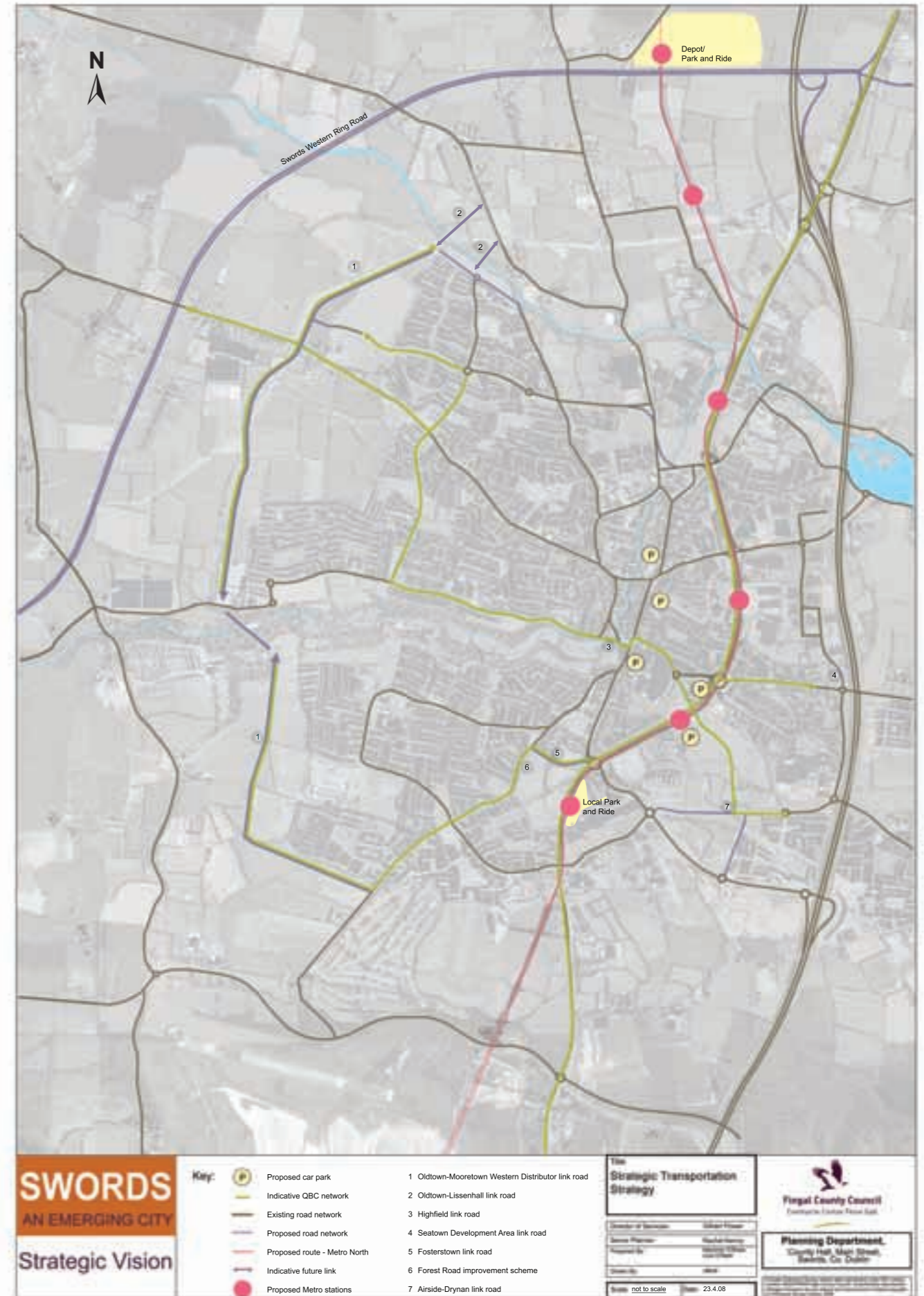
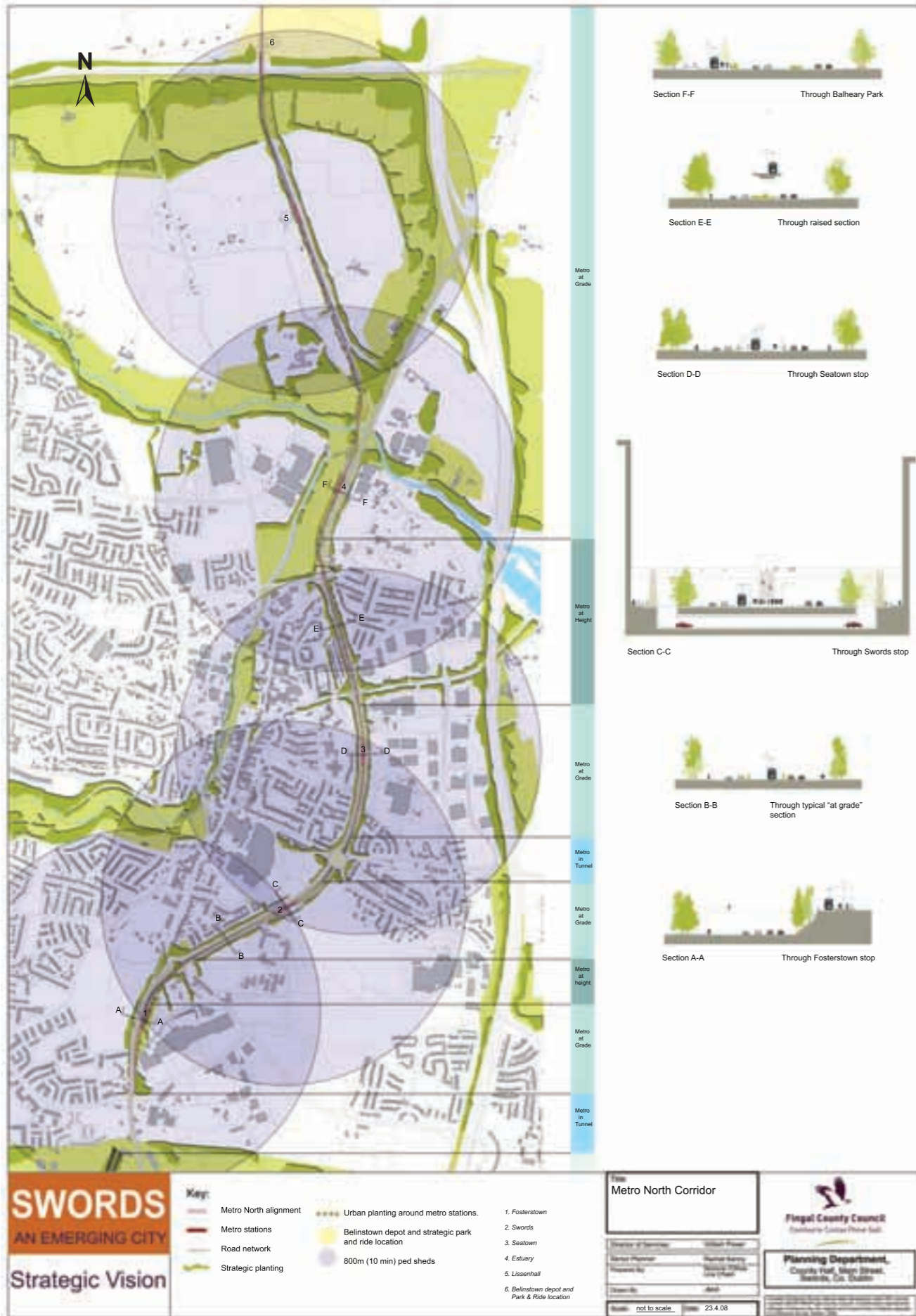


METRO NORTH

The metro alignment and station locations as proposed by the RPA are shown on drawing 'Metro North corridor'.

The metro is located along the R132 for the most part. Six Metro Stops are proposed by the RPA at: Fosterstown; Swords; Seatown; Estuary; Lissenhall and Belinstown. Four of these stops [Fosterstown; Swords; Seatown and Estuary] are located within or adjacent to the Masterplan lands.

The Swords Stop, which is the primary stop serving Swords Town Centre, is located centrally on the R132 in proximity to the Pavilions Shopping Centre and Barrysparks site, within what is defined as the Metro Quarter. This stop will have a number of important functions/roles as the most strategic Metro Stop and public transport interchange:



- An accessible public transport interchange point for buses, taxis and cyclists over the R132;
- A high quality, well designed public realm at ground level and on the plaza/podium. (A Metro plaza above the Metro line/R132, linking pedestrians/cyclists/buses to developments east and west of R132).
- Metro Box*: An integrated vehicular network, which accommodates vehicular movements east and west, under the R132;
- High-quality and architecturally significant Town Centre developments east and west of the stop, which will be fully integrated with the stop.

This area will become established as the Metro Quarter and will form a vital orientation node for those arriving in Swords. The Masterplan, in recognition of the importance of the Metro Quarter and in order to fulfil Objective SWORDS 9, sets out under section 7 an urban design framework in relation to the development and integration of the Metro Quarter and Swords Metro Stop.

BUS NETWORK AND INTERCHANGE FACILITIES

It is an objective of Fingal County Council to ensure the development of a highly accessible and well connected Town. In order to maximise the benefits of and the efficient use of the Metro for the benefit of those living and working in Swords, it is critical that an improved and integrated town wide bus network is developed, linking to the Town Centre and each of the Metro Stops. This will allow local feeder bus services to be developed that will extend the effective catchment area of the Metro. Delivering quality bus services will require the provision of sections of Quality Bus Corridor [including bus lanes to bypass potential locations of delay and congestion]; developing bus only links and providing for bus priority measures.

The delivery of a QBC from west Swords into Main Street and the Swords Metro Stop is an important element of proposed improvements to the bus network. To achieve this QBC a Highfield Link Road for buses only, in addition to junction improvements, is necessary to link efficiently those living and working in west Swords to the Metro Stop, the Town Centre, and to east Swords.

BUS INTERCHANGE FACILITIES

The Council will require that sufficient bus interchange facilities will be provided at each Metro Stop. Bus interchange facilities shall be provided at grade level adjacent to the Fosterstown, Swords, Seatown and Estuary Stops. In addition to the bus lay-by proposed at grade adjacent to the Swords Stop, bus interchange facilities shall also be provided at the Metro Plaza level.

The Masterplan sets down the urban design framework for a multi-modal transport interchange at the Swords Metro Stop/Metro Quarter, which includes provision for a new local public transport route over the R132, regional bus lay-by facilities at grade, and connection points between the at grade Metro/Bus stops and the +1 local bus stop. A sloping street called The Rise will link Malahide Road through the Pavilions development area over the R132 and return to ground level within the Barrysparks development area. The focus of this street is for public transport, as well as pedestrian and cyclist movements. This bus route is critical in ensuring the compact and integrated growth of Swords Town Centre. In addition it is expected that most people who live beyond convenient walking distance of the Metro corridor will use the local bus service to access the Metro at the Swords Stop. A secondary transportation node will be provided at the Fosterstown Stop where bus services from the southern parts of Swords will join the R132 corridor.

The Metro Plaza/R132 Podium, the bus only route over the R132 and the Metro Box* will be required to provide appropriately enhanced east-west connectivity and to mitigate any severance effect created by the R132 and/or Metro North. This will become critical as Barrysparks lands are developed on the eastern side of the R132. The delivery of this infrastructure will occur on a phased basis in tandem with development. The delivery of this bus only route together with the Highfield Link Road will facilitate the development of a QBC through the Town Centre to the Swords Metro Stop and will link those living and working on lands east and west of the R132.

PRIORITISING PUBLIC TRANSPORT

The delivery of key elements of physical infrastructure together with required traffic management measures to give priority to public transport will be required to promote the development and the use of direct, frequent and punctual town bus services.

TRAFFIC MANAGEMENT

A traffic management strategy for Swords Town Centre will be prepared in tandem with the development of the public realm strategy for the Town Centre. While this strategy will be prepared as a separate study to this Masterplan, its implementation is integral to the success of the masterplan area. A key part of the traffic management strategy will be to identify and decide upon the priority measures needed to facilitate the development of frequent, direct and punctual town bus services throughout Swords and to promote walking and cycling within Swords. Priority will be given to public transport and the 'Green Modes' [pedestrians and cyclists] within the Town Centre and particularly along Main Street to ensure that buses, pedestrians and cyclists can move easily within the Town Centre (east and west of the R132) and access metro stops.

PARK AND RIDE FACILITIES

Two Park and Ride facilities are proposed along the Metro route. A strategic facility [serving a minimum of 2000 cars] is proposed on the north side of Swords at the Belinstown Stop and a local facility [300 spaces] is proposed on the south side of the Town adjoining Fosterstown Stop. The strategic Park and Ride facility is intended to serve those who live outside Swords and who have no option but to drive to access Metro North. The local facility will provide only a limited amount of car parking. No car parking facilities are proposed to be provided at any of the other Metro Stops. It will be Council policy to promote the use of town bus services to access the Metro as well as cycling and walking and to generally discourage people living in Swords from driving to the Metro. The Council will require that all Metro stops will provide for adequate bus interchange facilities, pedestrian and cycle access, cycle parking and shelter facilities. The Council proposes the delivery of a pedestrian and public transport link/podium above the Swords Stop.

PLANNING FOR ADEQUATE SHORT TERM CAR PARKING IN THE TOWN CENTRE

On-site car parking will be required to support future proposed mixed use developments on the Pavilions and Barrysparks lands.

In addition the development of adequate levels of short term car parking facilities in the vicinity of Main Street will also be required to support its continued role as the 'heart' of Swords Town Centre. Currently there is extensive on-street parking within the Town Centre as well as a number of surface car parks and one multi storey facility. A number of locations [including Bridge Street, North Street and Forsters Way] will be examined by Fingal County

Council in order to decide on the most suitable location/s for the development and delivery of a multi storey facility/ies adequate to serve the growing short/medium term parking demands within the Town Centre.

Future on street car parking arrangements and potential location of a multi-storey will be considered as part of the Traffic Management and Public Realm Strategy which will be developed as a separate study to this Masterplan.

PARKING STANDARDS

Parking has for some time been accepted as a key tool to manage traffic demand. The implementation of an effective parking control policy has the potential to positively support future development within Swords. Fingal County Council is considering what are the appropriate parking standards for developments within the Town Centre with the metro in place. The objective of Fingal County Council in this regard will be to ensure that parking levels provided for in association with future development are sufficient to support such development and that unnecessary car use is discouraged.

STRATEGIC PEDESTRIAN AND CYCLE ROUTES

It will be an objective of Fingal County Council to develop a strategic network of new and enhanced pedestrian and cycle routes within the Town to encourage greater use of these modes. There is considerable scope for the provision of "green-way" routes through parkland setting/corridors that would provide attractive and direct routes between adjoining residential areas and to the Town Centre and encourage people to "Bike & Ride" to the Metro.

The Council will require that adequate levels of cycle parking and bike shelters be provided at all Metro Stops. The number of cycle parking spaces to be provided should be determined though an evaluation of the likely demand for these facilities at each Stop based on market research through comparison with similar facilities in other countries where high cycle usage has been promoted. Cycle parking areas should be located immediately adjacent to the Stop and should be supervised by CCTV surveillance systems. Well designed and sited wind and rain shelter should also be provided to cycle stands.

In addition sufficient cycle facilities shall be provided in convenient and accessible locations within the Town Centre, Pavilions development area and Barrysparks development area. Permeability and connectivity will be of primary importance in the establishment of new street networks/movement strategies in relation to these expanding Town Centre areas.

ROAD NETWORK

The future indicative road network is shown on the drawing 'Strategic Transportation Strategy' map.

Enhancements to the road network at both local and regional level will cater for the remaining transportation demand by car and reduce potential traffic demands within Swords Town Centre.

A new Swords Western Ring Road as inductively outlined in 'Your Swords, An Emerging City, Strategic Vision 2035' will remove significant volumes of traffic from the Swords Town Centre area, as well as serving strategic traffic between the M1 and M2/M50 corridors.

* Metro Box is an underground vehicular connection across the R132, associated ramps, and necessary realignment of the R132 locally, in order to provide for access to lands zoned 'Swords Town Centre' east and west of the R132, as shown in the Fingal Development Plan 2005 to 2011. This Metro Box plan was adopted under Part 8.

To provide suitable access to major development areas on the northern and western edge of Swords, a Western Distributor Link Road is proposed. This will provide local links between existing and future residential communities, and eliminate the need to travel through the Town Centre to access such areas. This road will traverse through development lands and will provide local links between existing and future residential communities. It will connect to the proposed Inner Ward River crossing.

In addition, a number of key short road links will be provided at various locations to complete the local road network and to provide alternative routes for traffic that is currently excessively concentrated through the Town Centre area. These routes include the Highfield link road, the Seatown Development Area Link Road, the Fosterstown Link Road and the Airside Drynam Link Road.

Metro Box*, a vehicular conduit below the R132, will be provided linking the MC zoned lands east and west of the R132.

Prior to the development of the lands at Barrysparks, the Airside Drynam Link Road, and Metro Box* should be constructed.

Finally, the upgrading of some existing roads in Swords is required to provide for continuous footpaths, cycle lanes and reconstruction of road pavements. Various junction capacity improvements are also required to allow efficient traffic circulation and in particular to allow for the efficient movement of buses through the town.

A Traffic Impact Assessment and Mobility Management Plan shall be required at planning application stages in relation to the expanded Town Centre areas and redevelopment of strategic infill sites, to ensure that the requirements of the transport network are integrated with development and do not have a negative impact on accessibility, permeability and connectivity within the Town Centre and connections to the surrounding areas.

PUBLIC REALM STRATEGY

In recognition of the importance of the public realm to the vibrancy and quality of the Town Centre environment, Fingal County Council will prepare a public realm strategy for the Town Centre.

This public realm strategy will be prepared in tandem with the development of the traffic management strategy for Swords Town Centre. The key public realm criteria to be considered in this future strategy are set out in section 9.0 of this Masterplan.

* Metro Box is an underground vehicular connection across the R132, associated ramps, and necessary realignment of the R132 locally, in order to provide for access to lands zoned 'Swords Town Centre' east and west of the R132, as shown in the Fingal Development Plan 2005 to 2011. This Metro Box plan was adopted under Part 8.

2.0 Masterplan Overview

Good things don't happen by accident; they require forethought, commitment and enthusiasm.

2.1 Urban Design

Urban design is the art of making places for people. It concerns the way places work, the interaction between the different elements that make up the town-scene and urban fabric, as much as the way they look. It concerns the inter-relationships between people and places, activities and built fabric, movement and urban form, connecting and identity building, so ensuring successful towns and cities. Most people want to visit, work or live in a place where they have access to a diverse range of quality facilities, a place that is well connected and feels welcoming; a place with an excellent public realm and good homes, local shopping and entertainment venues on the doorstep; a place with good employment opportunities and easy access to education and recreation facilities.

The need for good urban design to achieve a high quality and sustainable urban environment is well established at a National and EU level in such policies as the Bristol Accord, Leipzig Charter, and the UK Urban Task Force report "Towards an Urban Renaissance".

The Swords Town Centre Masterplan promotes the adoption of the principles of good urban design as a central tool of the planning and development process.

2.2 Role of the Masterplan

The planning of the Town Centre and its relationship to the surrounding environs require an organisational logic that addresses the needs of the area as a whole. This leads to a coherent and functional model for planning changes, understanding interdependencies and prioritising investment in critical aspects of the area.

The intention of the Masterplan is to both inspire confidence in and guide proposed development to 2020 (with a review in 2014, post arrival of Metro North) according to a readily understood plan with the motivating factor of realising the inherent benefits to all of a high quality Town Centre urban environment.

The defined objectives and urban design framework of the Masterplan provide essential guidance for generating detailed development proposals. The plan will be used as the planning context in assessing applications/pre-planning discussions for development in the Town Centre area. As such the guidance will be subject to interpretation and the parameters set out by the plan will have varying degrees of flexibility to achieve the required outcome. The merits of detailed design proposals will require a rigorous assessment method to distinguish if acceptable urban design standards have been met within the conditions established by the Masterplan.

2.2.1 MASTERPLAN AS A GUIDANCE DOCUMENT

The Masterplan includes guidance in respect of building heights and street widths (where new streets are proposed) within the MC zoned lands generally, as well as further elaboration on architectural style, materials and urban form within the traditional Town Centre and in particular for the key opportunity sites identified.

The Masterplan architectural and urban design guidance is given to stimulate development, introduce some degree of certainty and to encourage quality design.

The indicative heights described in the Masterplan are the recommended maximum permissible heights, based on established urban design principles and in particular having regard to the following:

- respecting the amenity of existing development;
- positive contribution to the public realm;
- heritage value;
- Landmark role and
- sustainable compact development.

However, **all such urban design and architectural guidance is indicative only, and each development proposal presented to the planning authority will be considered on its merits through the development management process.**

Non-compliance with the Masterplan Guidelines will not automatically preclude planning permission being granted, however, applications which propose deviations from the Masterplan Guidance must be set within a detailed urban design rationale and in the context of the urban design appraisal and guidelines set out within the Masterplan before they can be considered favourably.

2.3 Urban Design Criteria and Assessment Method

The assessment of the adequacy of detailed development proposals requires a procedure that uses common criteria that are sensitive to the overall model and the demands of the site urban design brief.

Consideration of the following key urban design criteria is essential to achieve urban design excellence:

Development Form Aspects:

- Structure – Layout, grain and street pattern
- Open space – landscape and public realm
- Density and Mix – Uses and appropriate densities
- Scale – Massing, density and height
- Appearance – Detail and Materials

Urban Design Qualities:

- Character – A place of its own identity
- Continuity and enclosure – A place where public and private spaces are clearly distinguished
- Quality of the Public Realm – A place with attractive and successful outdoor areas
- Permeability – Ease of movement – A place that is easy to get to and move through
- Legibility – A place that has a clear image and is easy to understand
- Adaptability – A place that can change easily
- Diversity – A place with variety and choice
- Sustainability – A place that is sensitive to the environment

Based on the above qualities, urban design guidance has been prepared for each of the character and development areas: (existing Town Centre (Commercial/Residential, Civic Community and the Retail/Commercial Core), Cultural Quarter, Pavilions Development Area, Metro Quarter and Barrysparks Development Area. These guidelines are contained within the relevant sections dealing with each discrete area. The above key urban design criteria and specific urban design guidance will be used in the assessment of the adequacy of detailed development proposals.



SWORDS
AN EMERGING CITY
Town Centre
Masterplan

- Key:**
- Activity nodes
 - Significant existing buildings
 - Zone 1 Development Area - North Street and Main Street
 - Zone 2 Development Area - Pavilions
 - Zone 3 Development Area - Barrysparks
 - Green Space
 - Future Development Zones
 - Public Space

Title	
Development and Character Areas	
Drawn by:	Colleen Frawley
Checked by:	Seán Kelly
Approved by:	Una O'Brien Mayor of Swords
Drawn by:	MBB
Scale: 1:10,000	Date: 16.07.18

Fingal County Council
Swords & Castle Fine Gael

Planning Department,
County Hall, Main Street,
Swords, Co. Dublin

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2.4 Masterplan Development Areas

The focus of the Swords Masterplan area relates to the MC zoned lands within the Main Street and North Street areas, Pavilions development area, and Barrysparks development area. The development plan boundary for the Masterplan extends beyond these areas to ensure that guidance and proposals are considered within the context of adjoining residential and existing general industrial areas.

The following strategic key areas and focal public spaces (described in order of north to south within the Town), are addressed within the Masterplan [see map Development and Character Areas]:

- **Civic Quarter:** This area at the junction of Main Street/North Street shall be designated as a Civic Quarter, where it is intended to build on civic uses in this area, thereby strengthening the attraction of the northern end of Main Street. As part of the enhancement of this area, a public realm strategy will be developed and implemented in front of the Swords Castle at the junction of Main Street/North Street.
- **Historic Town Core/the Main Street and North Street Areas:** This area has great development growth and potential with a variety of Main Street uses and attractions. The backlands to the west of Main Street (fronting the Ward River Walk) and key strategic infill sites are identified in the Masterplan and present an opportunity for future redevelopment, as do a number of infill sites along North Street. In conjunction with these developments, it is important that there is an improvement in cultural and recreational functions of Swords Town Centre and in conjunction significant improvements of the public realm.

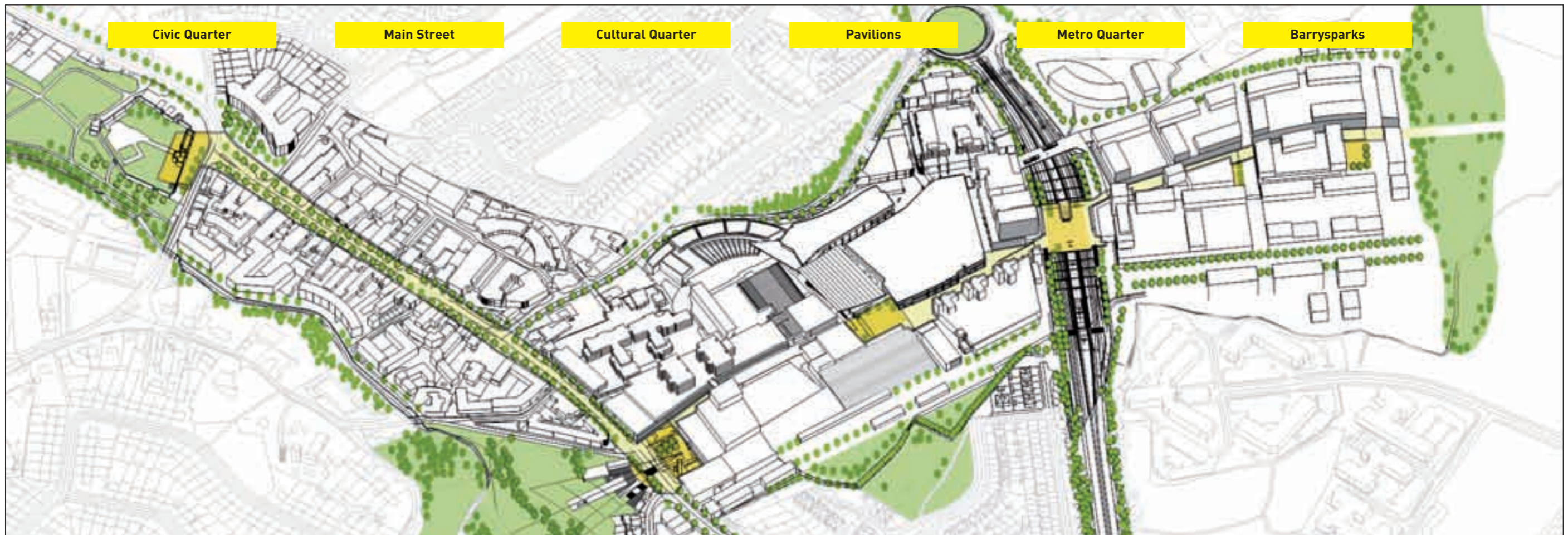
- **Cultural Quarter:** The Cultural Quarter will include culturally focused uses, such as a town library, theatre, galleries and arts centre. This area will accommodate a direct pedestrian connection through the Pavilions site to the Metro Quarter and links into the Ward River Valley Park. A public square will be the central focus and setting to cultural buildings and the scale of the public space will be capable of supporting outdoor performance, outdoor dining, performance art, promenade as well as meeting/seating spaces.
- **Pavilions Town Centre Expansion:** Facilitation of the planned and integrated expansion of the retail/mixed use core of Swords will be accommodated within the Pavilions development area. This section of the Masterplan identifies the mixed use potential of this area, proposed movement strategy, public realm strategy, identification of a street hierarchy and heights strategy. A key element of the development of this area will be the integration of this urban expansion area with the existing Town Centre through the Cultural Quarter and Malahide Road area and linkages with the Town Centre Metro Stop and bus interchange facility, as well as MC zoned lands east of the R132 at Barrysparks.
- **Metro Quarter:** Swords Town Centre will be served by a Metro North Stop located centrally on the R132 in proximity to MC zoned lands at Pavilions and Barrysparks development areas. The Swords Metro North stop will be distinguished from the other Metro North stops by the following elements: A high quality well designed public realm plaza above the Metro line; An accessible public transport interchange point over the R132; Metro Box*; High-quality and architecturally significant Town Centre developments east and west of the stop, which will be fully integrated with the stop. This area will become established as the Metro Quarter and will form a vital orientation node for those arriving in Swords.

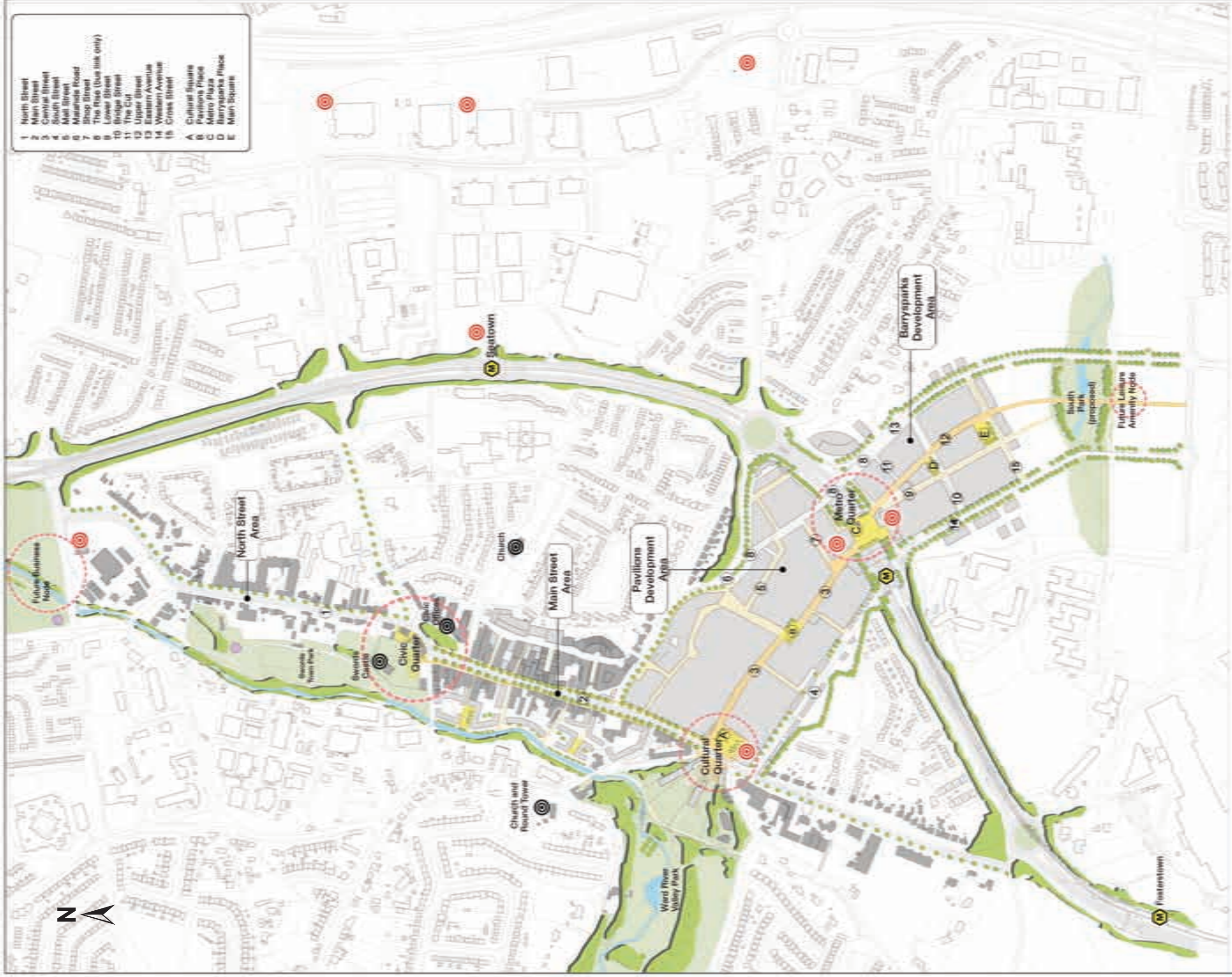
- **Barrysparks Town Centre Expansion:** Swords, which is a major Town Centre, must continue to expand its mixed-use role in conjunction with the Pavilions development area. This section of the Masterplan sets out a movement strategy, public realm strategy, street hierarchy and heights strategy. The scale, form and massing of development, in conjunction with its permeability and connectivity to the Town Centre is important for the future development of this area. Connectivity to the multi-modal interchange at Metro Quarter and to the Town Centre west of the R132 is key to this development area.

The Masterplan examines the above areas in detail within this document. In the long term, it is envisaged that additional development areas south of MC zoned lands within Barrysparks, and the Dublin Road area (see 'Future Development Zone' on map 'Development and Character Areas') will be developed/rejuvenated as part of the Town Centre. These areas will be examined through a separate LAP process and in subsequent reviews of this Masterplan.

The Masterplan aims to enable the achievement of a vibrant Town Centre with a focus on the continued development and interaction of multi-functional uses, permeability, connectivity, vitality and viability of Swords Town Centre within a legible and sustainable context.

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- 1 North Street
- 2 Main Street
- 3 Central Street
- 4 South Street
- 5 Mill Street
- 6 Marahale Road
- 7 Shop Street
- 8 The Rise (bus link only)
- 9 Lower Street
- 10 Bridge Street
- 11 The Cut
- 12 Upper Street
- 13 Eastern Avenue
- 14 Western Avenue
- 15 Cross Street
- A Cultural Square
- B Pavilions Place
- C Metro Plaza
- D Barrysparks Place
- E Main Squares

SWORDS
AN EMERGING CITY
Town Centre
Masterplan

Key:

- Existing buildings (main streets)
- Existing buildings (surroundings)
- Existing significant buildings
- Proposed landmark buildings
- Proposed buildings
- Activity nodes
- Public Spaces
- Pedestrian links
- Secondary pedestrian links
- Metro Stops
- Existing/new semi mature trees
- Existing/enhanced tree belt
- Park Land
- Existing play areas

Title: Masterplan

Drawn by:	Colleen Power
Checked by:	Seán Kirby
Approved by:	Uilleann Ó Súilleabháin
Drawn by:	akb

Scale: 1:10,000 **Date:** 16.07.18

Fingal County Council
Swords Civic Centre, Fine Gael

Planning Department,
County Hall, Main Street,
Swords, Co. Dublin

2.5 Public Realm Strategy

In recognition of the importance of the public realm to the vibrancy and quality of the Town Centre environment, Fingal County Council will prepare a public realm strategy for the Town Centre within 12-24 months of the preparation of this Masterplan. The key public realm criteria to be considered within such a strategy are outlined under section 9.0 of this Masterplan.

The public realm strategy for the Town Centre will be prepared in tandem with the development of the traffic management strategy for Swords Town Centre.

2.6 Strategic Objectives

The Swords Masterplan Area will address the following key objectives necessary to the future development of Swords:

- Facilitate the growth and development of Swords Town Centre as a multi-functional highly accessible destination, where people can shop, work, live and recreate within a high quality environment.
- Promote and enhance the identity of the Town Centre through the identification of 3 key activity nodes/focal public spaces which will build on the existing character areas within the Town Centre and link existing retail/commercial areas to Metro North. These key nodes/focal urban spaces will be as follows: Civic Quarter, Cultural Quarter, and Metro Quarter.
- Promote core recreational and amenity spaces, utilising in particular the Ward River Valley Park, Cultural Quarter, Civic Quarter at Swords Castle and Town Park, Ward River Walk west of Main Street, and Metro Quarter.
- Require modern, energy efficient and quality designed and finished buildings within the traditional Town Centre and Town Centre expansion areas. New buildings will have regard to the Swords Heights Strategy.
- Develop Urban Design Guidelines for Main Street and North Street areas which will seek to enhance the Town's character.
- Facilitate the creation of a new street fronting the river walk to the west of Main Street and support co-ordinated infill development on key strategic sites along Main Street and North Street in accordance with Urban Design Guidelines and Site Development Briefs.
- Encourage the growth and expansion of the range and mix of Town Centre uses through the expansion of the Town Centre to the south and southwest, within the Pavilions development area and Barrysparks development area, in accordance with an Urban Design Framework addressing these areas.
- Ensure the integration of the Town Centre expansion areas of the Pavilions and Barrysparks, with the Main Street area and activity nodes, through the creation of a permeable and legible street network, active 24 hour streets, and provision of a mix of attractive Town Centre uses, within a quality urban environment.
- Facilitate the development of a multi-modal transport interchange within the Metro Quarter, linking Metro North with local and regional bus networks and surrounding vehicular network.
- Provide for the integration of Swords Metro Stop with developments east and west of the R132 (Pavilions and Barrysparks) and with the Town Centre through the creation of a Metro Quarter. Metro Quarter will comprise a

Metro Plaza and bridge over the R132 connecting Pavilions and Barrysparks for pedestrian/cyclist and public transport. It will also comprise a vehicular road network under the R132/Metro North providing access to underground car parks connecting development east and west of the R132. This vehicular network under the R132/Metro North will be referred to throughout this document as the 'Metro Box'*. Access to this network is via ramps directly from the R132.

- Provide for the free movement of pedestrians, cyclists, vehicular traffic and public transport within the Metro Quarter, in particular crossing east and west over the R132 at the Metro Plaza level and under the R132 via the proposed vehicular road network.
- Encourage the development of Landmark Buildings at the Swords Metro Stop location, which will be the gateway entrance to Swords. Such Landmark buildings will be high quality, modern and energy efficient structures with the highest quality of finishes.

2.6.1 FUTURE DELIVERABLES

- Develop a Traffic Management and Parking Strategy to facilitate improved movement and access within the existing and expanded Town Centre, and assess the potential benefits of a pedestrian and public transport prioritisation scheme on Main Street.
- Develop a coherent Public Realm Strategy in conjunction with the Traffic Management Strategy for Swords Town Centre within 12-24 months of the preparation of this Masterplan.
- Develop a Landscape and Recreation Strategy for the Ward River Valley Park within 12-24 months of the preparation of this Masterplan.
- Design and deliver a multi-modal transport interchange within the Metro Quarter, linking Metro North with local and regional bus networks and surrounding vehicular network.

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3.0 Traditional Town Centre

The Town Centre environment has fundamentally to work well for its intended purpose. It has a local, regional, national and international dimension to that purpose; a performance that is measured against an increasingly competitive market populated by highly mobile users, from local residents to multinational employers; a performance that is determined by the ability to meet the needs of the community as a whole now and in the future.

The quality of the Town Centre environment is central to its core purpose of supporting both commercial and social communities. The built environment directly influences the performance of all activities in business and society as a whole. It is important to create a Town Centre environment that understands and matches those needs.

3.1 Vision Statement

The vision for Swords Town Centre encompasses the following 5 concepts:

1 A Thriving Town Centre

- Encourage quality design.
- Strengthen the northern end of Main Street as a Civic Quarter, with a dedicated public space and associated uses to the front of Swords Castle which will act as a people draw.
- Develop a diverse Cultural Quarter, which will support a more diverse evening economy, and which will integrate with the Ward River Valley Park and the Pavilions development area.
- Develop a Metro Quarter which facilitates the integration of high quality public transport into the core of Swords.
- Create a new coherent architectural language for Swords that respects the town's character.
- Create exciting new urban quarters on lands west of Main Street overlooking the Ward River, with shops, apartments and activity.
- Support quality well designed infill development within the Town Centre.

2 Quality of Life

- Promote physical improvements to the public realm of Main Street (robust lighting, attractive paving, encouragement of active frontages, removal of street clutter).
- Enhance the pedestrian links between Main Street and the Pavilions Shopping Centre.
- Develop a green corridor through the town linking the main green open

spaces associated with the Ward River that is the Ward River Valley Park, Town Park, Balheary Park and the Estuary.

- Strengthen pedestrian links within the Town Centre to Swords Castle with the introduction of a pedestrian wayfinding signage system as well as physical improvements.
- Improve the pedestrian environment in the Town Centre through prioritising pedestrian and public transport along Main Street.

3 A Cultural Destination

- Develop a high quality Cultural Quarter, providing for a theatre, library, cultural/arts centre, associated restaurants/cafés and a large public square. This will be an attractive and vibrant environment throughout the day and into the evening.
- Implement a comprehensive and distinct signage system throughout the town to include information boards signposting to key sites and gateway welcome signs.
- Build upon Swords' rich architectural heritage.
- Raise specifically the profile of Swords Castle, St Columba's Church and the associated Round Tower.

4 An Attractive Public Realm

- Prepare a detailed public realm strategy for the Town Centre in tandem with the development of a Traffic Management Strategy.
- Enhance the Civic Quarter at the northern end of Main Street as a central public gathering space.
- Improve the visibility of Swords Castle.
- Facilitate the provision of multi-storey car park/s close to the northern end of Main Street.
- Improve vehicular accessibility to the Town Centre.
- Provide for street lighting to the pathways leading east and west from the Main Street.
- Improve local bus movement through Swords Main Street, connecting with the public transport interchange at Metro Quarter.

5 A Sustainable Town Centre

- Encourage the development of high quality public and private spaces and a variety of uses including a balanced mix of commercial, leisure, civic and residential accommodation. Landscape, public realm and a diverse range of uses are the cornerstones of the vision.
- The expanded Town Centre will attract people to the Greater Dublin area as

well as Swords itself and aims to also retain them as regular visitors, to create a stable working population and build a residential community with civic pride. Its selling points will be its quality landscape and architectural design, its range of facilities and work places, and large apartments ideal for a variety of people from single people to large families.



Source: the Academy for Sustainable Futures – www.ascskills.org

3.2 Analysis of Historic Town Centre

From an examination of existing land uses, 3 main character areas can be identified within Swords Town Centre [see map – Existing Character Areas and Movement Strategy]:

1. **Retail/Commercial Core:** This area comprises Main Street and extends to the Pavilions Shopping Centre site. The majority of the small scale retail/services functions within the Town Centre are focused on Main Street and its associated side streets, while the large scale retailers are facilitated within the Pavilions Shopping Centre. Main Street is predominantly vernacular in character with development being mostly 2 and 3 storeys. There have been some modern interventions onto the Main Street which are unsympathetic to the prevailing low rise character.
2. **Commercial/Residential Core:** North Street is characterised by a more commercial focus, with offices, small scale retail outlets, small mix of terraced housing, public houses, apartments and a hotel. The current trend is for medium size offices to locate here. Dublin Street and Dublin Road also accommodates a range of small scale commercial units, as well as residential uses and a secondary school.
3. **Civic/Community:** The civic core is located at the junction of Main Street, North Street, and Seatown Road. The character area lies east between North Street and Main Street. School, community and civic buildings, in addition to housing, are located along this route. The County Hall, Swords Castle and Courthouse are focal buildings at the junction of Seatown Road/Main Street/North Street. Seatown Road reinforces the east-west linkages through the town and has the potential to become a strong and attractive route into the Town Centre from the proposed Seatown Metro Stop.

Key to the success of Swords will be the consolidation and strengthening of the above character areas and multi-functional role of Main Street/North Street area, as well as future integration/connectivity with Town Centre expansion areas. Improvements to the associated public realm will be important, as well as improved accessibility for all modes of transport, including strong linkages to Metro North.

In terms of Swords retail/commercial core, the historic Town Centre is unsuitable for large format retailers due to the tight urban grain and plot sizes. This constraint within the historic fabric has resulted in the extension of the retail area to the Pavilions site. The extension of the shopping core from Main Street to the Pavilions has allowed the retail core of Swords to develop and extend in close proximity to Main Street, whilst retaining the historic character and role of the Town Centre. However, connectivity between Main Street and the Pavilions site is currently poor, with a limited number of visitors to the Pavilions attracted to Main Street. The Fingal County Council Offices and other offices/services on Main Street currently sustain flow between the 2 areas. However a reverse attraction to Main Street from the Pavilions Shopping Centre needs to be established. This is critical in the event of the further expansion of the retail core into the Pavilions and Barrysparks lands, adjoining the Metro North stop.

The existing Cultural/Recreational attractions are the Castle and its Park, the riverside walk to the west of Main Street, the Ward River Valley Park, and the round tower. While these facilities are within the core, they are poorly developed and poorly integrated with the Town Centre. Views of Swords Castle from Main Street are obstructed by the buildings in front of it. There are few buildings overlooking the riverside walk, which is of poor environmental quality, attracting few people. The Ward River Valley Park has few facilities and is very poorly linked to the Town Centre. For Swords Town Centre to develop, emphasis must be placed on the diverse range of activities and attractions within the core area and its amenity value.

In terms of accessibility for all modes of transport, car parking facilities are limited within the Town and public transport services are inadequate. Facilities for cyclists are poor, with only a limited network in isolated sections of the Town. These issues must be addressed as part of the Masterplan for Swords.



25 inch Historic Map: 1897 - 1913

3.3 Development of Activity Nodes

3.3.1 3 ACTIVITY NODES

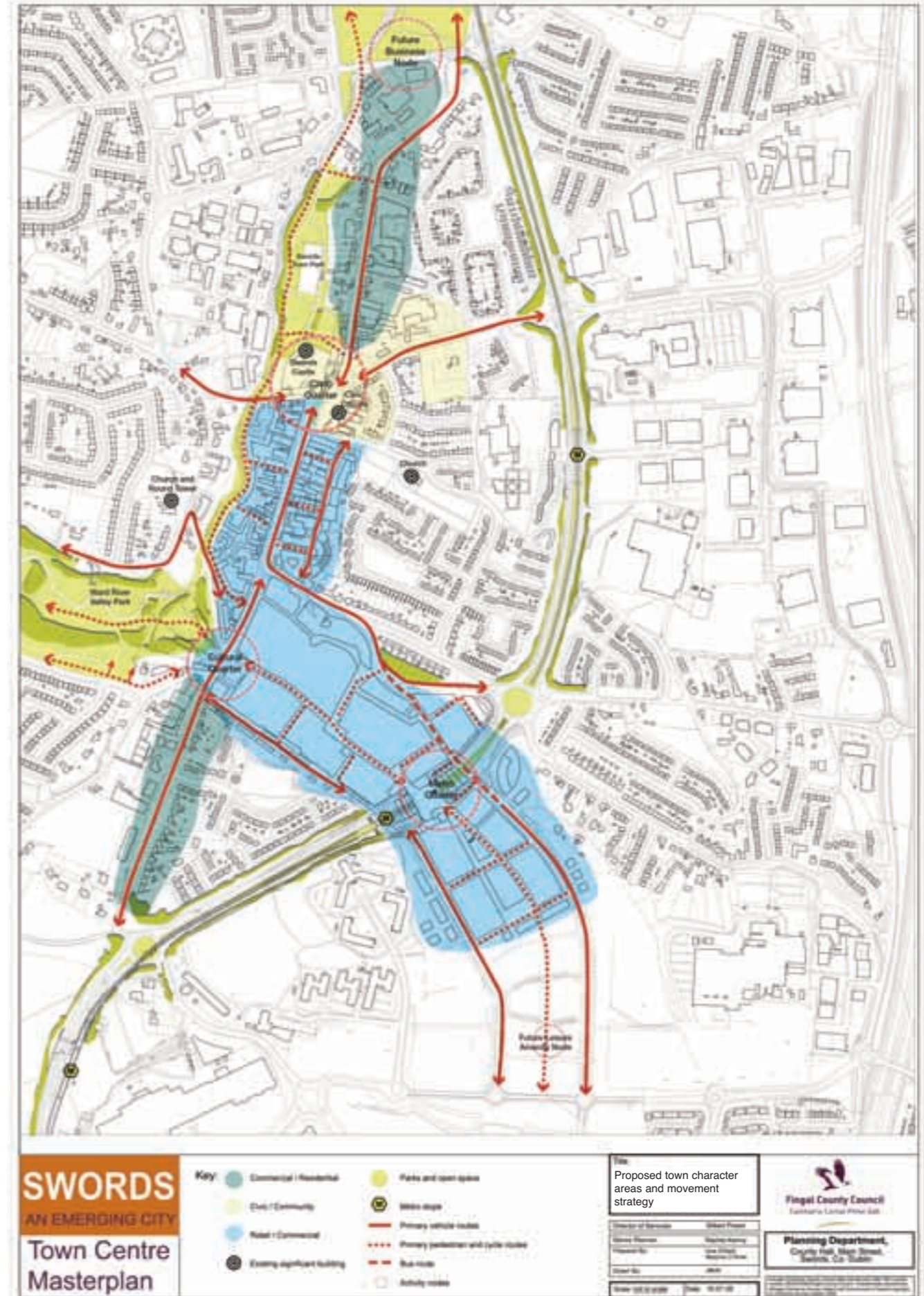
To ensure the vibrancy and activity of Swords Main Street is developed into the future, it is proposed that there will be 3 key nodes of attraction in the Town Centre which will build upon the existing nodes and character areas through the strengthening and development of links between the Main Street, the Pavilions and the Barrysparks development areas.

The proposed Town Centre Nodes are:

- Civic Quarter
- Cultural Quarter
- Metro Quarter

The design and character of these activity nodes is explored later within this Masterplan.

There are 2 ancillary nodes identified, these being Balheary Park and the proposed Leisure Node within the south-eastern extent of the Barrysparks land. While these 2 nodes are considered important in the future expansion of the town, they will not be fully defined or designed pending the preparation of a Local Area Plan(s) for these areas. [see Existing Character Areas and Movement Strategy map and Proposed Character Areas and Movement Strategy map].



3.4 Urban Design Guidelines

To achieve the Vision for Swords traditional Town Centre, urban design guidelines have been prepared for each of the Character Areas based on the urban design appraisal in Appendix 1. The character areas addressed are as follows:

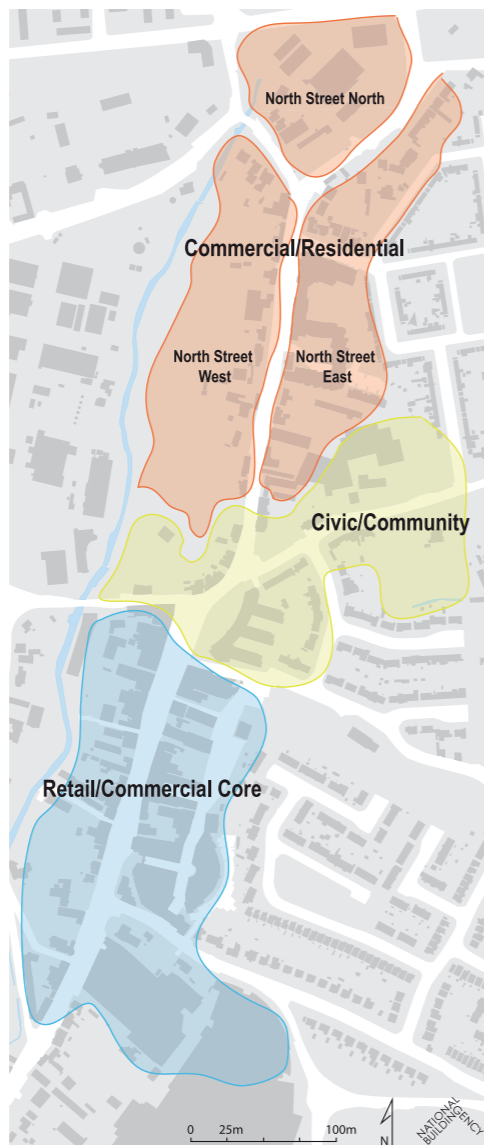
- 3.4.1 Commercial/Residential,
- 3.4.2 Civic Community
- 3.4.3 Retail/Commercial Core.

The Commercial/Residential character area has been divided into sub character areas for the purposes of the guidelines given the varied existing characteristics of the area:

- 3.4.3(a) North Street East
- 3.4.3(b) North Street West
- 3.4.3(c) North Street North

The urban design analysis is addressed under the following headings:

- Land Uses
- Topography
- Views
- Public Open Space
- Legibility
- Connections + Routes
- Parking
- Safety and Security
- Protected Structures
- Development Block Size
- Urban Grain
- Quality of Building Frontage
- Building Orientation
- Building Height and Massing
- Architectural Features
- Materials



3.4.1 THE RETAIL /COMMERCIAL CORE

Urban Design Principle

Issue

Urban Design Guidelines

Land Uses	The Town Centre area has a large convenience retail offer due to the location of the Pavilions Shopping Centre and a broad range of comparison shopping. The Main Street area is unlikely to attract a large format retailer due to the centre’s tight plot size, but could accommodate specialist niche retailers.	Promote the development of specialist comparison retail supported by an evening cafe/restaurant culture to the backlands areas both east and west of Main Street. Promote the development of active ground floor uses along Main Street. Promote a pavement culture with spill out areas from restaurants and cafes along Main Street which will also aim to diversify the current evening economy offer. To maintain Main Street’s position as a mixed use street in Swords it is considered appropriate to limit the expansion of certain non retail and inactive street frontages that would otherwise detract from the overall character of the street. These uses are as follows: <ul style="list-style-type: none"> • Amusement Arcades • Bookmakers/Betting shops • Discount Shops • Take Aways/Fast Food Outlets • Public Houses • Financial Institutions These uses shall not be permitted to locate around the civic or cultural nodes or at existing or new junctions (both pedestrian and vehicular) at Main Street, Dublin Street and the new riverside streets. In addition they shall not be allowed to predominate along any section of Main Street or the new riverside street.
Topography	The Town Centre’s flat topography heightens the significance of landmark buildings.	Protect strong views within Swords, particularly towards Swords Castle, St Columba’s Church and St Colmcille’s Church.
Views	Swords’ largely flat topography increases the importance of street-long views and vistas within the town. Views of Swords Castle, St Columba’s and St Colmcille’s Church are especially important in Swords, projecting the town’s positive image.	Retain the view of Swords Castle from Main Street and Dublin Street. Protect the view to the historic core of Swords at St Columba’s Church and round tower from Main Street, Church Road and Well Road. Protect the view to the spire of St Columcille’s Roman Catholic Church from Main Street and New Street.
Public Open Space	Planting and greenery is a distinctive feature of the retail/commercial core. The wide pavements of Main Street accommodate large mature treestands (Plane variety). To supplement this there is a small parcel of green open space and mature tree stands of Holmes Oak (Evergreen Oak) north of Main Street associated with the entrance to Fingal County Hall. There exists along Main Street excessive visual clutter in terms of road barriers, bollards etc. The River Ward currently is an underutilised asset which the Town Centre effectively turns its back on.	The Town Centre area is a sensitive area for new landscaping interventions given the number of protected structures and buildings of interest within the area. Enhance the entrance to Swords Castle at Main Street to a hard landscaped pedestrian friendly public open space. Legibility and identity at a large scale (blocks) or at a small scale (groups of buildings) can be defined by the use of tree planting and street furniture. This can be achieved by specifying particular material/planting palettes distinctive to that area. It is the intention of Fingal County Council to develop a positive public realm around Swords Castle as part of a separate project. There is an opportunity to rationalise the visual clutter along the Main Street to provide a more pedestrian friendly environment. There is potential to front buildings onto the river Ward to provide surveillance, an active edge, and an attractive visual aspect.
Legibility	The Town Centre area contains some very legible features: Swords Castle, the historic street shape of Bridge Street, North Street and Main Street and the historic buildings which line them. Enhanced gateways and public realm improvements to the Town Centre would counter negative images of the town among visitors and residents. There are a number of significant barriers within and around Swords, such as the river to the west of the Town Centre, Dublin Road to the east of the Town Centre and the lack of a pedestrian friendly link between the Pavilions and the Town Centre.	Increase the legibility of the area through the use of landmark features such as focal buildings, intimate spaces and street trees. Seek ways of overcoming the barrier to the west, the River Ward, by the addition of a pedestrian bridge. Connect the existing river walk to Bridge Street and northwards towards Swords Town Park.

3.4.1 THE RETAIL/COMMERCIAL CORE

Urban Design Principle	Issue	Urban Design Guidelines
Connections + Routes	The Town Centre area is dominated by heavy traffic flow and could be more pedestrian-friendly.	Promote the Town Centre area as a pedestrian prioritised environment, utilising the overall width of Main Street to create wider pavements. Create a well-signed, direct pedestrian route between the Pavilions and the Town Centre to encourage increased pedestrian flow. The proposed Metro North link to Swords and the permeability of Swords Town Centre make the town well suited to pedestrian oriented public realm improvements. Continue the pedestrian route along the riverwalk northwards and connect onto Bridge Street and to Swords Town Park and beyond.
Parking	Car parking occupies prominent areas in the Town Centre area, some of which might provide excellent infill development opportunities.	Create a parking strategy for Swords to investigate ways of freeing up parking space. Investigate ways of developing multi level car parking fronted by commercial/retail uses at key opportunity sites north of the Town Centre. Provide for adequate levels of short term carparking especially along Main Street. Please refer to Section 3.6 on Traffic Management Strategy.
Safety and Security	The side streets and backland areas should be well lit to minimise safety problems.	The backland areas east and west of Main Street present themselves as particularly vulnerable in terms of safety and security. Urban Design interventions should be seen as one of many ways of addressing this. Buildings should front onto streets and laneways should be well lit and in certain cases covered by CCTV. The principles of 'Safer Places: the planning system and crime prevention' by the ODPM should be incorporated as part of any new development (Please refer to Appendix 2).
Protected Structures	Swords' medieval street layout and some buildings of high architectural quality contribute to the town's character by creating enclosed linear spaces at the heart of the town along the Main Street, North Street and Dublin Street. The high level of vacant and derelict buildings detracts from the character of the area.	The Town Centre is a sensitive area for new architecture, and an area where public realm quality is more noticeable. New buildings and public realm improvements should compliment the existing protected structures, historic streetscape and high quality of architecture. Work to Protected Structures should match the original details and materials, and specialist advice sought prior to any major repair or refurbishment. Normally planning permission will be required for any such works.
Development Block Size	The block layout in the retail/commercial core is typically organic in form and layout.	New block structures should retain the prevailing block dimensions. However some flexibility and variation in size and shape should be promoted to reinforce the distinctive organic nature of the town. To the east of Main Street promote a block size of approximately 40m x 50m. To the west of Main Street promote a block size of approximately 80m x 140m. Opportunities exist to complete the block formation in the backlands to Main Street.
Urban Grain	The street structure of Swords Town Centre clearly reveals the town's historical roles as a market town with Swords Castle at the meeting point of North Street, Bridge Street and Main Street. The urban grain of the town shows a tight grain at Main Street gradually becoming more looser as one moves northwards to North Street and Seatown Road.	Use existing urban grain dimensions as a basis to guide new development into a settlement with distinct character areas. For the retail/commercial core maintain the prevailing building width of 8m-10m to the western edge of Main Street and a width of 15-20m to the eastern edge. New development can address this grain through facade design with variations in the facade composition to echo the historical grain pattern.
Quality of Building Frontage and Orientation	Main Street offers a continuous wall of positive frontages with associated laneways, fronted by ground floor active uses. However to the backland area west of Main Street the quality of frontage needs improvement.	To the backlands west of Main Street propose new infill development with a positive orientation and frontage to the River Ward.
Building Height and Massing	Traditionally, Swords has been developed to consistent building heights (2-3 storeys) creating a pleasing unity about the town as a whole. This aids the Town Centre's legibility as the heart of the town.	Swords Main Street has developed with buildings of two storeys with a variation in their height from ground to eaves heights. The prevailing roof height should generally be maintained. The variation in the pitches of the street's roofscape should be encouraged. Flat roofed infill developments should be rejected especially at the western edge of Main Street in an effort to preserve the existing pitched roof character. To maintain the varied roofline, detailed layouts of new buildings within the retail/commercial core should avoid straight runs of more than 3 buildings with the same height dimensions. The consistent storey heights within the Town Centre should not be broken by inappropriately tall buildings, particularly around Swords Castle, the County Hall and St Colmcille's Church. Within the backland areas adjacent to the River Ward to the west of Main Street is the most suitable area for buildings of up to five storeys (approx 16m), given its topography relative to the Main Street. At the planning application stage proposals within this area should be subject to a visual impact assessment in an effort to ensure that the integrity of the Main Street is not compromised.
Architectural Features	Many older properties along Main Street have a vertical emphasis to their character, with good solid to void ratios and architectural features of merit.	Ensure the following in new developments to respect the existing character of Main Street: Building Proportion <ul style="list-style-type: none"> • A high solid to void ratio in all new buildings. • Vertical window forms should be promoted to enhance the vertical emphasis. Roofs and Chimneys <ul style="list-style-type: none"> • Roofs should always span the shortest plan dimension to avoid excessively large roof spans. • Roof pitches should generally be between 30°-35°. • Lean-to roofs should be of equal or lower pitch than the main roof. • Flat roofs should be avoided in particular to the western edge of Main Street. • Promote the use of gable and hipped roof styles to reflect the prevailing roof profile. • Discourage the use of gambrel/mansard or bonnet type roof profiles. • Retain existing chimneys, even if not in use, and new chimneys and repairs should match the traditional details. • Avoid the standard modern box eaves with deep projecting fascia, flat soffit and projecting barge boards.

3.4.1 THE RETAIL/COMMERCIAL CORE

Urban Design Principle	Issue	Urban Design Guidelines												
<p>Architectural Features</p>	<p>Many older properties along Main Street have a vertical emphasis to their character, with good solid to void ratios and architectural features of merit.</p>	<p>Shop fronts</p> <ul style="list-style-type: none"> • Modern interpretations of the traditional shop front details are particularly welcomed along Main Street. • Ensure fascias should not be greater than 1/4th of the height of the shop front (as measured from cornice to floor). <p>Security Shutters</p> <ul style="list-style-type: none"> • Where possible shutters should be contained inside the shop behind the window display, so that the shop front and its contents are still visible after opening hours. • The colour of the internal shutters should not cause them to detract from the character of the building or the area and should be dark in colour so that they are less visible from a distance. <p>Hanging and Projecting Elements</p> <ul style="list-style-type: none"> • Projecting or hanging elements should be located at fascia level, their height should be not more than that of the fascia, and generally should not harm the appearance of the rest of the shop front. • In general no more than one hanging sign per shop front will be permitted. <p>Shop Signage</p> <ul style="list-style-type: none"> • New signage should respect the character and style of the building in terms of design and materials • New signage should not obscure architectural features such as windows and other shop front details • Avoid large area of glossy or reflective materials • Avoid excessive signage, projecting elements and clutter • When designing signs consideration should be given to colour and type of lettering. • On signboards, the lettering should not exceed 3/4 of the depth of the fascia. • Generally banner advertisements shall be prohibited along Main Street. <p>Canopies</p> <ul style="list-style-type: none"> • Canopies should be made from traditional canvas and located directly below the fascia. • They should not extend across the pilasters or obscure architectural detail • The preferred canopy for Swords shall be the rigid traditional type • Filled pelmets around the base of the canopy will not be accepted • Curved Dutch and balloon canopies are to be avoided as they are not traditional to Swords and provide minimal shading and shelter. <p>Lighting</p> <ul style="list-style-type: none"> • Lighting should be considered as part of the overall shop front design rather than as an ad-hoc addition. • Lighting sources should be concealed, and carefully directed at the main signage. <p>Outdoor Seating Areas</p> <ul style="list-style-type: none"> • Outdoor seating areas should be located outside/clear of the lines of main pedestrian movement. • All emergency exits and routes must be kept clear. • When in use the outdoor seating area will need to be enclosed, to demarcate the area which contains the tables and chairs, thus making it distinguishable from other footpath users. • Planters can be particularly attractive and can be used as part of the means of enclosure, but may need to be removed from the footpath outside of trading hours. • The furniture should be of a high quality and uniform style within the area. • Where umbrellas are used these must be fabric type (i.e. non reflective). Umbrellas are to be positioned so as to avoid overhanging outside the enclosure or impairing vehicle sight lines. 												
<p>Materials</p>	<p>The buildings along Main Street have a consistent palette of materials – stone, brick, render and slate.</p>	<p>Materials to be used should age well and weather well. The materials specified in the table below are the 'base materials' to be used in a building. Exceptions to this list may be allowed. While glass is obviously permitted, buildings where the entire façade is glazed will not generally be permitted. For the design of façades along Main Street, the relationship between solid (wall) to void (openings) is important, and this is expressed as a percentage where the minimum 'solid' is to comprise 40% of the façade. The maximum void or glazed area is expressed as 60% of the façade.</p> <table border="1" data-bbox="1270 1629 2226 1793"> <thead> <tr> <th data-bbox="1270 1629 1329 1654">Walls</th> <th data-bbox="1590 1629 1650 1654">Roofs</th> <th data-bbox="1911 1629 2030 1654">Windows</th> </tr> </thead> <tbody> <tr> <td data-bbox="1270 1667 1329 1692">Stone</td> <td data-bbox="1590 1667 1739 1692">Natural Slate</td> <td data-bbox="1911 1667 2000 1692">Timber</td> </tr> <tr> <td data-bbox="1270 1705 1442 1759">Brick (in various bonds)</td> <td data-bbox="1590 1705 1798 1759">Metal Standing Seam e.g Lead, Zinc</td> <td data-bbox="1911 1705 2065 1759">Extruded metal (excluding uPVC)</td> </tr> <tr> <td data-bbox="1270 1772 1347 1793">Render</td> <td></td> <td></td> </tr> </tbody> </table>	Walls	Roofs	Windows	Stone	Natural Slate	Timber	Brick (in various bonds)	Metal Standing Seam e.g Lead, Zinc	Extruded metal (excluding uPVC)	Render		
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3.4.2 CIVIC/COMMUNITY

Urban Design Principle	Issue	Urban Design Guidelines
Land Uses	The proposed Metro North stop at Seatown will create increased pedestrian flows. There is an opportunity to increase the vibrancy and vitality in this area. Existing uses of Swords Castle, Fingal County Council and Courthouse at junction of Seatown Road/North St./Main St. indicates a currently strong civic use presence in this area. A primary school and 'Fingal Prosper' are also located along Seatown Road, aswell as a number of residential units.	Promote the development of community and civic related development within this area.
Topography	The Town Centre's flat topography heightens the significance of landmark buildings.	Consider the impact of tall buildings on sightlines and views around Swords.
Views	Swords' largely flat topography increases the importance of street-long views and vistas within the settlement.	Protect Swords building heights and skyline. Protect and enhance view of Swords Castle.
Public Open Space	Seatown Road appears bare where the street does not accommodate green planting or verges.	Planted buffer zones and street trees should be encouraged to increase the amount of greenery on the street.
Legibility	Seatown Road provides a legible structure to the character area as well as key pedestrian and vehicular paths.	Increase the legibility of the area through the use of landmark features such as focal buildings, intimate spaces and street trees.
Connections + Routes	Seatown Road provides direct access eastwards to the R132 and the M1 motorway. The proposed Metro North extension to Swords should reduce the volume of traffic travelling through the centre of Swords and increase its role as a pedestrian friendly urban centre.	Consideration should be given to improving the pedestrian realm along Seatown Road where the numbers of pedestrians is likely to grow with the construction of the Metro.
Parking	Within the civic community character area there exists one overspill surface car park for employees of Fingal County Council. This car park has a capacity of approximately 90 car parking spaces. On street parking can constrict narrow streets in residential areas (Seatown Villas) adjacent to Seatown Road.	Within this area, parking should be on-street where possible or communal in landscaped courts to minimise the interruptions along the footpath. Permit parking could be considered as a means of discouraging users from parking within residential streets such as at Seatown Villas and St Colmcilles Park. The car park at the junction of Seatown Road/North Street has significant development potential. Redevelopment should have regard to these Urban Design Guidelines.
Safety and Security	The Seatown Road is popular with pedestrians. However, differing front boundary treatments and building setbacks make for differing levels of street security. Some are open plan; some have low boundaries whilst others have high walls and dense hedging.	High hedges and walls reduce visual connectivity between the public and private realm, and should be discouraged. The principles of 'Safer Places: the planning system and crime prevention' by the ODPM should be incorporated as part of any new development. Please refer to Appendix 2.
Protected Structures	The character area retains its medieval street pattern and a number of historically significant buildings.	The Parochial House is a local protected structure building along Seatown Road. Swords Castle and the Courthouse are also significant protected structures within this area. The buildings and their curtilages should be retained and their setting enhanced through streetscape improvements to reinforce local character.
Development Block Size	The block layout in the civic/community core is typically organic in form and layout.	New block structures should retain the prevailing block dimensions. However some flexibility and variation in size and shape should be promoted to reinforce the distinctive organic nature of the town.
Urban Grain	On the northern side of Seatown Road the building widths are at least 25m wide. The semidetached dwellings, at the junction of Main Street and North Street have widths that are significantly less at 10 wide.	Use the existing urban grain dimensions as a basis to guide new development. For the Civic/Community character area maintain the prevailing building width of 10m to the southern edge of Seatown Road and up to 30m to the northern edge of Seatown Road. New development can address this grain also through facade articulation.
Quality of Building Frontage	The quality of building frontages are generally positive, with the exception of the frontages at the junction of Main St./North St./Seatown Rd.	Encourage the removal of vacant buildings north of Main Street to allow for high quality public realm fronting Swords Castle. A public realm strategy is required to address the area at the junction of Seatown Rd./North St./Main St.
Building Orientation	Buildings are set in rows facing the street, with some blank frontages. The junction of Seatown Rd./North St. lacks definition given the presence of Fingal County Council car park on the corner.	New development should maintain strong continuity of building frontage addressing the street. A specific site design and development brief is required for the site currently occupied by Fingal County Council car park. Building orientation and addressing of street will be considered as part of this.
Building Height and Massing	Buildings within this character area are predominantly 2 storey with varying floor to ceiling heights.	Specific site design and development briefs are required for opportunity sites along Seatown Road. New development will have regard to existing residential amenity.

3.4.2 CIVIC/COMMUNITY

Urban Design Principle	Issue	Urban Design Guidelines
Architectural Features	Many older properties along Seatown Road have a vertical emphasis to their character, with good solid to void ratios and architectural features of merit.	<p>Ensure the following in new developments to respect the existing character of Seatown Road:</p> <p>Building Proportion</p> <ul style="list-style-type: none"> • A high solid to void ratio in all new buildings. • Vertical window forms should be promoted to enhance the vertical emphasis. <p>Roofs and Chimneys</p> <ul style="list-style-type: none"> • Roofs should always span the shortest plan dimension to avoid excessively large roof spans. • Roof pitches should generally be between 30°-35°. • Lean-to roofs should be of equal or lower pitch than the main roof. • Roof profiles other than pitched are encouraged in the design of civic and community buildings. • Promote the use of a gable roof styles on proposed new residential uses to reflect the prevailing roof profile. • Discourage the use of gambrel/mansard or bonnet type roof profiles. • Retain existing chimneys, even if not in use, and new chimneys and repairs should match the traditional details. • Avoid the standard modern box eaves with deep projecting fascia, flat soffit and projecting barge boards. <p>Shop fronts</p> <ul style="list-style-type: none"> • Modern interpretations of traditional shop front details are particularly welcomed along Seatown Road. • Ensure fascias should not be greater than 1/4th of the height of the shop front (as measured from cornice to floor) <p>Security Shutters</p> <ul style="list-style-type: none"> • Where possible shutters should be contained inside the shop behind the window display, so that the shop front and its contents are still visible after opening hours. • The colour of the internal shutters should not cause them to detract from the character of the building or the area and should be dark in colour so that they are less visible from a distance. <p>Shop Signage</p> <ul style="list-style-type: none"> • New signage should respect the character and style of the building in terms of design and materials • New signage should not obscure architectural features such as windows and other shop front details • Avoid large area of glossy or reflective materials • Avoid excessive signage, projecting elements and clutter • When designing signs consideration should be given to colour and type of lettering. • On signboards, the lettering should not exceed 3/4 of the depth of the fascia. • Generally banner advertisements shall be prohibited along Seatown Street. <p>Canopies</p> <ul style="list-style-type: none"> • Canopies should be made from traditional canvas and located directly below the fascia. • They should not extend across the pilasters or obscure architectural detail • The preferred canopy for Swords shall be the rigid traditional type • Frilled pelmets around the base of the canopy will not be accepted • Curved Dutch and balloon canopies are to be avoided as they are not traditional to Swords and provide minimal shading and shelter. <p>Hanging and Projecting Elements</p> <ul style="list-style-type: none"> • Projecting or hanging elements should be located at fascia level, their height should be not more than that of the fascia, and generally should not harm the appearance of the rest of the shop front. • In general no more than one hanging sign per shop front will be permitted. <p>Lighting</p> <ul style="list-style-type: none"> • Lighting should be considered as part of the overall shop front design rather than as an ad-hoc addition. • Lighting sources should be concealed, and carefully directed at the main signage. <p>Outdoor Seating Areas</p> <ul style="list-style-type: none"> • Outdoor seating areas should be located outside/clear of the lines of main pedestrian movement. • All emergency exits and routes must be kept clear. • When in use the outdoor seating area will need to be enclosed, to demarcate the area which contains the tables and chairs, thus making it distinguishable from other footpath users. • Planters can be particularly attractive and can be used as part of the means of enclosure, but may need to be removed from the footpath outside of trading hours. • The furniture should be of a high quality and uniform style within the area. • Where umbrellas are used these must be fabric type (i.e. non reflective). Umbrellas are to be positioned so as to avoid overhanging outside the enclosure or impairing.

3.4.2 CIVIC/COMMUNITY

Urban Design Principle	Issue	Urban Design Guidelines																					
Materials	Buildings along Seatown Road have a consistent palette of materials – stone, brick and slate. Recent development to the northern edge of Seatown Road has introduced more innovative materials and features in keeping with the colour palette of the area.	<p>Materials to be used at Seatown Road should age well and weather well. The materials specified in the table below are the 'base materials' to be used in a building. Exceptions to this list may be allowed. While glass is obviously permitted, buildings where the entire façade is glazed will not generally be permitted. For the design of façades along Main Street, the relationship between solid (wall) to void (openings) is important, and this is expressed as a percentage where the minimum 'solid' is to comprise 40% of the façade. The maximum void or glazed area is expressed as 60% of the façade.</p> <table border="1"> <thead> <tr> <th>Walls</th> <th>Roofs</th> <th>Windows</th> </tr> </thead> <tbody> <tr> <td>Stone</td> <td>Natural Slate</td> <td>Timber</td> </tr> <tr> <td>Brick (in various bonds)</td> <td>Metal Standing Seam e.g Lead, Zinc</td> <td>Extruded metal (excluding uPVC)</td> </tr> <tr> <td>Render</td> <td>Photovoltaics</td> <td></td> </tr> <tr> <td>Wood (i.e. iroko, red cedar, oak)</td> <td>Green Roof</td> <td></td> </tr> <tr> <td>Metal (i.e. copper, aluminium)</td> <td>Fibre Cement Slate</td> <td></td> </tr> <tr> <td>Concrete</td> <td></td> <td></td> </tr> </tbody> </table>	Walls	Roofs	Windows	Stone	Natural Slate	Timber	Brick (in various bonds)	Metal Standing Seam e.g Lead, Zinc	Extruded metal (excluding uPVC)	Render	Photovoltaics		Wood (i.e. iroko, red cedar, oak)	Green Roof		Metal (i.e. copper, aluminium)	Fibre Cement Slate		Concrete		
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3.4.3(a) COMMERCIAL/RESIDENTIAL – NORTH STREET EAST

Urban Design Principle	Issue	Urban Design Guidelines
Land Uses	North Street East contains a range of valued local comparison and service provision. This is primarily concentrated around the junctions both north and south of North Street.	Protect and enhance the local comparison, office and service provision within North Street.
Topography	The character area's flat topography heightens the significance of the protected structures within the area.	Buildings here should fit in with the existing townscape which brings out Swords local topography. Consider the impact of buildings on sightlines and views within this character area.
Views	The protected structures as outlined in Appendix 1 (Protected Structures) provide some positive short range views within North Street East.	Protect the views of the protected structures along North Street East.
Public Open Space	There is limited green landscaping along North Street East.	Potential opportunities to create new public spaces along the street should be explored such as at the junction of Seatown Road and North Street. Consider the potential to introduce additional green space and street tree planting as part of new development.
Legibility	This eastern edge of North Street contains some very legible features: the historic street shape of North Street and the historic buildings which lines them.	Ensure that key landmarks such as the protected structures east of North Street East remain dominant within the streetscape. Consider opportunities to enhance on street parking provision along North Street. Consider the introduction of a coherent signage strategy across the town to improve legibility.
Connections + Routes	North Street East is the key direct north-south route through Swords and will be a direct route to the proposed Estuary Metro Stop. Pedestrian footpaths and the quality of the public realm within this area are generally poor. At certain parts along North Street East illegal parking on the footpaths are evident.	The environment within North Street East should be made conducive to pedestrians and on-street car parking. Pedestrian crossings should be protected and enhanced, and on-street car parking should be improved upon.
Parking	On-street parking is an important convenience for visitors to North Street. There are no significant off-street car parks in the area, which increases pressure on neighbouring residential streets.	Consider carefully parking provision in the character area and explore the potential for additional public parking as part of new developments. The existing on-street parking within the North Street East area should be protected and bays clearly delineated.
Safety and Security	To the east of North Street, directly north of the Carnegie Court Hotel block there exists a street of high walls and inactive frontages to the public realm.	Encourage where possible low boundary walls/hedging to ensure optimum surveillance of the public realm. The principles of 'Safer Places: the planning system and crime prevention' by the ODPM should be incorporated as part of any new development. Please refer to Appendix 2.

3.4.3(a) COMMERCIAL/RESIDENTIAL – NORTH STREET EAST

Urban Design Principle	Issue	Urban Design Guidelines
Protected Structures	The character area retains its medieval street pattern and a number of historically significant buildings.	With several protected structures this is therefore a sensitive area for new architecture, and an area where high public realm quality is of utmost priority. New buildings and public realm improvements should compliment the existing protected structures, historic streetscape and high quality of architecture. Work to Protected Structures should match the original details and materials, and specialist advice sought prior to any major repair or refurbishment. Normally planning permission will be required for any such works.
Development Block Size	The block layout in the retail/commercial core is typically organic in form and layout.	New block structures should include flexibility and variation in size and shape to promote the distinctive organic nature of the town.
Urban Grain	North Street East to the east has a strongly defined consistent urban grain which provides a legible layout.	Opportunities to reinforce the urban grain should be explored east and west of North Street particularly where there are breaks in the building line and gap sites. A building width of between 10m-30m should be promoted to the eastern edge of North Street. The vertical articulation of façades of plots wider than 20m should be modulated vertically to avoid an overly monolithic appearance.
Quality of Building Frontage	To the east of North Street the quality of frontage is positive.	Buildings should provide a high quality frontage directly to the street along North Street to provide good enclosure and enhance the public realm.
Building Orientation	To the east of North Street buildings maintain a strong continuity of active frontage to the street.	New development, to the east of North Street should maintain strong continuity of building frontage along the street.
Building Height and Massing	North Street to the east is predominantly 3 storey in scale.	Up to 3 storeys is appropriate to the east of North Street, so long as they do not adversely impact upon existing residential amenity.
Architectural Features	Many older properties along the eastern edge of North Street have a vertical emphasis to their character, with good solid to void ratios and architectural features of merit. The rooflines across the street are consistent with pitched roofs.	<p>Ensure the following in new developments to respect the existing character of North Street East:</p> <p>Building Proportion</p> <ul style="list-style-type: none"> • A high solid to void ratio in all new buildings. • Vertical window forms should be promoted to enhance the vertical emphasis. <p>Roofs and Chimneys</p> <ul style="list-style-type: none"> • Roofs should always span the shortest plan dimension to avoid excessively large roof spans. • Roof pitches should generally be between 35°-40°. • Lean-to roofs should be of equal or lower pitch than the main roof. • Flat roofs should be avoided in particular to the eastern edge of North Street. • Promote the use of a gable and hipped roof styles to reflect the prevailing roof profile. • Discourage the use of gambrel/mansard or bonnet type roof profiles. • Retain existing chimneys, even if not in use, and new chimneys and repairs should match the traditional details. • Avoid the standard modern box eaves with deep projecting fascia, flat soffit and projecting barge boards. <p>Shop fronts</p> <ul style="list-style-type: none"> • Modern interpretations of the traditional shop front details are particularly welcomed along of North Street East. • Ensure fascias should not be greater than 1/4th of the height of the shop front (as measured from cornice to floor). <p>Security Shutters</p> <ul style="list-style-type: none"> • Where possible shutters should be contained inside the shop behind the window display, so that the shop front and its contents are still visible after opening hours. • The colour of the internal shutters should not cause them to detract from the character of the building or the area and should be dark in colour so that they are less visible from a distance. <p>Shop Signage</p> <ul style="list-style-type: none"> • New signage should respect the character and style of the building in terms of design and materials • New signage should not obscure architectural features such as windows and other shop front details • Avoid large area of glossy or reflective materials • Avoid excessive signage, projecting elements and clutter • When designing signs consideration should be given to colour and type of lettering. • On signboards, the lettering should not exceed 3/4 of the depth of the fascia. • Generally banner advertisements shall be prohibited along North Street East. <p>Canopies</p> <ul style="list-style-type: none"> • Canopies should be made from traditional canvas and located directly below the fascia. • They should not extend across the pilasters or obscure architectural detail • The preferred canopy for Swords shall be the rigid traditional type • Frilled pelmets around the base of the canopy will not be accepted • Curved Dutch and balloon canopies are to be avoided as they are not traditional to Swords and provide minimal shading and shelter.

3.4.3(a) COMMERCIAL/RESIDENTIAL – NORTH STREET EAST

Urban Design Principle	Issue	Urban Design Guidelines												
Architectural Features	Many older properties along the eastern edge of North Street have a vertical emphasis to their character, with good solid to void ratios and architectural features of merit. The rooflines across the street are consistent with pitched roofs.	<p>Hanging and Projecting Elements</p> <ul style="list-style-type: none"> Projecting or hanging elements should be located at fascia level, their height should be not more than that of the fascia, and generally should not harm the appearance of the rest of the shop front. In general no more than one hanging sign per shop front will be permitted. <p>Lighting</p> <ul style="list-style-type: none"> Lighting should be considered as part of the overall shop front design rather than as an ad-hoc addition. Lighting sources should be concealed, and carefully directed at the main signage. <p>Outdoor Seating Areas</p> <ul style="list-style-type: none"> Outdoor seating areas should be located outside/clear of the lines of main pedestrian movement. All emergency exits and routes must be kept clear. When in use the outdoor seating area will need to be enclosed, to demarcate the area which contains the tables and chairs, thus making it distinguishable from other footpath users. Planters can be particularly attractive and can be used as part of the means of enclosure, but may need to be removed from the footpath outside of trading hours. The furniture should be of a high quality and uniform style within the area. Where umbrellas are used these must be fabric type (i.e. non reflective). Umbrellas are to be positioned so as to avoid overhanging outside the enclosure or impairing vehicle sight lines. 												
Materials	Buildings along North Street East have a consistent palette of materials – stone, brick and slate. Recent development to the northern edge of North Street has respected this palette.	<p>Materials to be used to the east of North Street should age well and weather well. The materials specified in the table below are the ‘base materials’ to be used in a building. Exceptions to this list may be allowed. While glass is obviously permitted, buildings where the entire façade is glazed will not generally be permitted. For the design of façades to the east of North Street, the relationship between solid (wall) to void (openings) is important, and this is expressed as a percentage where the minimum ‘solid’ is to comprise 40% of the façade. The maximum void or glazed area is expressed as 60% of the façade.</p> <table border="1"> <thead> <tr> <th>Walls</th> <th>Roofs</th> <th>Windows</th> </tr> </thead> <tbody> <tr> <td>Stone</td> <td>Natural Slate</td> <td>Timber</td> </tr> <tr> <td>Brick (in various bonds)</td> <td>Metal Standing Seam e.g Lead, Zinc</td> <td>Extruded metal (excluding uPVC)</td> </tr> <tr> <td>Render</td> <td></td> <td></td> </tr> </tbody> </table>	Walls	Roofs	Windows	Stone	Natural Slate	Timber	Brick (in various bonds)	Metal Standing Seam e.g Lead, Zinc	Extruded metal (excluding uPVC)	Render		
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Render														

3.4.3(b) COMMERCIAL/RESIDENTIAL – NORTH STREET WEST

Urban Design Principle	Issue	Urban Design Guidelines
Land Uses	North Street West contains a range of valued local comparison and service provision. This is primarily concentrated around the junctions both north and south of North Street. There are a number of uncharacteristic Town Centre land uses to the west of North Street including a few car showrooms and a crash repairs yard.	Protect and enhance the local comparison, office and service provision within North Street. Discourage the use of large building formats such as garages, showrooms etc at this location. However, commercial and retail anchor tenants will be encouraged.
Topography	The character area’s flat topography heightens the significance of the protected structures within the area.	Buildings here should fit in with the existing townscape which brings out Swords local topography. Consider the impact of buildings on sightlines and views within this character area especially in relation to Swords Castle and also in relation to views from the Town Park.
Views	The protected structures as outlined in Section 5 (Protected Structures) provide some positive short range views within North Street.	Protect the views of the protected structures along North Street and especially to Swords Castle and also in relation to views from the Town Park.
Public Open Space	There is limited landscaping along North Street. Swords Town Park is located to the west of North Street with two access points entered from the street.	Consider the potential to introduce additional green space and street tree planting as part of new development proposals along the western edge of North Street.

3.4.3(b) COMMERCIAL/RESIDENTIAL – NORTH STREET WEST

Urban Design Principle	Issue	Urban Design Guidelines
Legibility	This character area contains some very legible features: Swords Castle, the historic street shape of North Street and the historic buildings which line them.	Ensure that key landmarks such as the protected structures along North Street remain dominant within the streetscape. Consider opportunities to enhance on-street parking provision along North Street. Consider the introduction of a coherent signage strategy across the town to improve legibility.
Connections + Routes	North Street is the key direct north-south route through Swords and will be a direct route to the proposed Estuary Metro Stop. Pedestrian footpaths and the quality of the public realm within this area are generally poor. At certain parts along North Street illegal parking on the footpaths are evident.	The environment within North Street should be made conducive to pedestrians and on-street car parking. Pedestrian crossings should be protected and enhanced, and on-street car parking should be improved upon. The existing access way to Swords Town Park to the northern end of North Street should be environmentally improved to provide an attractive gateway to the park.
Parking	On-street parking is an important convenience for visitors to the North Street area, although it creates some problems in terms of safety issues. There are no significant off-street car parks in the area, which increases pressure on neighboring residential streets.	Consider carefully parking provision in the character area and explore the potential for additional public parking as part of new developments to the western edge of North Street. The existing on-street parking within the North Street area should be protected and bays clearly delineated.
Safety and Security	The existing access way to Swords Town Park to the northern western end of North Street is poorly lit and lacking in natural surveillance.	Improve lighting levels along key pedestrian footpaths leading to Swords Town Park to the west of North Street. Consider the potential of utilizing the long rear gardens west of North Street to front development onto Swords Town Park and therefore improve natural surveillance. The principles of 'Safer Places: the planning system and crime prevention' by the ODPM should be incorporated as part of any new development. Please refer to Appendix 2.0 of this document.
Protected Structures	Unlike the eastern edge of North Street the western edge of contains no protected structures. However buildings of interest exist such as the terrace of two storey residential buildings to the north western end.	Protect the existing architectural character of the street.
Development Block Size	The block dimensions to the west of North Street are large in comparison to the retail/commercial core of Main Street.	Promote new block structures which should include flexibility and variation in size and shape to promote the distinctive organic nature of the town.
Urban Grain	The majority of North Street to the west has a loose urban grain due to the prevalence of large format garages. To the northern and southern end the existing residential terrace and recent infill development respectively provides a fine urban grain.	Opportunities to reinforce a fine urban grain should be explored west of North Street particularly where there are breaks in the building line and gap sites. A building width of between 10m-15m should be promoted to the western edge of North Street. New development can address this grain through facade design with variations in the façade composition to echo the historical grain pattern.
Quality of Building Frontage	To the west of North Street some buildings possess frontages of a negative quality.	Buildings should provide a high quality frontage directly to the street along North Street to provide good enclosure and enhance the public realm. Landmark pavilion-type buildings providing views and access to the town park will also be facilitated along North Street west.
Building Orientation	Along North Street to the west the significant building set backs from the large format garages present an inactive frontage to the public realm. To the backlands west of North Street, rear gardens and high walls present an unattractive inactive frontage to the Town Park.	New development, especially to the west of North Street should maintain strong continuity of building frontage along North Street. To the backlands, opportunities to front buildings onto Swords Town Park should be promoted to increase safety and security in the area.
Building Height and Massing	The northern end of North Street is predominantly 2 storeys in scale. However to the southern end of North Street with recent infill developments heights of three storeys exist.	Between two and three storeys is appropriate to the west of North Street, so long as they do not adversely impact upon existing residential amenity.
Architectural Features	The older properties to the northern edge of North Street as well as the recent infill development to the south of the street have a vertical emphasis to their character, with good solid to void ratios and architectural features of merit.	Ensure the following in new developments to respect the existing character of North Street West: Building Proportion <ul style="list-style-type: none"> • A high solid to void ratio in all new buildings. • Vertical window forms should be promoted to enhance the vertical emphasis. Roofs and Chimneys <ul style="list-style-type: none"> • Roofs should always span the shortest plan dimension to avoid excessively large roof spans. • Roof pitches should generally be between 30°-35°. • Lean-to roofs should be of equal or lower pitch than the main roof. • Flat roofs and other roof profiles should be promoted at North Street West. • Promote the use of a gable and hipped roof styles to reflect the prevailing roof profile. • Discourage the use of gambrel/mansard or bonnet type roof profiles. • Retain existing chimneys, even if not in use, and new chimneys and repairs should match the traditional details. • Avoid the standard modern box eaves with deep projecting fascia, flat soffit and projecting barge boards.

3.4.3(b) COMMERCIAL/RESIDENTIAL – NORTH STREET WEST

Urban Design Principle	Issue	Urban Design Guidelines																					
<p>Architectural Features</p>	<p>The older properties to the northern edge of North Street as well as the recent infill development to the south of the street have a vertical emphasis to their character, with good solid to void ratios and architectural features of merit.</p>	<p>Shop fronts</p> <ul style="list-style-type: none"> • Modern interpretations of the traditional shop front details are particularly welcomed along North Street West. • Ensure fascias should not be greater than 1/4th of the height of the shop front (as measured from cornice to floor) <p>Security Shutters</p> <ul style="list-style-type: none"> • Where possible shutters should be contained inside the shop behind the window display, so that the shop front and its contents are still visible after opening hours. • The colour of the internal shutters should not cause them to detract from the character of the building or the area and should be dark in colour so that they are less visible from a distance. <p>Shop Signage</p> <ul style="list-style-type: none"> • New signage should respect the character and style of the building in terms of design and materials • New signage should not obscure architectural features such as windows and other shopfront details • Avoid large area of glossy or reflective materials • Avoid excessive signage, projecting elements and clutter • When designing signs consideration should be given to colour and type of lettering. • On signboards, the lettering should not exceed 3/4 of the depth of the fascia. • Generally banner advertisements shall be prohibited along North Street West. <p>Canopies</p> <ul style="list-style-type: none"> • Canopies should be made from traditional canvas and located directly below the fascia. • They should not extend across the pilasters or obscure architectural detail • The preferred canopy for Swords shall be the rigid traditional type • Frilled pelmets around the base of the canopy will not be accepted • Curved Dutch and balloon canopies are to be avoided as they are not traditional to Swords and provide minimal shading and shelter. <p>Hanging and Projecting Elements</p> <ul style="list-style-type: none"> • Projecting or hanging elements should be located at fascia level, their height should be not more than that of the fascia, and generally should not harm the appearance of the rest of the shopfront. • In general no more than one hanging sign per shopfront will be permitted. <p>Lighting</p> <ul style="list-style-type: none"> • Lighting should be considered as part of the overall shopfront design rather than as an ad-hoc addition. • Lighting sources should be concealed, and carefully directed at the main signage. <p>Outdoor Seating Areas</p> <ul style="list-style-type: none"> • Outdoor seating areas should be located outside/clear of the lines of main pedestrian movement. • All emergency exits and routes must be kept clear. • When in use the outdoor seating area will need to be enclosed, to demarcate the area which contains the tables and chairs, thus making it distinguishable from other footpath users. • Planters can be particularly attractive and can be used as part of the means of enclosure, but may need to be removed from the footpath outside of trading hours. • The furniture should be of a high quality and uniform style within the area. • Where umbrellas are used these must be fabric type (i.e. non reflective). Umbrellas are to be positioned so as to avoid overhanging outside the enclosure or impairing vehicle sight lines. 																					
<p>Materials</p>	<p>Existing residential development to the northern edge of North Street West retains a consistent palette of materials – render, brick and slate. In contrast, recent infill development to the southern edge of North Street displays a wide variety of materials.</p>	<p>Materials to be used to the west of North Street should age well and weather well. The materials specified in the table below are the ‘base materials’ to be used in a building. Exceptions to this list may be allowed. For the design of façades to the west of North Street, the relationship between solid (wall) to void (openings) is important, and this is expressed as a percentage where the minimum ‘solid’ is to comprise 25% of the façade. The maximum void or glazed area is expressed as 75% of the façade.</p> <table border="1" data-bbox="1270 1619 2226 1892"> <thead> <tr> <th data-bbox="1270 1619 1329 1650">Walls</th> <th data-bbox="1626 1619 1685 1650">Roofs</th> <th data-bbox="1941 1619 2030 1650">Windows</th> </tr> </thead> <tbody> <tr> <td data-bbox="1270 1654 1329 1686">Stone</td> <td data-bbox="1626 1654 1762 1686">Natural Slate</td> <td data-bbox="1941 1654 2030 1686">Timber</td> </tr> <tr> <td data-bbox="1270 1690 1448 1749">Brick (in various bonds)</td> <td data-bbox="1626 1690 1834 1749">Metal Standing Seam e.g Lead, Zinc</td> <td data-bbox="1941 1690 2119 1749">Extruded metal (excluding uPVC)</td> </tr> <tr> <td data-bbox="1270 1753 1347 1785">Render</td> <td data-bbox="1626 1753 1762 1785">Photovoltaics</td> <td></td> </tr> <tr> <td data-bbox="1270 1789 1584 1820">Wood (i.e. iroko, red cedar, oak)</td> <td data-bbox="1626 1789 1745 1820">Green Roof</td> <td></td> </tr> <tr> <td data-bbox="1270 1824 1584 1856">Metal (i.e. copper, aluminium)</td> <td data-bbox="1626 1824 1822 1856">Fibre Cement Slate</td> <td></td> </tr> <tr> <td data-bbox="1270 1860 1377 1892">Concrete</td> <td></td> <td></td> </tr> </tbody> </table>	Walls	Roofs	Windows	Stone	Natural Slate	Timber	Brick (in various bonds)	Metal Standing Seam e.g Lead, Zinc	Extruded metal (excluding uPVC)	Render	Photovoltaics		Wood (i.e. iroko, red cedar, oak)	Green Roof		Metal (i.e. copper, aluminium)	Fibre Cement Slate		Concrete		
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Concrete																							

3.4.3(c) COMMERCIAL/RESIDENTIAL – NORTH STREET NORTH

Urban Design Principle	Issue	Urban Design Guidelines
Land Uses	North Street contains a range of uses including commercial, retail and community uses. A site design and development brief is required to be prepared for this area.	Protect and enhance a mixed use typology in accordance with a future site design and development brief.
Topography	The character area’s flat topography potentially will heighten the significance of landmark buildings within the area.	Buildings here should fit in with the existing townscape which brings out Swords local topography. Consider the impact of buildings on sightlines and views within this character area.
Views	n/a	n/a
Public Open Space	The River Ward with some mature tree stands skirts the western edge of the block.	Potential opportunities to create new public spaces along the River Ward should be explored at the western edge of the block.
Legibility	This southern edge of the block consists of a landmark building which assists in the areas legibility.	Ensure that any key landmark building proposed within this block remains dominant within the streetscape. Consider the introduction of a coherent signage strategy within this area and also across the town to improve legibility.
Connections	The block is located along the direct route to the proposed Estuary Metro Stop. Pedestrian footpaths and the quality of the public realm within this area are generally poor.	The pedestrian environment within this block should be made conducive to pedestrians and on-street car parking. Pedestrian crossings should be protected and enhanced, and on-street car parking should be improved upon.
Parking	There are no significant off-street car parks in the area.	Consider carefully parking provision in the character area and explore the potential for additional public parking as part of new developments.
Safety and Security	To the western and northern edge of the block there exists areas of high walls and inactive frontages to the public realm.	Encourage where possible low boundary walls/hedging to ensure optimum surveillance of the public realm. Buildings should front onto the public realm to encourage ‘eyes on the street’. The principles of ‘Safer Places: the planning system and crime prevention’ by the ODPM should be incorporated as part of any new development. Please refer to Appendix 2.0 of this document.
Protected Structures	The area contains no protected or historically significant buildings.	n/a
Development Block Size	The block layout is typically organic in form and layout.	Promote new block structures which should include flexibility and variation in size and shape to promote the distinctive organic nature of the town.
Urban Grain	This area has an inconsistent loose urban grain which detracts from the overall legibility of the area.	Opportunities to reinforce the urban grain should be explored particularly where there are breaks in the building line and gap sites.
Quality of Building Frontage	To the north and east of the area there are buildings where the frontage is of a negative quality.	Buildings should provide a high quality frontage directly to the street to provide good enclosure and enhance the public realm.
Building Orientation	To the south of the block buildings maintain a strong continuity of active frontage to the street.	New development to the east, west and north of the block should maintain strong continuity of building frontage along the street.
Building Height and Massing	The massing of the 4 storey commercial block at the northern end of North Street dominates the street.	A consistent height of up to five storeys is appropriate at this location. There is also potential for a landmark building within this gateway area with heights exceeding the recommended five stories. This landmark building should retain an appropriate slenderness ratio of between 1:3 and 1:4. At the planning application stage proposals for a landmark building within this area should be subject to a visual impact assessment in an effort to ensure that the integrity of the area is not compromised.

3.4.3(c) COMMERCIAL/RESIDENTIAL – NORTH STREET NORTH

Urban Design Principle	Issue	Urban Design Guidelines
Architectural Features	The older properties to the northern edge of North Street as well as the recent infill development to the south of the street have a vertical emphasis to their character, with good solid to void ratios and architectural features of merit.	<p>Ensure the following guidance criteria in new developments to North Street North:</p> <p>Building Proportion</p> <ul style="list-style-type: none"> • A high solid to void ratio in all new buildings. • Vertical window forms should be promoted to enhance the vertical emphasis. <p>Roofs and Chimneys</p> <ul style="list-style-type: none"> • Roofs should always span the shortest plan dimension to avoid excessively large roof spans. • Roof pitches should generally be between 30°-35°. • Lean-to roofs should be of equal or lower pitch than the main roof. • Flat roofs and other roof profiles should be promoted at this location. • Discourage the use of gambrel/mansard or bonnet type roof profiles. • Retain existing chimneys, even if not in use, and new chimneys and repairs should match the traditional details. • Avoid the standard modern box eaves with deep projecting fascia, flat soffit and projecting barge boards. <p>Shop fronts</p> <ul style="list-style-type: none"> • Modern interpretations of the traditional shopfront details are particularly welcomed. • Ensure fascias should not be greater than 1/4th of the height of the shopfront (as measured from cornice to floor) <p>Security Shutters</p> <ul style="list-style-type: none"> • Where possible shutters should be contained inside the shop behind the window display, so that the shopfront and its contents are still visible after opening hours. • The colour of the internal shutters should not cause them to detract from the character of the building or the area and should be dark in colour so that they are less visible from a distance. <p>Shop Signage</p> <ul style="list-style-type: none"> • New signage should respect the character and style of the building in terms of design and materials • New signage should not obscure architectural features such as windows and other shopfront details • Avoid large area of glossy or reflective materials • Avoid excessive signage, projecting elements and clutter • When designing signs consideration should be given to colour and type of lettering. • On signboards, the lettering should not exceed 3/4 of the depth of the fascia. • Generally banner advertisements shall be prohibited. <p>Canopies</p> <ul style="list-style-type: none"> • Canopies should be made from traditional canvas and located directly below the fascia. • They should not extend across the pilasters or obscure architectural detail • The preferred canopy for Swords shall be the rigid traditional type • Frilled pelmets around the base of the canopy will not be accepted • Curved Dutch and balloon canopies are to be avoided as they are not traditional to Swords and provide minimal shading and shelter. <p>Hanging and Projecting Elements</p> <ul style="list-style-type: none"> • Projecting or hanging elements should be located at fascia level, their height should be not more than that of the fascia, and generally should not harm the appearance of the rest of the shopfront. • In general no more than one hanging sign per shopfront will be permitted. <p>Lighting</p> <ul style="list-style-type: none"> • Lighting should be considered as part of the overall shopfront design rather than as an ad-hoc addition. • Lighting sources should be concealed, and carefully directed at the main signage. <p>Outdoor Seating Areas</p> <ul style="list-style-type: none"> • Outdoor seating areas should be located outside/clear of the lines of main pedestrian movement. • All emergency exits and routes must be kept clear. • When in use the outdoor seating area will need to be enclosed, to demarcate the area which contains the tables and chairs, thus making it distinguishable from other footpath users. • Planters can be particularly attractive and can be used as part of the means of enclosure, but may need to be removed from the footpath outside of trading hours. • The furniture should be of a high quality and uniform style within the area. • Where umbrellas are used these must be fabric type (i.e. non reflective). Umbrellas are to be positioned so as to avoid overhanging outside the enclosure or impairing vehicle sight lines.

3.4.3(c) COMMERCIAL/RESIDENTIAL – NORTH STREET NORTH

Urban Design Principle

Issue

Materials

The buildings palette within this area have a variety of materials – stone, brick, render, metal.

Urban Design Guidelines

Materials to be used within North Street North should age well and weather well. The materials specified in the table below are the 'base materials' to be used in a building. Exceptions to this list may be allowed. For the design of façades to the west of North Street, the relationship between solid (wall) to void (openings) is important, and this is expressed as a percentage where the minimum 'solid' is to comprise 25% of the façade. The maximum void or glazed area is expressed as 75% of the façade.

Walls	Roofs	Windows
Stone	Natural Slate	Timber
Brick (in various bonds)	Metal Standing Seam e.g Lead, Zinc	Extruded metal (excluding uPVC)
Render	Photovoltaics	
Wood (i.e. iroko, red cedar, oak)	Green Roof	
Metal (i.e. copper, aluminium)	Fibre Cement Slate	
Concrete		

3.5 Architectural Features

In conjunction with the Urban Design Guidelines set out in the previous section, the following section identifies the key architectural features within the existing built environment in the Town Centre, which need to be considered in any future development proposals.

The following themes are considered:

- Building Proportion
- Roofs and Chimneys
- Respecting the Traditional Shopfront
- Security Shutters
- Shop Signage
- Hanging and Projecting Elements
- Canopies
- Outdoor Seating Areas

BUILDING PROPORTION

Many older properties along Main Street have a vertical emphasis to their character, having narrow frontages mostly of two storeys. Vertical windows enhance the vertical emphasis of the building elevation. However in recent times typical modern insertions have introduced a horizontal element that can be visually jarring to the prevailing vertical rhythm. This can be addressed in a number of ways, for example subdivision of the glass with mullions or the subdivision of the shopfront with pilasters or vertical columns.

A solid to void ratio describes how blank or windowed a building looks. It is interesting to note that the more successful buildings that fit well in the Main Street have a high degree of visual solidity. To respect this tradition in Swords Town Centre it is imperative that all new buildings maintain a high solid to void ratio.



A poor ratio of solid to void. The shopfront appears too horizontal.



A good ratio of solid to void. The shopfronts appear vertical.



A good ratio of solid to void.

ROOFS AND CHIMNEYS

The roofscape of Swords is the first indication we have of the character and scale from a distance. On entering the town one becomes aware of the shape and elements of the building roofs which contribute to our general impressions of the town.

Most of the buildings along the western edge of the Main Street and the eastern edge of North Street have pitched roofs that are uncomplicated and low. Subtle changes of pitch between adjoining dwellings, which is characteristic of these streets (a matter of a few degrees) can help enliven a roofscape. Also, a distinctive feature is the use of hipped roofs at the end of a terrace, or on isolated properties. Flat roofs and other roof profiles are evident on more recent insertions to the eastern edge of the Main Street and the western edge of North Street.

Traditionally, chimneys are of stone, render or brick with a corbel detail at the top and finished with a terracotta pot. It is important that any new infill development retain the existing chimney proportions and match the traditional details.

The depth and projection of the fascia is a key design issue. The eaves lines along the Main Street with simple and shallow timber fascias are visually more successful than the standard modern box eave with deep projecting fascia, flat soffit and projecting barge board that is evident to the eastern edge of the Street.



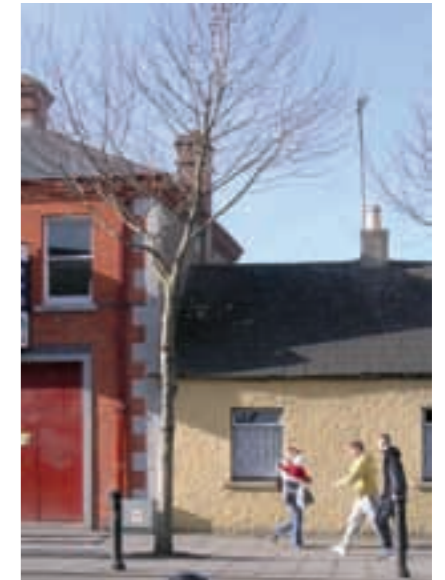
Characteristic roofs and chimneys of Swords



A distinctive feature is the use of hipped roofs at the end of a terrace, or on isolated properties.



Subtle changes of pitch between adjoining dwellings (a matter of a few degrees) can help enliven a roofscape.



Avoid the span of roofs over long distances.



Flat roofs are inconsistent to the prevailing pitched roofscape and should generally be avoided on the Main Street.



Examples of a shallow eaves line.



Example of a modern box eave.

RESPECTING THE TRADITIONAL SHOPFRONT

Existing shopfronts in Swords are based upon a series of principles that are successful in achieving a suitable relationship between the shopfront and the elevation as a whole. Key features of the traditional shopfront are outlined below:

Fascia

The fascia provides the principal space for the name of the shop/business. Best practice states that fascias should not be greater than 1/5th of the height of the shopfront (cornice to floor). The fascia should contain only the name of the business, and the street number, and should not extend uninterrupted across a number of buildings.

Cornice

Above the fascia is a cornice, a projecting decorative moulding to give a visual cap.

Pilasters

Pilasters are the vertical columns built into traditional shopfronts that are used to define the width of a shop. The pilasters visually support the fascia and give vertical rhythm.

Corbels

These contain the fascia by being placed at either end, sitting above the pilaster. They are a key feature of traditional shopfronts. Modern equivalents used throughout Swords can appear over simplistic or crude.

Windows and Doors

Excessively large unbroken areas of glass should be avoided, in favour of vertically subdivided areas. Recessed entrances can help to overcome the scale of window panes. Doors can be located in the centre of the shopfront or adjacent to one of the pilasters.

Modern Shopfronts

In the case of modern shopfronts, creative interpretation of the traditional shopfront design principles (as set out above) will be encouraged. The creation of innovative shopfront designs, where they generate visual interest, while not detracting from the property, or Main Street generally, will be supported.



Elements of a traditional shopfront



A modern interpretation of a traditional shopfront

Examples of Modern Shopfronts:



SECURITY SHUTTERS

The appearance of shops after normal hours has an important impact on shoppers and visitors perception of Swords. External solid shutters can have a detrimental effect on the visual appearance of both the shopfront and the street itself. Planning permission is required for the erection of roller shutters. External roller shutters will generally not be permitted. However, if they are the only solution in a particular case, they should be an open mesh design in preference to a solid screen.

Where possible shutters should be contained inside the shop behind the window display, so that the shopfront and its contents are still visible after opening hours. The colour of the internal shutters should not cause them to detract from the character of the building or the area and should be dark in colour so that they are less visible from a distance.



External solid shutters having a deadening effect during the day.



A good example of security shuttering: Internal shutters behind glass.

SHOP SIGNAGE & FASCIA LETTERING

In general, shop signage in Swords town have a size and design that detracts from the character of buildings and undermines the quality of the street, in particular along Main Street.

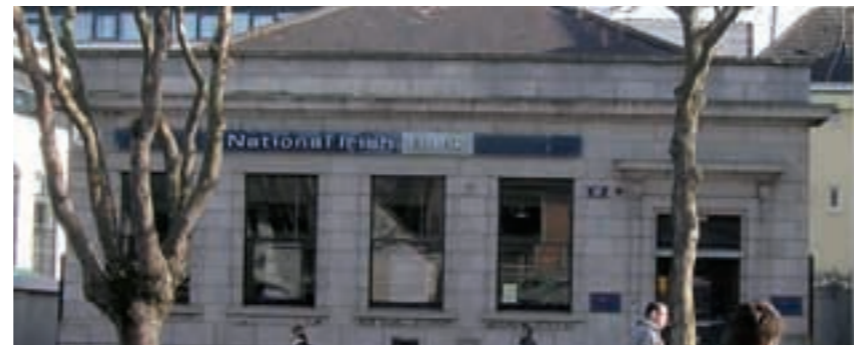
New signage should respect the character and style of the building in terms of design and materials. Protected Structures will require signage to be of a very high standard. Signage placed on protected structures is subject to planning permission.

New signage should not obscure architectural features such as windows and other shopfront details. Excessive amounts of signage serve only to clutter and detract from the overall streetscene.

The materials for shop signage should relate directly to the character of the building and the immediate area. Large areas of glossy or reflective materials are not acceptable. Script style and scale should complement the period of the buildings and scale of the space available. Internally illuminated and plastic fascia boxes are not appropriate. When designing signs consideration should be given to colour and type of lettering. On signboards, the lettering should not exceed 3/4 of the depth of the fascia.



Untidy and overly busy signage is confusing. Large banner advertisements are harmful to the appearance of the traditional buildings.



A clear and well proportioned fascia sign.



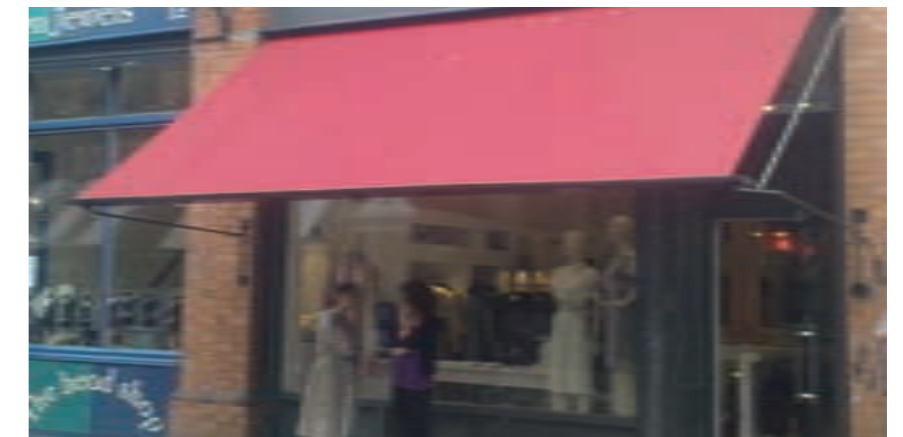
The location, type and colour of lettering respects the overall proportion of the building.

HANGING AND PROJECTING ELEMENTS

Hanging and projecting elements can add interest to a street environment. However too many elements of this type can cause clutter in the building and streetscape. Normally, projecting or hanging elements should be located at fascia level, their height should be not more than that of the fascia, and they should not harm the appearance of the rest of the shopfront. In general no more than one hanging sign per shopfront will be permitted and banner advertisements shall not be permitted.



Poor design with canopy obscuring the main fascia.



A rigid traditional type canopy: the preferred style for Swords.



Curved Dutch canopies are to be avoided in Swords.

CANOPIES

Canopies can make a significant impact on the amenity of streets and consequently the following guidance should be adhered to:

- Canopies should be made from traditional canvas and located directly below the fascia.
- They should not extend across the pilasters or obscure architectural detail.
- The preferred canopy shall be the rigid traditional type.
- Frilled pelmets around the base of the canopy will not be accepted.
- Curved Dutch and balloon canopies are to be avoided as they are not traditional to Swords and provide minimal shading and shelter.
- The blind box shall be recessed and designed as part of the shopfront.

LIGHTING

Lighting should be considered as part of the overall shopfront design rather than as an ad-hoc addition. Subtle lighting can add vitality to the night-time street scene. However poorly designed light sources can detract from the overall appearance of the building during the day. Lighting sources should be concealed, and carefully directed at the main signage. Flood lighting and horizontal strip lighting is to be avoided.



Poor quality hanging/projecting elements at various levels, serves to detract from the appearance of this group of buildings.

OUTDOOR SEATING AREAS

To place tables and chairs outside a hotel, restaurant, public house or other establishment where food is sold for consumption on the premises, an application must be made to the Council for a Street Furniture Licence. The following guidance is recommended in relation to Outdoor Seating Areas:

- Outdoor seating areas should be located outside/clear of the lines of main pedestrian movement.
- All emergency exits and routes must be kept clear.
- When in use the outdoor seating area will need to be enclosed, to demarcate the area which contains the tables and chairs, thus making it distinguishable from other footpath users.
- Planters can be particularly attractive and can be used as part of the means of enclosure, but may need to be removed from the footpath outside of trading hours.
- The furniture should be of a high quality and uniform style within the area.
- Where umbrellas are used these must be fabric type (i.e. non reflective). Umbrellas are to be positioned so as to avoid overhanging outside the enclosure or impairing vehicle sightlines.



Planters used as a successful means of enclosure.



Outdoor seating area located clear of the main pedestrian footway.

Summary

The Urban Design Guidelines for the Main Street and North Street areas highlight characteristics and architectural features which make Swords Town Centre a unique and coherent place. In facilitating and encouraging the continued growth and success of the Town Centre into the future, it is important that the quality of the built environment and public realm is enhanced within a legible and sustainable context.



3.6 Public Realm Strategy

In recognition of the importance of the public realm to the vibrancy and quality of the Town Centre environment, Fingal County Council will prepare a public realm strategy for the Town Centre. The issues which will be addressed in this strategy will include inter alia the following:

- Provision of two public plazas at the Civic (Swords Castle) node and the proposed Cultural node at the southern end of Main Street.
- Enhancement of the 'Green' character of Main Street.
- Reduction in the width of the carriageway along Main Street and the widening of footpaths, thereby enhancing pedestrian connectivity and priority.
- Traffic management measures, including on-street parking arrangements.
- Removal of superfluous street clutter and providing for integrated street furniture, public lighting and signage strategy.
- Integration of hard and soft landscaping, with an appropriate maintenance regime [with particular regard to providing a palette of [1] appropriate tree species and [2] material palette for hard landscaping/surface materials within the Town Centre].
- Establishment of criteria for the provision and location of public art.

The criteria to be considered within the public realm strategy are discussed further in this document under Section 9.0. This public realm strategy for the Town Centre will be prepared in tandem with the development of the traffic management strategy for Swords Town Centre and shall be complete within 12-24 months of the preparation of the Masterplan.

TRAFFIC MANAGEMENT STRATEGY FOR SWORDS TOWN CENTRE

The public realm strategy for the Town Centre will be prepared in tandem with the development of the traffic management strategy for Swords Town Centre.

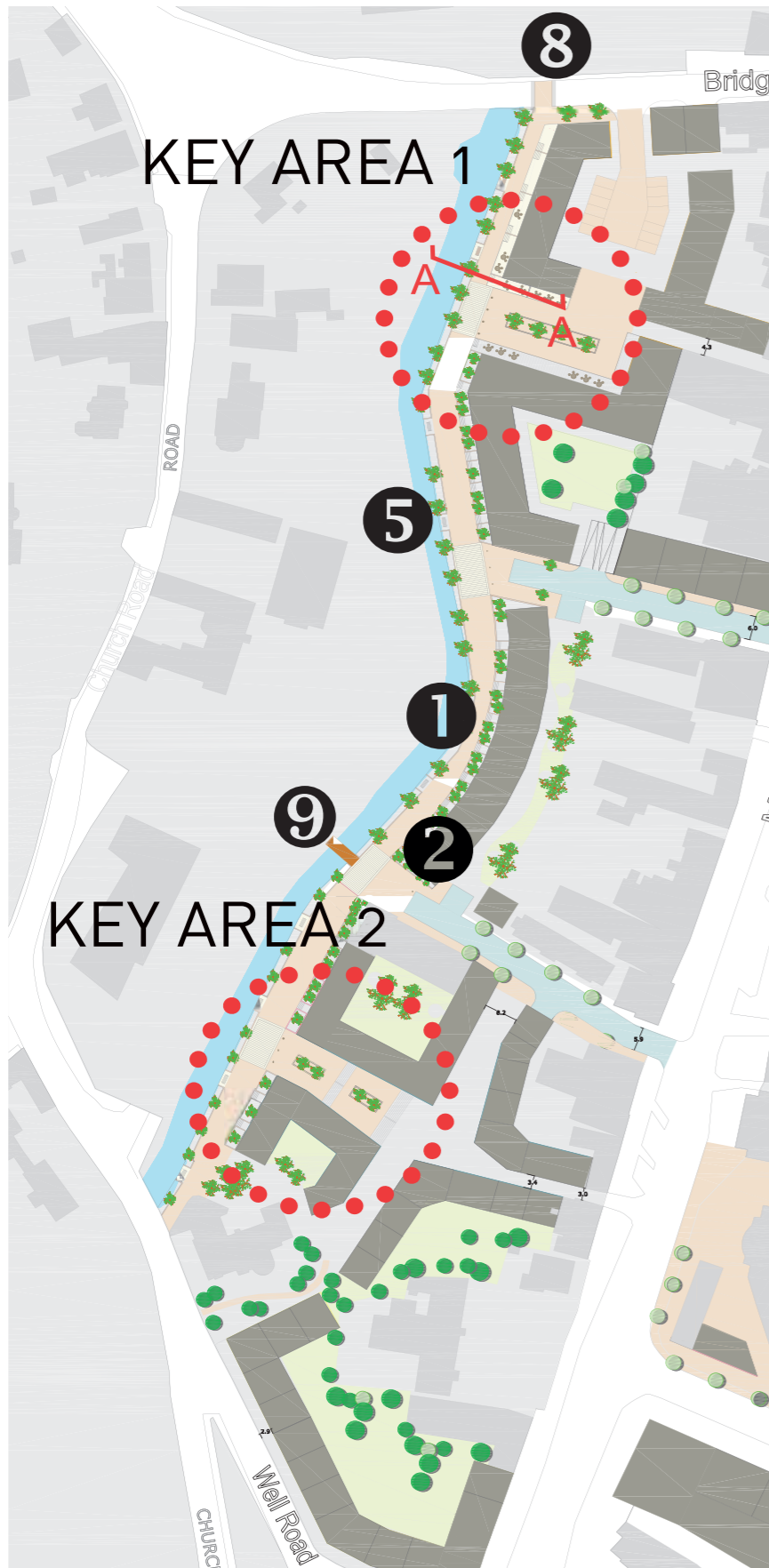
A key element in any traffic Future Traffic Management Strategy for the Town Centre will be determining appropriate on-street car parking arrangements within the Town Centre and deciding on suitable location/s for a multi-storey facility. There is a need to provide for the development of adequate levels of short term car parking facilities in the vicinity of the Main Street to support its continued role as the 'heart' of Swords Town Centre. Currently there is extensive on-street parking within the Town Centre as well as a number of surface car parks and one multi-storey facility. A number of locations [including Bridge Street, North Street and Forsters Way] will be examined by Fingal County Council in order to decide on the most suitable location/s for the development and delivery of a multi-storey facility/facilities within the Town Centre adequate to serve the growing short term parking demands within the Town Centre.

Bridge Street: A multi storey car park at Bridge Street could provide two levels below ground and three levels above. Access to the car park could be from the eastern edge of Bridge Street. The multi storey car park could be screened with commercial/office and residential development fronting onto Bridge Street, the River Ward and to the southern edge of the proposed development block. Apart from creating an attractive car park this would provide activity at street level and additional security. The height of the development should not over dominate that of the adjacent buildings and a suitable distance between the existing buildings should be allowed to protect residential amenity.

At Forsters Way: The provision of a multi storey car park at Forster Way could provide two levels below ground and four levels above. Access to the car park would be from New Street. The multi storey car park will be screened with commercial/office and residential development fronting onto New Street and Chapel Lane as well as the pedestrian lane leading into Oaklands Park.

At North Street: Providing an over ground multi storey car park at North Street could provide over 4 levels of car parking spaces. This would involve the redevelopment of 3 motor forecourt areas including Des Darcy Motors Ltd, Swords Auto and Castle Rock. Access to the car park would be from North Street and the development could be screened with commercial/office and residential development fronting onto North Street.

3.7 Riverside Walk

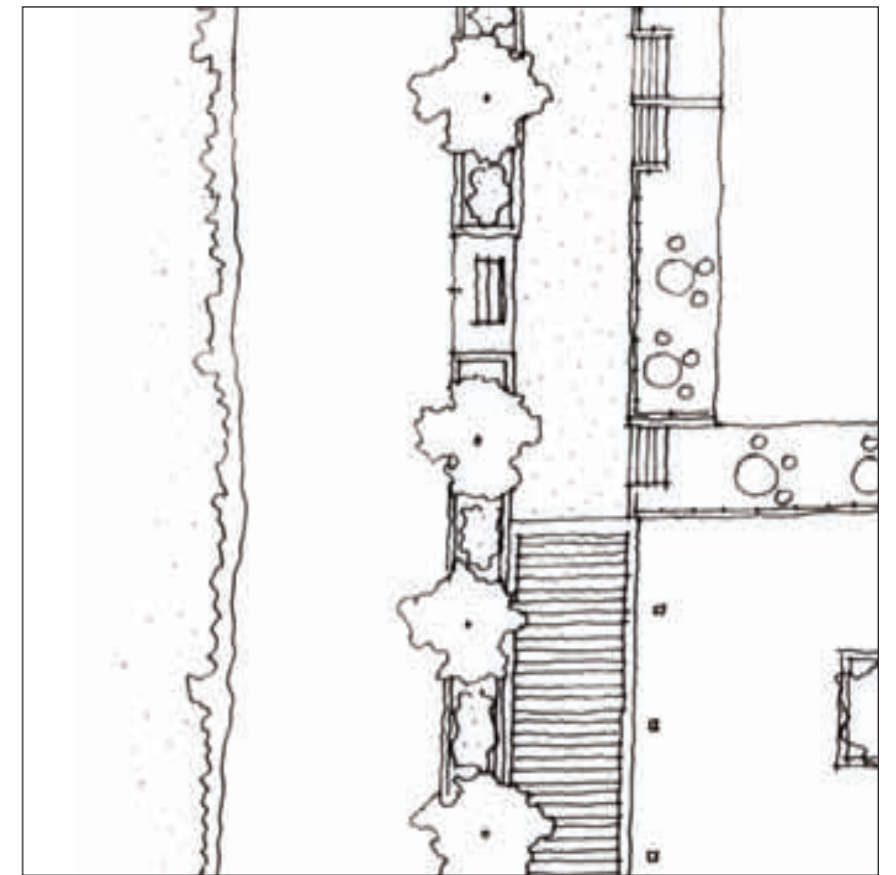
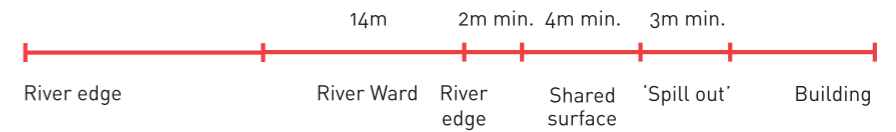
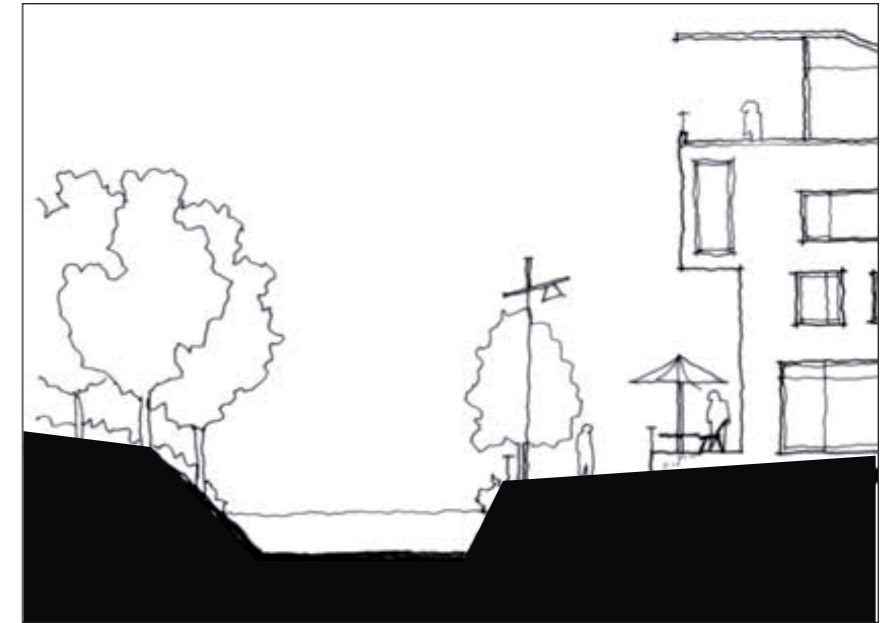


THE RIVERSIDE

The river Ward flows through the heart of Swords. One of the aims of the Vision for Swords is to establish the river as a key green corridor at the heart of the town. The proposed pedestrianised and riverside promenade will be lined by cafes, restaurants, shops and residences, maximising the west facing orientation and riverfront views, providing for a high level of informal surveillance and linking the valley to the south northwards to the Town Park and beyond. It is envisaged that the riverside promenade will be non vehicular except for emergency and service vehicle access.

Objectives:

- ❶ To create a strong, high quality riverside walk which provides a safe and pedestrian-friendly promenade with an uncluttered public realm including integrated street furniture.
- ❷ To create a mixed use street with attractive high quality façades overlooking the street and river.
- ❸ To provide emergency vehicular access within a shared surface carriageway.
- ❹ To provide a safe cycle route.
- ❺ To strengthen the existing wildlife habitat by maintaining and improving green embankments.
- ❻ To positively integrate flood defences as part of the public realm design.
- ❼ To create a vibrant and overlooked edge to the riverside and maximise the west facing aspect.
- ❽ To continue the riverside walk northwards at Bridge Street to the Town Park beyond.
- ❾ To improve accessibility east and west of the River Ward through the provision of a pedestrian bridge.



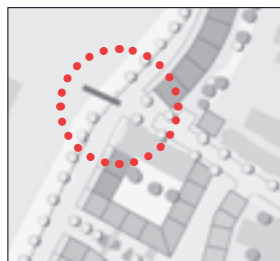
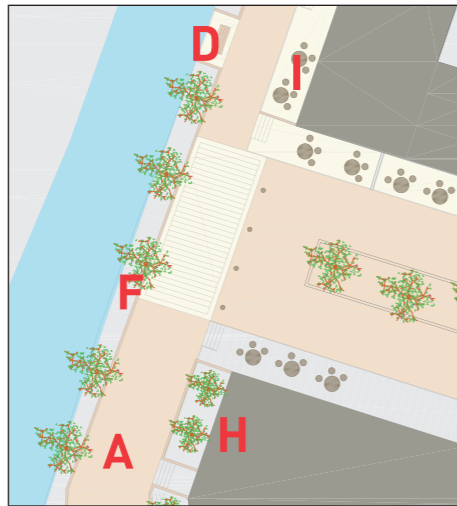
Indicative sketch section A-A and plan

Design Principles:

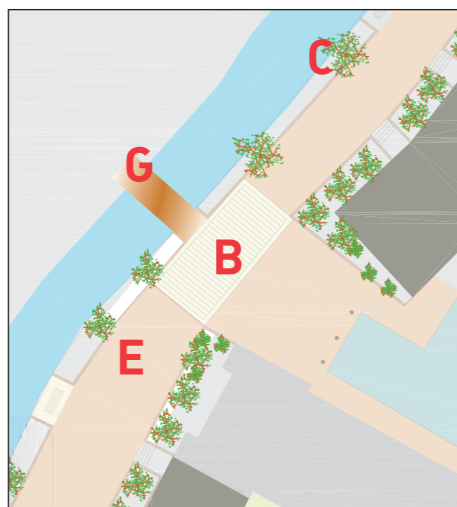
- A** Typical materials to create a robust and functional public realm to the riverside should include natural aggregate/hoggin/bound gravel with a robust good quality edging.
- B** Natural sandstone and timber decking in key public realm areas.
- C** Line of single species trees to strengthen the linear character of the waterfront and enhance wildlife habitats.
- D** Street furniture design to be complementary along the length of the waterfront.
- E** A minimum of 4m continuous shared surface between river's edge and adjacent buildings. Cycleway to be combined with footpath.
- F** Low level lighting to achieve safe standards while minimising disturbance to wildlife.
- G** Cross-river pedestrian and cycle access shall be improved through provision of a new bridge linking northwards towards the school. The bridge should provide an opportunity for high quality public realm design and public art. The bridge should be universally accessible and have a minimum width of 3m.
- H** All new development should be set back from the river edge by a minimum of 9m. Where possible this off set should be increased to strengthen the river corridor as a recreational and biodiversity resource.
- I** 'Spill out' seating area for cafés/restaurants overlooking the promenade and river Ward.



KEY AREA 1



KEY AREA 2

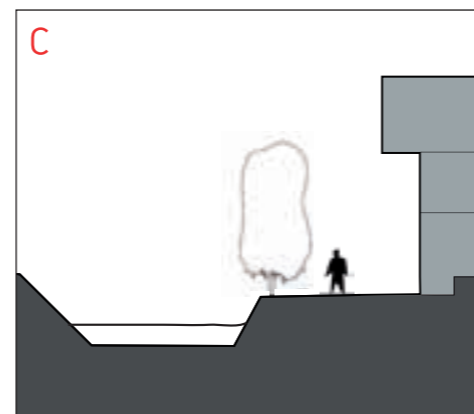
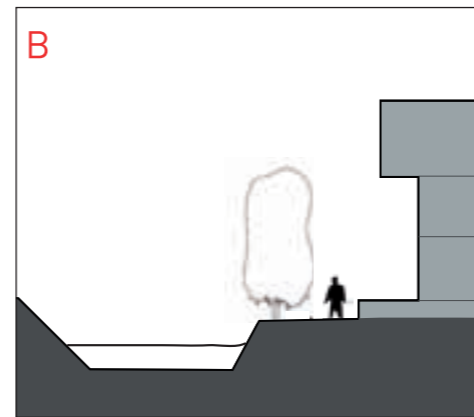
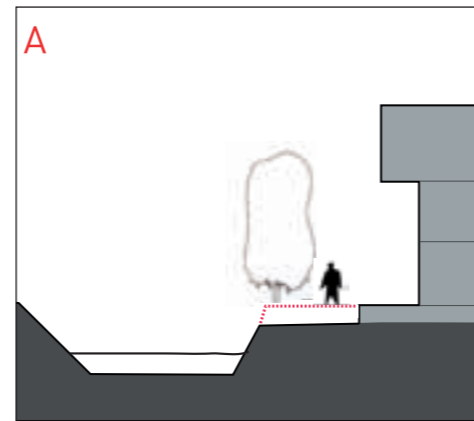


Flood Defence in more detail:

All proposed new development along the river Ward will need to manage flood risk. It is essential to the quality and character of the riverside that flood defences are designed as an integral part of the public realm and are to be treated as a design opportunity. The Department of the Environment, Heritage and Local Government as well as Fingal County Council shall be consulted at the outset of the design process to provide the necessary guidance on flood mitigation.

The diagrams below illustrate the three methods for mitigating flood risk along the river Ward. These are:

- A** Raise land levels up to the required threshold.
- B** Raise levels up to the required threshold around the perimeter of the building.
- C** Raise levels up to required threshold internally within the building



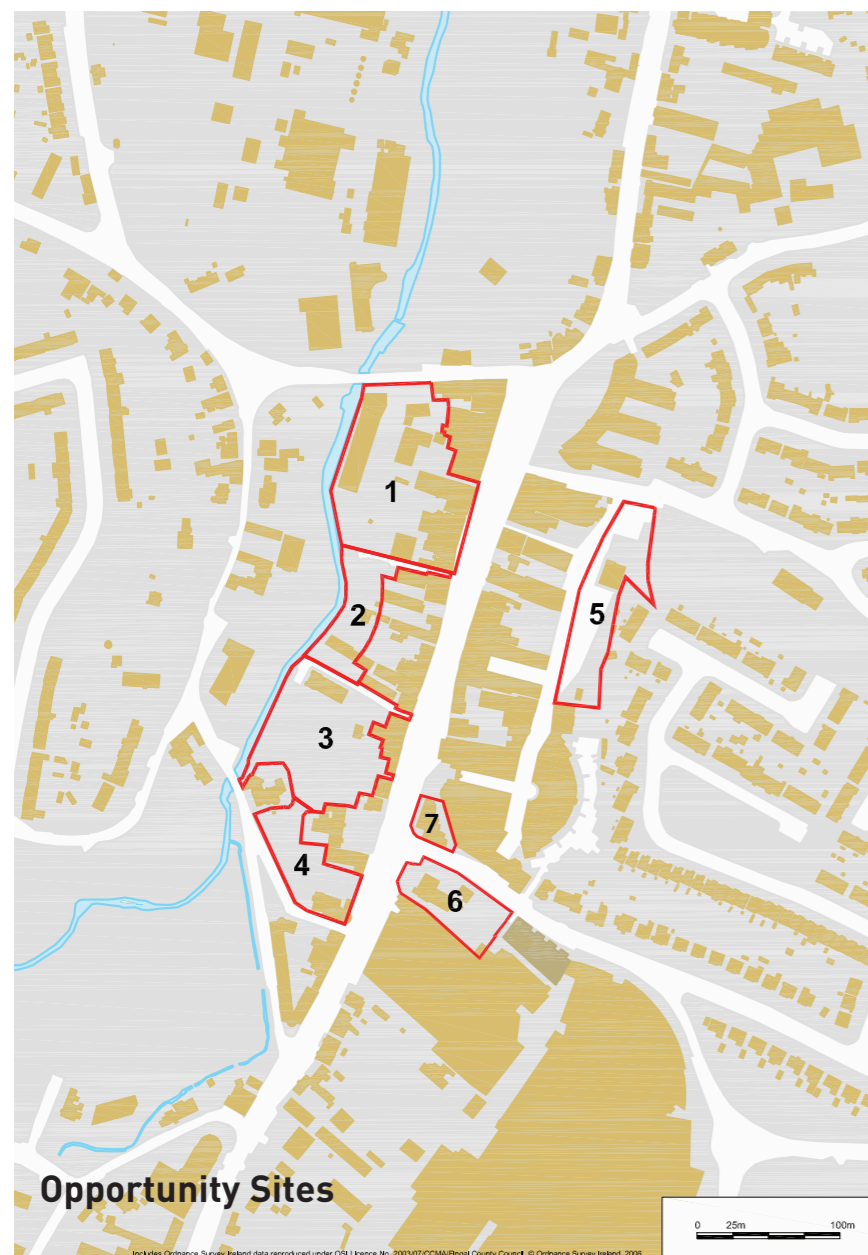
Indicative Character:



3.8 Opportunity Sites: Retail/Commercial Core

A number of opportunity sites have been identified within the retail/commercial core character area specifically. These sites represent significant opportunities for delivering environmental and economic benefits to Swords.

Indicative site development briefs have been prepared for these sites to guide their redevelopment thus ensuring that future development proposals adhere to the principles of good design described previously and contribute to the achievement of the overall vision. To allow for optimum integration with the existing Town Centre, piecemeal or ad hoc development will not be permitted within the development brief boundaries. The individual landowners will be encouraged to coordinate and integrate an approach for the redevelopment of their sites through use of development briefs. Indicative guidance is provided in the following sections 3.8.1 to 3.8.7

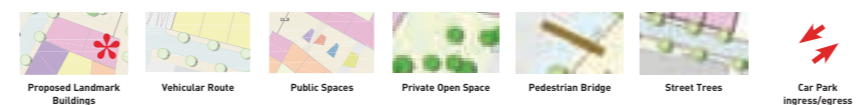


The heights described in this section of the document are the recommended maximum permissible heights. However each proposal will be required to carry out a visual impact assessment to ensure that the integrity of Main Street is not compromised.

It will be a key principle that new development will positively front onto the public space network (roads and river), so that they contribute to the activity and thus the safety of these spaces and routes. Also the development of good connections will be sought by the creation of links between sites and surrounding areas.

Development proposals must demonstrate compliance with:

- County Development Plan standards relating to private, semiprivate and public open spaces, car parking and separation distances.
- Relevant Standards relating to daylight, sunlight and overshadowing.

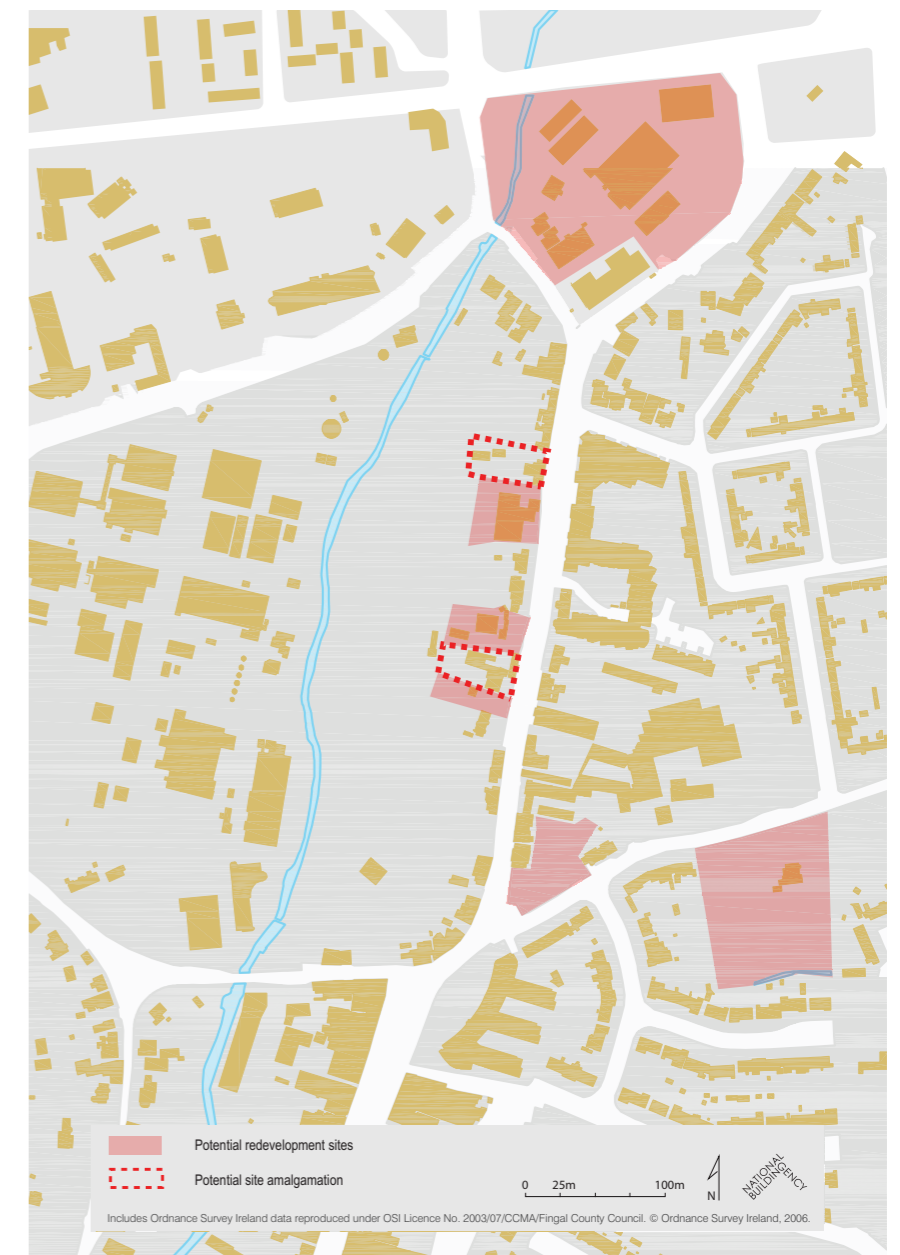


OTHER KEY IMPORTANT SITES

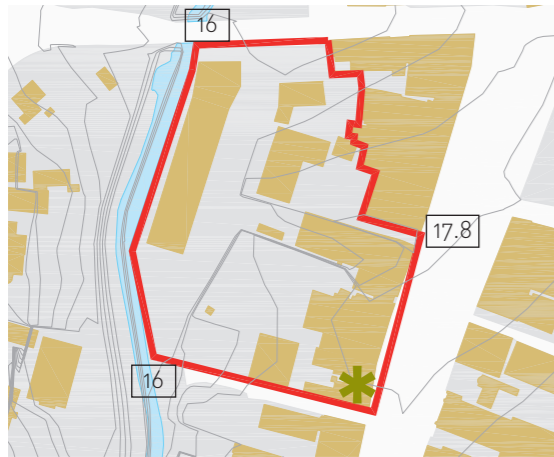
It is an objective of Fingal County Council to ensure that specific site design and development briefs shall be prepared for potential redevelopment sites identified in the map adjacent prior to considering any future proposals including planning applications. These shall clearly present an analysis and vision for how these areas might be developed in an integrated manner to provide for additional facilities to serve the Town Centre in a manner that will enhance and improve the civic and environmental quality as well as the vibrancy and vitality of the town.

The sites include the following:

1. The Council car park at Seatown Road
2. Lands south of Seatown Road
3. Garage/forecourt areas along North Street
4. North Street Business Park, Seat garage, Scouts hall and FÁS premises



3.8.1 OPPORTUNITY SITE 1



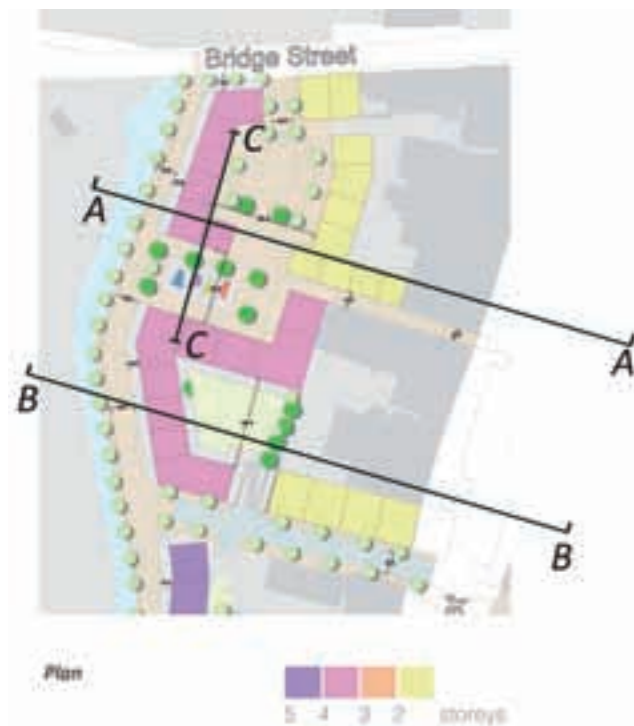
- * McNamaras Pharmacy
- 16 Level in metres AOD

Analysis

This site comprises of a two storey shopping centre with associated car parking. The site abuts the eastern banks of the Ward River. The river has a relatively low level of passive surveillance. Opportunities exist to create a more pleasant pedestrian/cycle route along the river with future development providing a strong frontage overlooking the river. To allow for sufficient and safe vehicular access to the site the removal of one building (either McNamaras Pharmacy or Vodafone) would be necessary. This use can easily be accommodated in the new site.

Vision

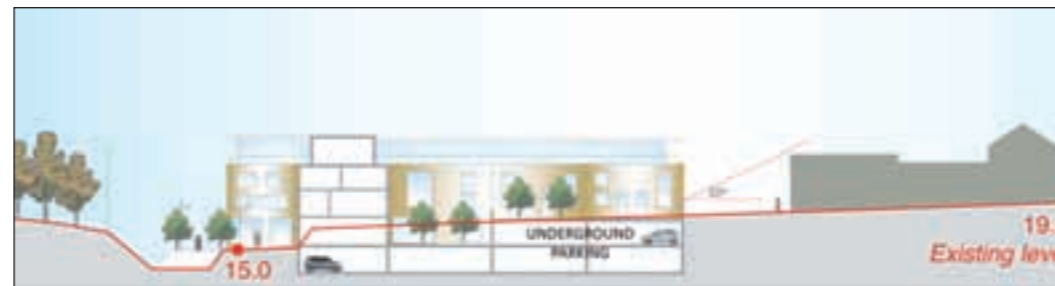
To allow the density of the town core to increase, whilst maintaining the urban grain and avoiding excessively large monolithic development.



Criteria	Specification
Gross area	0.28ha
Max total residential development potential	6080m ² (72 units approx: 20% 1 bed/ 65% 2 bed/ 15% 3 bed)
Min non-residential development	1270m ² : retail/commercial at ground floor levels
Max perimeter building height	4 storeys (13 metres approx.)
Max landmark building height	n/a
Min local public open space	800m ²
Edge definition of public realm	Street tree planting to river walk
Min carriageway width	6.0 metres (access road to site from Bridge Street and at River Mall)
Min pavement width	2.0 metres
Min threshold zone	3.2 metres on river walk
Parking	Under ground parking of approximately 150 spaces utilising the sites contours. Access ramping from Bridge Street. Surface car parking for approximately 12 spaces.
Access	Vehicular access to the site will be taken from Bridge Street and also at River Mall. A new east-west pedestrian way is proposed from the Main Street.
Unit Sizes	8-10 metre wide frontages



Section AA



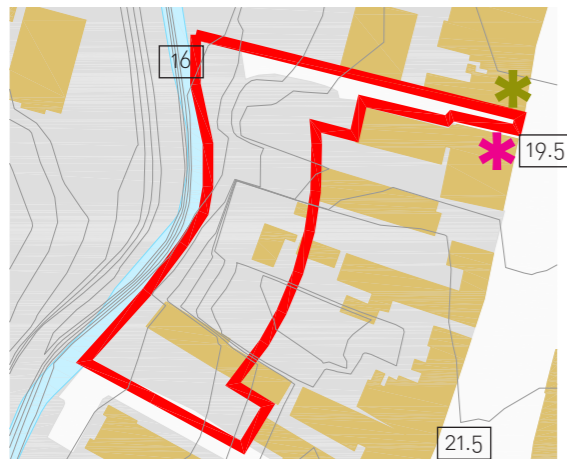
Section BB showing the 25 degree rule for effective daylight transmittance to proposed residential



Section CC showing the 25 degree rule for effective daylight transmittance



3.8.2 OPPORTUNITY SITE 2



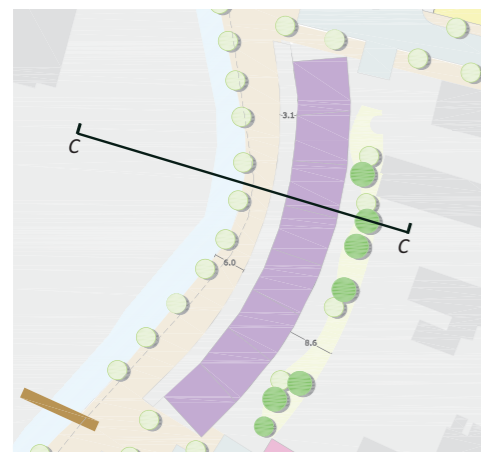
- McNamaras Pharmacy
- Vodafone
- Level in metres AOD

Analysis

This site comprises rough land, to the rear of the buildings on Main Street. It abuts the River Ward to the west. The river, with no buildings overlooking it, has a relatively low level of passive surveillance. Opportunities exist to improve the footpath links as part of the development of the site along the eastern banks of the river, all of which would help to secure the integration of the river into the network of public open space and movement in the area. To allow for sufficient and safe vehicular access to the proposed new development, the removal of one building (either McNamaras Pharmacy or Vodafone) would be necessary to facilitate access. Egress movement will be via Colmcille Court, which will require the removal of one building (either Mizzone's or Shane Redmond Residential). The removed uses can easily be accommodated within the new redevelopment proposals.

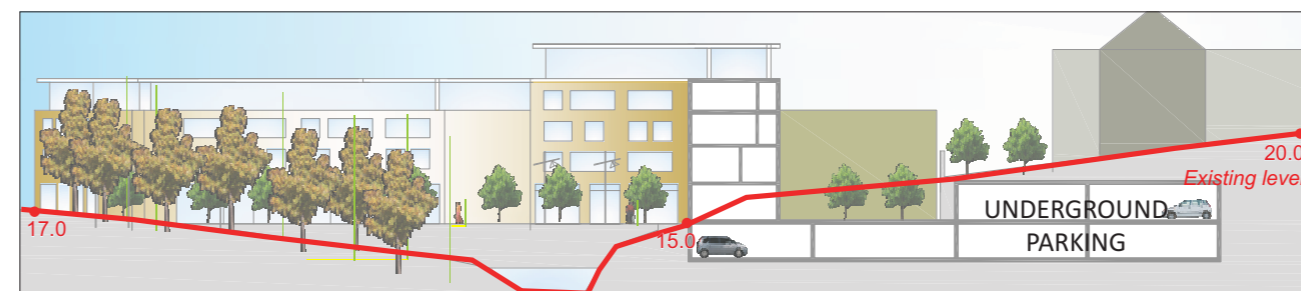
Vision

Develop to the rear of Main Street, to protect the backs of existing development and to provide overlooking to river.

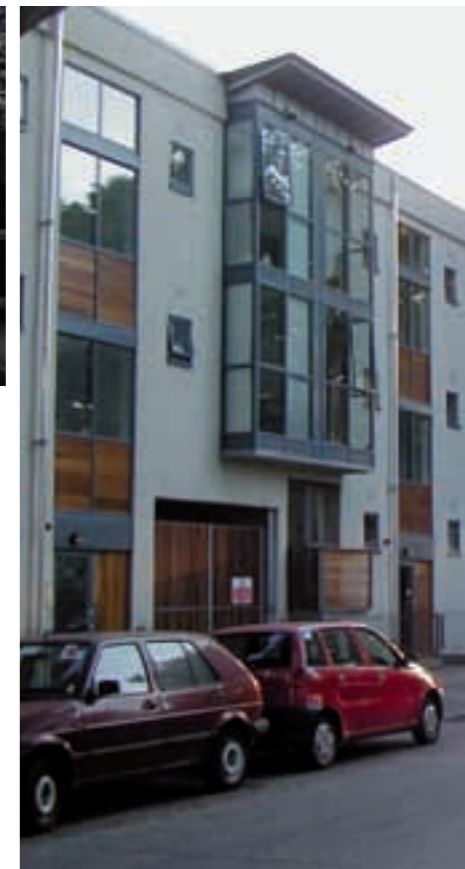


Plan storeys

Criteria	Specification
Gross area	0.2ha
Max total residential development potential	2904m ² (33 units approx: 20% 1 bed/ 65% 2 bed/ 15% 3 bed)
Min non-residential development	726m ² : retail/commercial at ground floor levels
Max perimeter building height	5 storeys (16 metres approx.)
Min local public open space	500m ²
Edge definition of public realm	Street tree planting to river walk.
Min carriageway width	6.0 metres (At River Mall and Colmcille Court)
Min pavement width	2.0 metres
Min threshold zone to buildings	2.6 metres on river walk
Parking	Under ground parking of approximately 66 spaces over two storeys utilising the sites contours.
Access	Entry to the underground carpark will take place from River Mall and exit from the underground through to Colmcille Court. Pedestrian access will be from the existing east west pedestrian ways from the Main Street and along the river bank.
Unit Sizes	8-10 metre wide frontages. All residential units to be dual aspect.

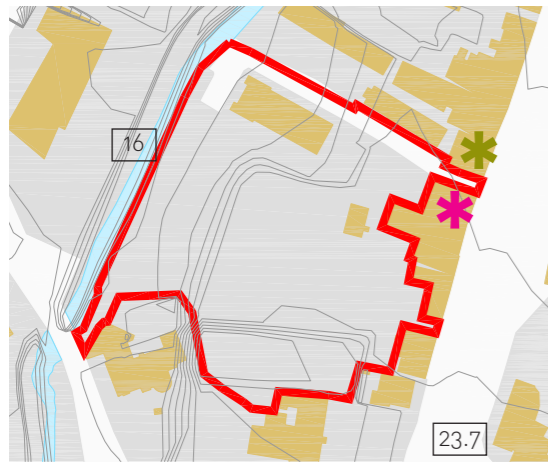




Section CC



Building fronting onto the public realm

3.8.3 OPPORTUNITY SITE 3



-  Mizzoni's Take Away
-  Shane Redmond Residential
- 16 Level in metres AOD

Analysis

This site comprises of underutilised scrub land, to the rear of the buildings on the Main Street. The site is currently steeply terrained with some established mature tree stands. To allow for sufficient and safe Vehicular access to the site the removal of one building (either Mizzoni's take away or Shane Redmond Residential) would be necessary. This use can easily be accommodated in the new site. To allow for maximum development potential of the site, the existing surface car park will need to be developed upon. The loss of these car parking spaces will be replaced as part of the future underground parking proposal.

Opportunity Site Brief

Vision

To allow new frontage development onto the River Ward. To create an enhanced and inviting pedestrian only public route between the new development, gently stepping down with a positive frontage overlooking the pedestrian route.



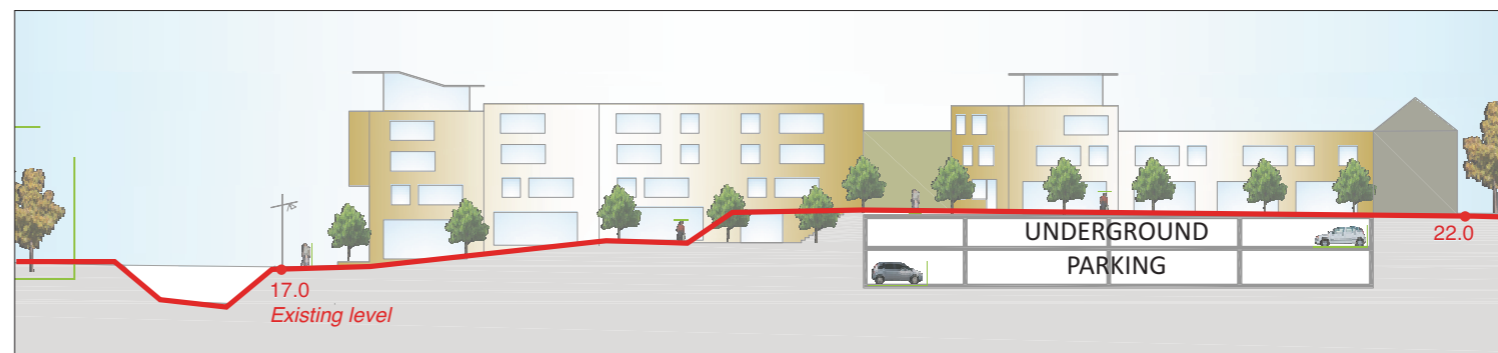
Plan

5 4 3 2 storeys

Criteria	Specification
Gross area	0.55ha
Max total residential development potential	3650m ² (54 units approx: 20% 1 bed/ 65% 2 bed/ 15% 3 bed)
Min non-residential development	1080m ² : retail/commercial at ground floor levels
Max perimeter building height	5 storeys (16 metres approx.)
Min local public open space	1400m ²
Edge definition of public realm	Street tree planting to river walk. Public realm meets building line in internal pedestrianised area.
Min carriageway width	6.0 metres (At Colmcille's Court)
Min pavement width	2.0 metres
Min threshold zone to buildings	2.6 metres on river walk
Parking	Under ground parking of approximately 112 spaces over two storeys utilising the sites contours.
Access	Vehicular access to the site will be taken from St Colmcille's Court. Pedestrian access will be from a new east west pedestrian way from the Main Street to the River Ward.
Unit Sizes	8-10 metre wide frontages. All residential units to be dual aspect.

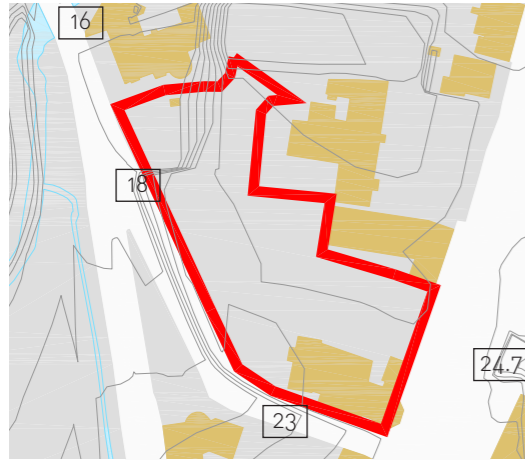


Riverfront development



Section DD

3.8.4 OPPORTUNITY SITE 4



16 Level in metres AOD

Analysis

This site sits strategically on the junction of Well Road and Dublin Street. Development along the Main Street is primarily two storeys whilst development southwards along Church Road is primarily four storey in nature. Any development on this key opportunity site opposite should address the existing built form positively, sensitively knitting the various heights together. The site is located in between two protected structures: The Old Borough School and The Old School House Restaurant.

Opportunity Site Brief

Vision

To accentuate the corner with a strong building. This will also mark the southern gateway to the Town Centre retail/commercial core.



Plan

5 4 3 2 storeys

Criteria	Specification
Gross area	0.27ha
Max total residential development potential	2190m ² (25 units approx: 20% 1 bed/ 65% 2 bed/ 15% 3 bed)
Min non-residential development	600m ² : retail/commercial at ground floor levels
Max perimeter building height	2/3 storeys (7/10 metres approx)
Max landmark building height	4 storeys (13 metres approx.)
Min local public open space	1045m ²
Edge definition of public realm	Street tree planting to river walk. Public realm meets building line along Well Road
Min carriageway width	n/a
Min pavement width	3.0 metres
Min threshold zone to buildings	Public realm meets building line
Parking	Under ground parking of approximately 53 spaces utilising the site's contours.
Access	Vehicular access to the site will be taken from Well Road.
Unit Sizes	8-10 metre wide frontages. All residential units to be dual aspect.

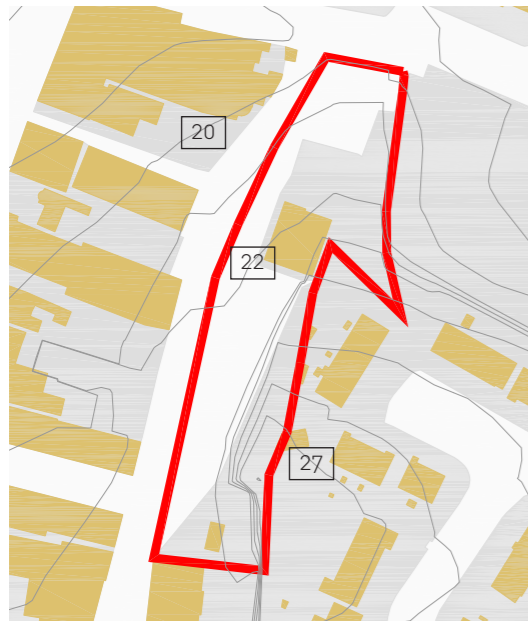


Filling out the building line



Elevation EE

3.8.5 OPPORTUNITY SITE 5



16 Level in metres AOD

Analysis

This area consists of a public surface car park and a two storey building housing the senior citizens club. The site is bounded to the east by St Colmcille's Church and Oaklands Park and New Street to the west. Intensification of use may be realised by developing a frontage along its west and northern boundaries thus bringing an increased level of surveillance to the existing area.

Opportunity Site Brief

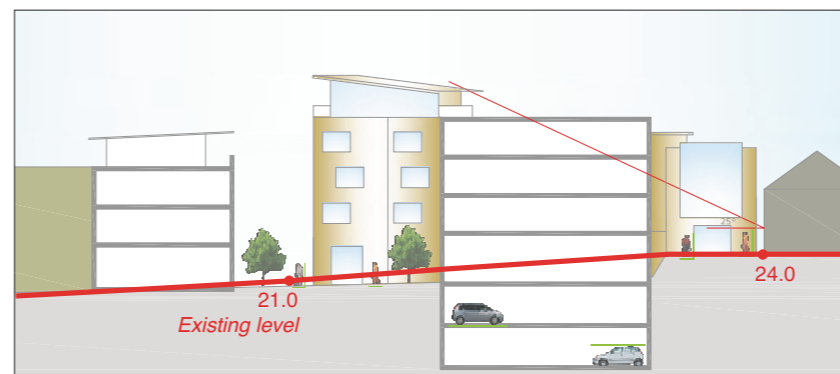
Vision

To allow for the redevelopment of the site with a positive frontage to the street whilst maintaining the amenity of Oaklands Park behind. Retain and enhance existing view of St Columcille's Church by the creation of a civic plaza to the northern edge of the site.

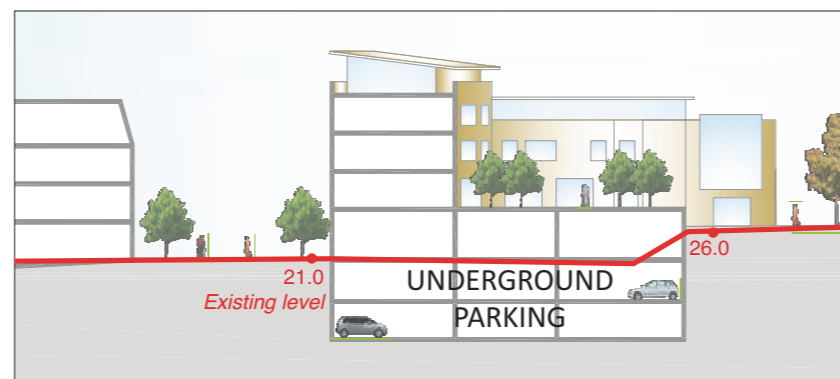


Plan
5 4 3 2 storeys

Criteria	Specification
Gross area	0.43ha
Max total residential development potential	7000m ² (82 units approx: 20% 1 bed/ 65% 2 bed/ 15% 3 bed)
Min non-residential development	3060m ² : retail/commercial at ground floor and first floor levels
Max perimeter building height	5 storeys (16 metres approx.)
Max landmark building height	n/a
Min local public open space	900m ²
Edge definition of public realm	Street tree planting
Min carriageway width	n/a
Min pavement width	2.0 metres
Min threshold zone to buildings	Public realm meets building line
Parking	Parking would be provided as an at basement levels, delivering maximum surveillance and frontage along New Street. Under ground parking over two basement levels would accommodate approximately 200 spaces.
Access	Vehicular access to the site will be taken from New Street. Entrances to serve the residential units over should be taken direct from the street.
Unit Sizes	8-10 metre wide frontages. All residential units to be dual aspect.



Section EE showing the 25 degree rule for effective daylight transmittance to existing residential

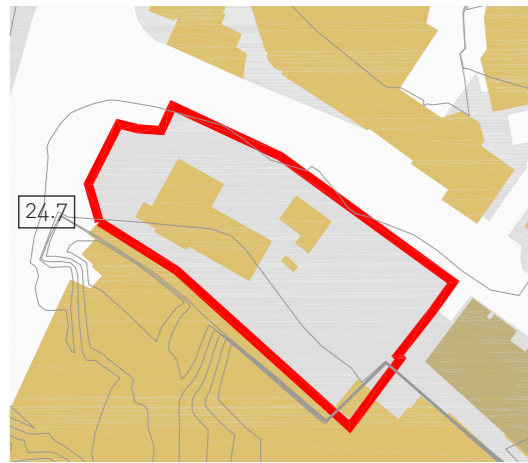


Section FF



High quality public realm

3.8.6 OPPORTUNITY SITE 6



16 Level in metres AOD

Analysis

This site comprises of an existing single storey Garda Station. Uses proposed range from retail/commercial at the lower floors with residential immediately above.

Opportunity Site Brief

Vision

To allow the density of the Town Centre to increase at this key corner location.

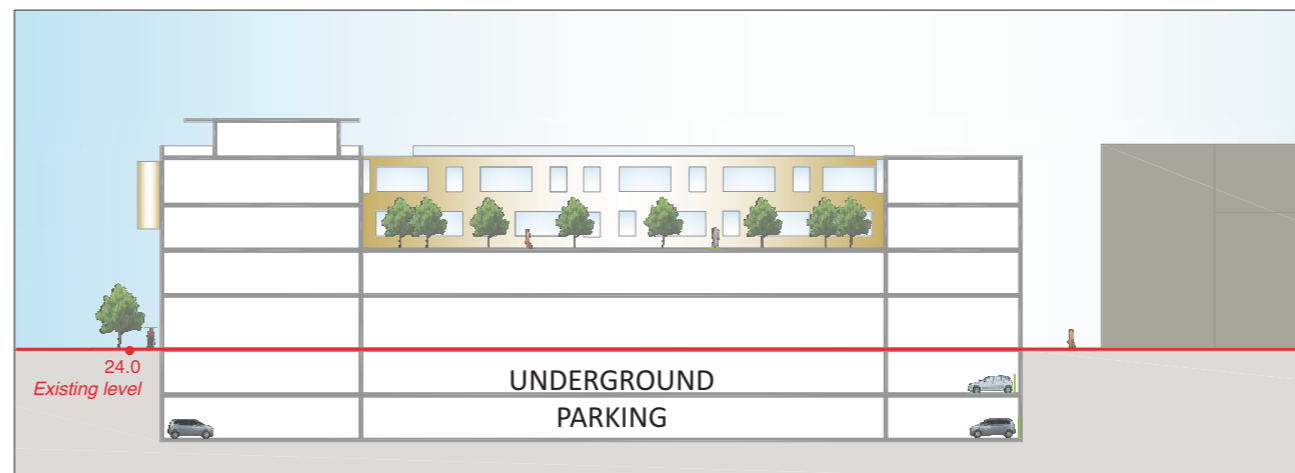


Plan
5 4 3 2 storeys

Criteria	Specification
Gross area	0.21ha
Max residential development potential	2900m ² (33 units approx: 20% 1 bed/ 65% 2 bed/ 15% 3 bed)
Min non-residential development	3500m ² : retail/commercial at ground floor and first floor levels
Max perimeter building height	4 storeys (13 metres approx.)
Max landmark building height	5 storeys (16 metres approx.)
Min local public open space	n/a
Edge definition of public realm	Street tree planting
Min carriageway width	n/a
Min pavement width	2.5 metres
Min threshold zone to buildings	Public realm meets building line
Parking	Under ground parking over two levels consisting of approximately 110 spaces.
Access	Vehicular access to the site will be taken from the Malahide Road.
Unit Sizes	8-10 metre wide frontages. All residential units to be dual aspect.



Examples of tight infill developments



Section GG

3.8.7 OPPORTUNITY SITE 7



16 Level in metres AOD

Analysis

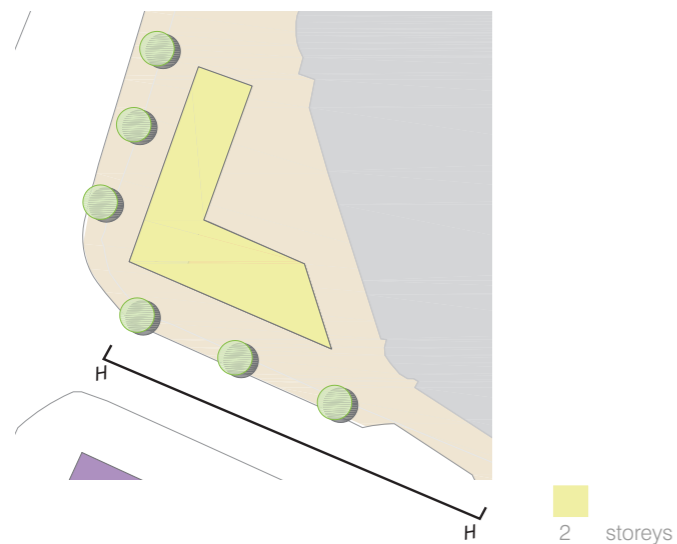
This site comprises of an existing two storey terrace and a single storey building fronting onto the Malahide Road. Uses proposed range from retail at ground floor levels to ensure an active frontage and commercial to the upper floors.

Opportunity Site Brief

Vision

This is a key corner site within the town. Any proposed redevelopment must ensure that it is “designed in the round” as all elevations will be visible to the public. Any redevelopment proposal must include all of the subject lands and provide for a positive relationship to:

- Malahide Road
- Main Street
- the pedestrian walkway to the east
- adjoining development to the east



Criteria	Specification
Gross area	0.03ha
Max residential development potential	n/a
Min non-residential development	460m ² : retail/commercial at ground floor and first floor levels
Max perimeter building height	2 storeys (7 metres approx.)
Max landmark building height	n/a
Min local public open space	n/a
Edge definition of public realm	Street tree planting
Min carriageway width	n/a
Min pavement width	2.5 metres
Min threshold zone to buildings	Public realm meets building line. Ensure active frontage to rear pedestrian laneway.
Parking	n/a
Access	n/a
Unit Sizes	8-10 metre wide frontages. All residential units to be dual aspect.



Existing buildings



An indicative example of the proposed scale of buildings



Elevation HH

4.0 Civic Quarter

4.1 Character

The Civic Quarter is defined as the area at the junction of Main Street/North Street where the main civic uses are located including the Fingal County Council offices, the Courthouse, and Swords Castle, which was the historic governing centre of the Town.

There is an opportunity to develop and consolidate this area as an expanded Civic Quarter and thereby strengthen the attraction of the northern end of Main Street, through the development of the Council owned surface car park at the junction of North Street and Seatown Road (*possible future uses for the Council car park could include a new Garda station and Courthouse*) and the development of a major civic space in front of Swords Castle.

4.2 Connectivity

The proposed Metro Stop at Seatown will be the main stop directly serving the Civic Quarter, further enhancing its accessibility and hence attractiveness. There may be an opportunity to develop the Council owned surface car park at the junction of the R132, as well as a number of key sites along Seatown Lane to strengthen this route from the Metro Stop back into the Civic Quarter and the Main Street.



4.3 Design

Key to the enhancement and attractiveness of this Civic Quarter area, is the development of a Public Realm Strategy for the Town Centre, which will include the design of the public realm at the junction of Main Street/Bridge Street/North Street incorporating the area in front of Swords Castle, the entrance to Main Street and the area adjoining Fingal County Council car park. The design of this public space at the Civic Quarter, including the increased visibility and enhancement of the Castle, is important to the success of this area.

The proposed Public Realm Strategy for the Town Centre will address a number of criteria including inter alia the following:

- Provision of two public plazas at the Civic (Swords Castle) node and the proposed Cultural node at the southern end of Main Street.
- Enhancement of the 'Green' character of Main Street.
- Reduction in the width of the carriageway along Main Street and the widening of footpaths, thereby enhancing pedestrian connectivity and priority.
- Traffic management measures, including on-street parking arrangements.
- Removal of superfluous street clutter and providing for integrated street furniture, public lighting and signage strategy.
- Integration of hard and soft landscaping, with an appropriate maintenance regime (with particular regard to providing a palette of [1] appropriate tree species and [2] material palette for hard landscaping/surface materials within the Town Centre).
- Establishment of criteria for the provision and location of public art.

The criteria to be considered within the Public Realm Strategy are discussed further in this Masterplan under Section 9.0. The strategy will be prepared in tandem with the development of the Traffic Management Strategy for Swords Town Centre.

In recognition of the importance of the public realm to the vibrancy and quality of the Town Centre environment, including the success of the Civic Quarter, the public realm strategy shall be complete within 12-24 months of the preparation of the Masterplan.





5.0 Cultural Quarter

5.1 Introduction

Swords is at present poorly represented in terms of its cultural attractions, a concern widely expressed during the Council's public consultation regarding Swords. In 2002, an audit of community culture and sport facilities was carried out by Fingal County Council Community Development Department, the results of which clearly highlighted the poor level of cultural facilities and services serving Swords. Towns of similar size and population were generally found to be well served by theatres, performing arts venues, high quality modern libraries, etc. This deficit needs to be redressed, in particular in the context of Swords emerging population of 100,000 people.

Having recognised the unambiguous and urgent need to improve the provision of cultural facilities in Swords, the Masterplan seeks to determine how and where such facilities should be located. To this end it was noted that in order to ensure the continued vitality and important role of Main Street, attractors at both ends of the Street would be required. Main Street, at its northern end enjoys the benefits and draw of the Council Offices, Swords Castle and associated Park and the proposed Civic Quarter. At present there is no obvious and successful public amenity focal point at its southern end. While the Penny's store and pedestrian link through to the Pavilions Shopping Centre is located at the southern end of Main Street and is an attractor in its own right, it is solely retail based. The existence of the Ward River Valley Park, located at the southern end of Main Street, and the opportunities and amenities it offers have not been utilised, as the Valley and Main Street have not been integrated.

To this end it is proposed to extend Main Street southwards with a major public space surrounded by new cultural facilities to form a Cultural Quarter serving and expanding Swords population and visitors to the Town.

The Cultural Quarter will include culturally focused uses, such as a town library, theatre, galleries, and arts centre. This area will accommodate a direct pedestrian connection through the Pavilions site to the Metro Quarter and links into the Ward River Valley Park. A public square will be the central focus and setting to cultural buildings and the scale of the public space will be capable of supporting outdoor performance, outdoor dining, performance art, promenade as well as meeting/seating spaces.

Urban design guidance and objectives are put forward within this Masterplan, which will address the key issues of movement, public realm, height and massing, and landscaping (see map Cultural Quarter). The Cultural Quarter will be designed and developed on a phased basis in tandem with proposed commercial and mixed use development east and west of the Dublin Road. The key elements in sections 5.2-5.6 establish the principles which will influence a future detailed design for this area. Additional public consultation will form part of future development consents in this area.



5.2 Movement Strategy

The Cultural Quarter will integrate fully with the existing Town Centre and the proposed Town Centre expansion areas. Its location on Main Street/Dublin Road ensures that it is readily accessible from all areas of Swords.

The nearest Metro Stop will be Swords Stop, at a distance of 430 metres (approximately 5 minutes walk) away. Bus links will also be available with stops located along the Main Street/Dublin Road as well as at the Metro Quarter.

To the east, there will be a direct 24 hour pedestrian link from the Cultural Quarter via "Central Street" (see Pavilions section 6.0) to the Metro Quarter. Central Street is an inclined street, accessed at grade from the Cultural Quarter.

To the south of the area lies "South Street", a 24 hour residential street. Although this street does not border the cultural square itself there will be a direct link to the square via a partly enclosed arcade.

To the west of the cultural square is the Main Street/Dublin Road. A significant pedestrian crossing will be located at this point to allow direct access to the Ward River Valley Park. While the vehicular through-traffic along this road needs to be maintained as part of the vehicular street network serving the Town Centre, it is envisaged that the number of vehicles using this street will diminish in accordance with transportation network improvements around Swords in the medium to long term.

New connections to the Ward River Valley Park will be created on the western side of Main Street/Dublin Road. This will be by means of a stepped landscape strategy and/or through a number of buildings located on this side of the road.

Movement Strategy – Key Requirements

- Adequate pedestrian crossing facilities shall be provided between the Cultural Square and the Ward River Valley Park.
- Central and South Street shall be constructed at a 1:20 gradient and no ramps will be built across facades addressing the Cultural Square.
- The provision of the Central Street connection to the Cultural Quarter shall be a level (at grade) connection.
- Ramp access at a gradient of less than 1:20 will be provided to the Ward River Valley Park.

5.3 Public Realm

The design of the public realm will be in keeping with the Cultural Quarter's status as a major cultural space on a regional level and as one of the three major activity nodes within the Town Centre Masterplan. It will be integrated with the Pavilions development and will support a mixture of cultural and retail services within the setting of a focal urban space.

Contained within the Cultural Quarter will be one major public place (Cultural Square) on the east side of the Dublin Road and one minor public space (Ward River Place) on the west side of the Dublin Road. Although these two spaces are divided by the road, the intention is that they will read as an integrated place at the heart of the Cultural Quarter. They will have similar treatments in terms of landscape and materials and will be dealt with as one element for the purpose of this section.

CHARACTER

- The Cultural Quarter will be a 24 hour major meeting and event space located at the meeting point of Main Street and Central Street and overlooking the Ward River Valley Park.
- The area will be defined by its uses which will be predominately cultural and it will host minor events such as street theatre, markets etc.
- The Cultural Square will be a space of approximately 800m²-1000m², leading into Ward River Place, which will comprise landscaping elements interwoven with well-designed buildings stepping down into the park.
- The main spaces will all focus on the view down the Ward River Valley Park.
- A Landscape and Recreation Strategy for the Ward River Valley Park is required to facilitate its planned extension westwards, to improve passive supervision, accessibility and permeability of the park and to develop additional passive and active recreational facilities and amenities within the Ward River Valley Park.

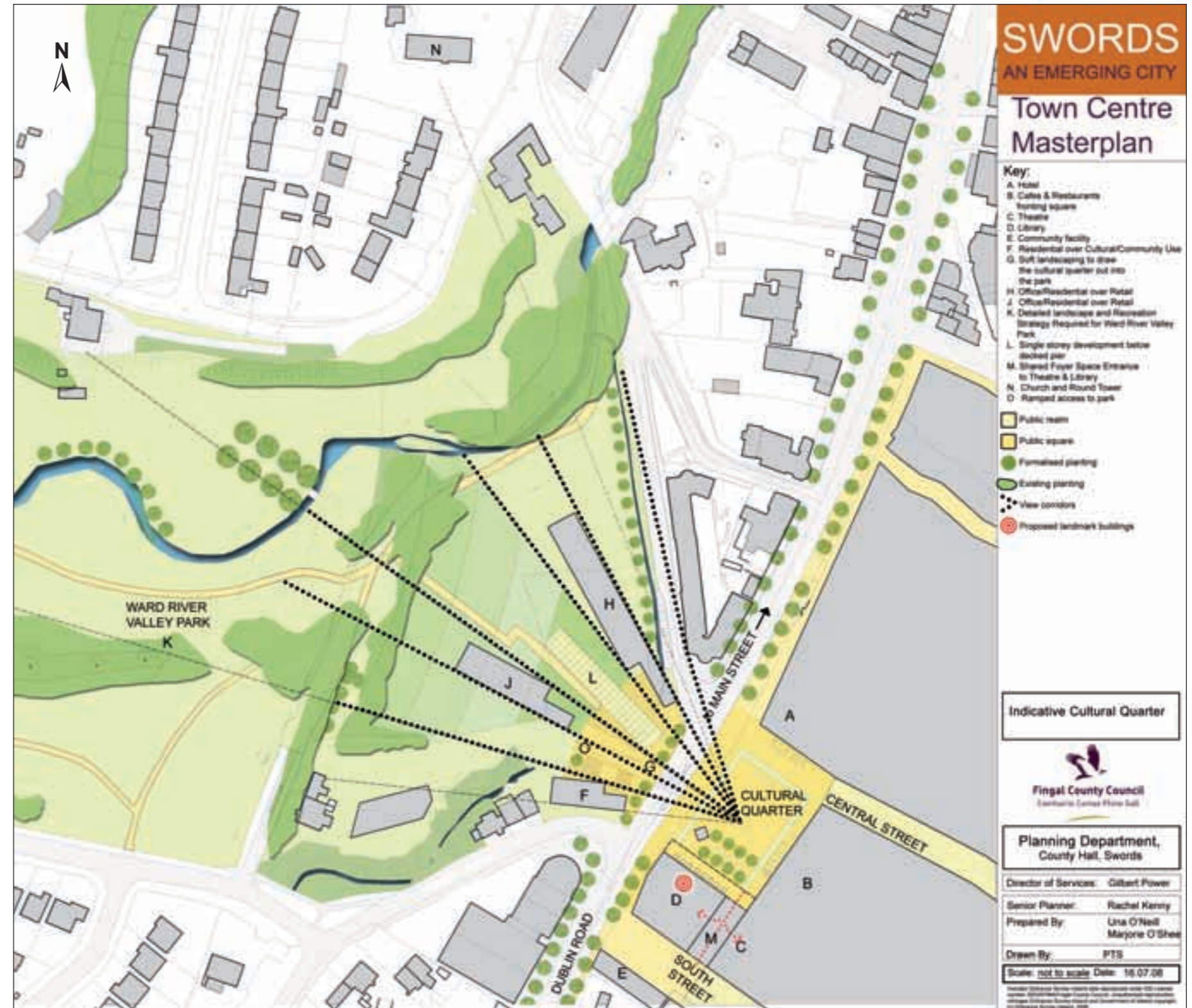
DESIGN

- In terms of urban quality, this space shall comprise of quality public seating, lighting and surface finishes.
- The area will have outdoor dining and meeting areas.
- The landscaping will graduate from hard to soft as the transition is made between the Pavilions development area and the Ward River Valley Park.
- Ward River Place will provide a series of vistas into the park culminating in areas of tree planting or other natural elements.

Public Realm – Key Requirements

- Buildings and landscaping will be positioned in such a way as to set up views and access into the Ward River Valley Park and views of St. Columbas Church and Round Tower. Landscaping elements will contribute to this view.
- Kiosks or other installations may be part of the overall design and will not obstruct views.
- Surface treatment on the Dublin Road will ensure a safe continuity of place between the Cultural Square and the Ward River Place.

The detailed design of the Cultural Square and Ward River Place will require further analysis. The indicative layout provided (*see map Indicative Cultural Quarter*) is one possible development scenario that embraces the overall vision for the Cultural Quarter. Any alternative design, which may be pursued, will be required to comply with the Council's Vision.



5.4 Land Use

The Cultural Quarter will accommodate a major civic space, cultural and recreational facilities including a theatre/arts centre, town library, community resource centre, art gallery and an exhibition venue extending into the landscape. Other elements may include a hotel with conferencing facilities and commercial and public facilities on the extended park fringe including uses such as a fitness centre/gym.

RETAIL

An anchor retail unit will adjoin the cultural square, providing active frontage onto this public square, and act as a draw to the area. The public square will be the main social and cultural space serving Swords Town Centre and will be capable of supporting outdoor performances, outdoor dining, artwork and meeting/seating spaces. It is imagined that a calendar of events would allow for markets, festivals etc.

Non retail services (including cafes, sandwich bars etc) may be located around the square to compliment the main cultural uses. These may be in the form of small kiosks within the square itself or on the park side of the Dublin Road. Any proposed Kiosk will form part of the overall design of the space and not be visually obtrusive or add clutter.



LIBRARY AND THEATRE / ARTS CENTRE

A town library of a scale similar or larger than that which currently exists in Blanchardstown will be developed fronting onto the cultural square. It is proposed to house both the Local Studies and Archives Units of the Council in the same library.

The Theatre/Arts Centre will be a multi-purpose facility which will become the focus of drama, music, dance and visual arts activities. It is envisaged that this Centre would be designed on a model similar to Draíocht in Blanchardstown and would be part of the same building as the Library, Local Studies, Archives et al.

A common lobby will serve the Library and the Theatre/Arts Centre.



HOTEL DEVELOPMENT

Any hotel development must provide active frontage to address the cultural square.

PARKING PROVISION

Parking facilities serving the Cultural Quarter will be provided via a new underground car park below the development area to the east.

Land Use – Key Requirements

- The land use mix in the Cultural Quarter will be predominantly made up of public and cultural uses.
- All buildings addressing the cultural square must provide direct active frontage.
- Shopfront signage shall integrate with the character of the Cultural Quarter and promote the formation of a legible urban environment
- Parking shall be facilitated below ground level.

5.5 Heights and Massing

Building heights and massing around the Cultural Quarter will be controlled within a strategy which responds to the size of the adjoining spaces. Shadow studies will be conducted to ensure that the height and mass of the buildings are appropriate and do not have a negative impact within the site or on the surrounding areas.

CULTURAL SQUARE

It is envisaged that there will be a reasonably consistent parapet height around the cultural square to ensure a coherent space. The scale of buildings in this location will be 5-6 storeys taking its cue from the existing Penney's store. This new datum is in keeping with the Pavilions retail mall adjacent and reflects the viable retail storey heights and required mixed use to achieve a compact and sustainable Town Centre development.

WARD RIVER PLACE

Buildings in this area will be 2-4 storeys in height and will be linear in their form responding to the park side location and the desire to form a view from the Cultural Square to the Ward River Valley Park and St. Columbas Church and Round Tower. It is important that no buildings in this location restrict this view and connection.

EDGE CONDITIONS

Buildings at the south side of South Street will step down in scale to respect their domestic neighbours along the Dublin Road and at Carlton Court. This issue is addressed further within Section 6.0 relating to the Pavilions Development Area.

LANDMARK BUILDINGS

There are opportunities to create landmark buildings on both sides of Dublin Road addressing the Cultural Square and the Ward River Valley Park. Landmark buildings are not required to be large in scale rather they should be distinctive in character and of outstanding architectural merit.

Height & Massing – Key Requirements

- No buildings fronting the Cultural Quarter shall be higher than the precedent set by the adjacent Penneys development on the Dublin Road.
- No buildings will obscure views into the Ward River Valley Park and St. Columba's Church and Round Tower from the Cultural Quarter.

5.6 Landscape Strategy

The landscape strategy for the area is prescribed by several essential factors.

- **Site context:**

- Proximity to existing Town Centre
- Proximity to adjoining residential areas of Carlton Court.
- Proximity to Ward River Valley Park.
- Proximity to the adjacent development of Pavilions and associated new routes and access points.

- **Pedestrian comfort and usability**

Where the Dublin Road comes in contact with the pedestrian realm, it is essential that the landscape treatment responds in such a way that the public space is adapted or treated in a manner that protects the users of the Cultural Quarter, while avoiding an unnecessary interruption to the vehicular flow.



The general landscape strategy in relation to the Cultural Quarter area is as follows.

- **The Ward River Valley Park** will be extended to engage with the Cultural Quarter and the current car parking use replaced with publicly accessible landscape around a park fringe development.
- **Elements, Materials and Expression:**
Artwork – elements such as sculpture, art and paving patterns will be used at appropriate locations throughout the Cultural Quarter.
- **Lighting:**
Lighting will be used as an integral design element while also acting as a functional safety feature. There will be several distinct types of lighting employed throughout the Cultural Quarter which will form part of the holistic lighting strategy for Swords Town Centre. These are as follows:
 - Pedestrian areas – mix of column mounted lanterns and building mounted lanterns. Mounting heights will be fully integrated with the form of the architecture and provide human scale.
 - Amenity – additional lighting around the public spaces.
 - Architectural – Illuminating building forms can help reinforce the identity of a place. This form of lighting may be internal or external.
 - Landscaping – The lighting of both hard and soft landscaping will be carefully integrated into the whole landscaping proposal and will extend to elements within the Ward River Valley Park.
 - Event and art – temporary and permanent use of light, particularly in a dynamic and creative way, will be employed in the Cultural Quarter.
 - Security and safety – Any specific lighting for security will be carefully controlled. Safety lighting will be considered wherever hazards may occur.
- **Vista and Termination:**
Particular attention will be paid to the vistas from Cultural Quarter to the Ward River Valley and St. Columbas Church and Round Tower. It is envisaged that tree nodes and green areas will form the terminations of views and it is important that proposed buildings do not obstruct important views.
- **Material Continuity:**
The continuation of a uniform quality paving material shall provide an apparent extension and integration of the Cultural Quarter into the Town Centre as well as the future Pavilions development area.



6.0 Pavilions Development Area

6.1 Redevelopment and Expansion

URBAN GRAIN

The Masterplan recognises the need to move away from the current box retail format and to integrate new development with the new Cultural and Metro Quarters as well as proposed urban expansion at Barrysparks. It is therefore proposed to support the provision of a network of streets and public spaces within the Pavilions site, which will integrate with and address adjoining areas. It is recognised that the delivery of the network will take place on a phased basis as sites are developed and appropriate infrastructure is provided. The form and scale of development will be greater than the existing, but will be so designed as to integrate with the existing surrounds and give priority to pedestrian and public transport movement and interconnections.

MIXED USE DEVELOPMENT

In addition to the urban design criteria governing the proposed redevelopment of this area, consideration must also be given to the quantum and mix of uses necessary within the Pavilions urban extension to sustain a vibrant and integrated Town Centre. The proposed mix of uses will include retail, retail services, residential, commercial, office, cultural, hotel, leisure, community, and other uses appropriate to a Town Centre location. It is acknowledged that a significant quantum of comparison retail floorspace is needed to respond to the deficit in comparison retail provision which currently exists. There will therefore be a significant provision of comparison retail within the urban extension areas.

DESIGN GUIDANCE

Urban design guidance and objectives are put forward within this Masterplan, which will address the key urban form issues of: movement, street hierarchy, open space (public realm and landscaping), height and massing.

6.2 Constraints and Opportunities

CONSTRAINTS:

- Traditional Shopping Mall format of the Pavilions does not facilitate linkages east to west between Dublin Street/proposed Cultural Quarter and the proposed Metro Quarter and Metro Stop.
- Protection of existing residential amenity to the south adjoining Carlton Court and to the north adjoining Longlands.
- Potential barrier of R132 and Metro North line between MC zoned lands west of the R132/Pavilions development area and east of the R132/Barrysparks development area.

OPPORTUNITIES:

- Development of the Metro Quarter to the east and gateway entrance into Swords Town Centre.
- Existing landscape corridor setting along the R132.
- New mixed-use development may act as a catalyst to stimulate and assist in the creation of a permeable network of streets and public spaces integrating with the Town Centre nodes/focal urban spaces of proposed Cultural Quarter and Metro Quarter.
- Create new linkages between proposed Cultural Quarter, Metro Quarter and Barrysparks lands.
- Introduction of a mix of uses, with greater levels of retail, residential, office, commercial and leisure elements further enhancing the attractiveness of the Town Centre.
- Development of landmark and high quality architectural and environmental standards.



6.3 Movement Strategy

OBJECTIVES

- To provide for a legible and permeable high quality urban environment, readily recognisable and easy to move around.
- To provide for a network of well-designed safe, direct and attractive pedestrian and cycle routes throughout the development lands, linking these lands to Main Street, Cultural Quarter, Metro Quarter and Barrysparks development area.
- To ensure the Pavilions development provides for the ultimate integration with the surrounding transport network, particularly with the Swords Metro Stop at ground and first floor level, with the vehicular road network ('Metro Box*') below ground level, with the public transport bridge over the R132 and vehicular connections to Dublin Street/Road and Malahide Road, as each element of this transport infrastructure comes on stream.

PERMEABILITY

The Pavilions site is bound by Dublin Street/Road to the west, R132 and proposed Swords Metro Stop to the east and Malahide Road to the north. This site must integrate physically with the Town Centre and ensure permeability and accessibility by all modes of transport, including public transport, pedestrians, cyclists and private car.

CONNECTIVITY

Connectivity between the Pavilions urban extension area and the surrounding lands will be supported overtime via:

- Pedestrian connectivity to the Cultural Quarter, Main Street, Metro Quarter and Malahide Place.
- Public transport, cycle and pedestrian connectivity from the Malahide Road over the R132 to Barrysparks.
- Connection with the Swords Metro Stop (multi-modal transport interchange).
- Private car access via R132 and Metro Box*; access from the Dublin Street/Road (south of Cultural Quarter); and from Malahide Road.

STREET PATTERN

The following network of streets is proposed [see map Movement Strategy - Pavilions Development]:

Three main streets:

- **Central Street**
A direct 24 hour pedestrian street, 'Central Street', will traverse the site east-west, thereby providing a direct link from Main Street/Cultural Quarter area to the Metro Quarter. This street, which is an inclined street, will

* Metro Box is an underground vehicular connection across the R132, associated ramps, and necessary realignment of the R132 locally, in order to provide for access to lands zoned 'Swords Town Centre' east and west of the R132, as shown in the Fingal Development Plan 2005 to 2011. This Metro Box plan was adopted under Part 8.

continue over the R132 connecting west and east Swords, specifically bulk of residential and new mixed-use development at Barrysparks.

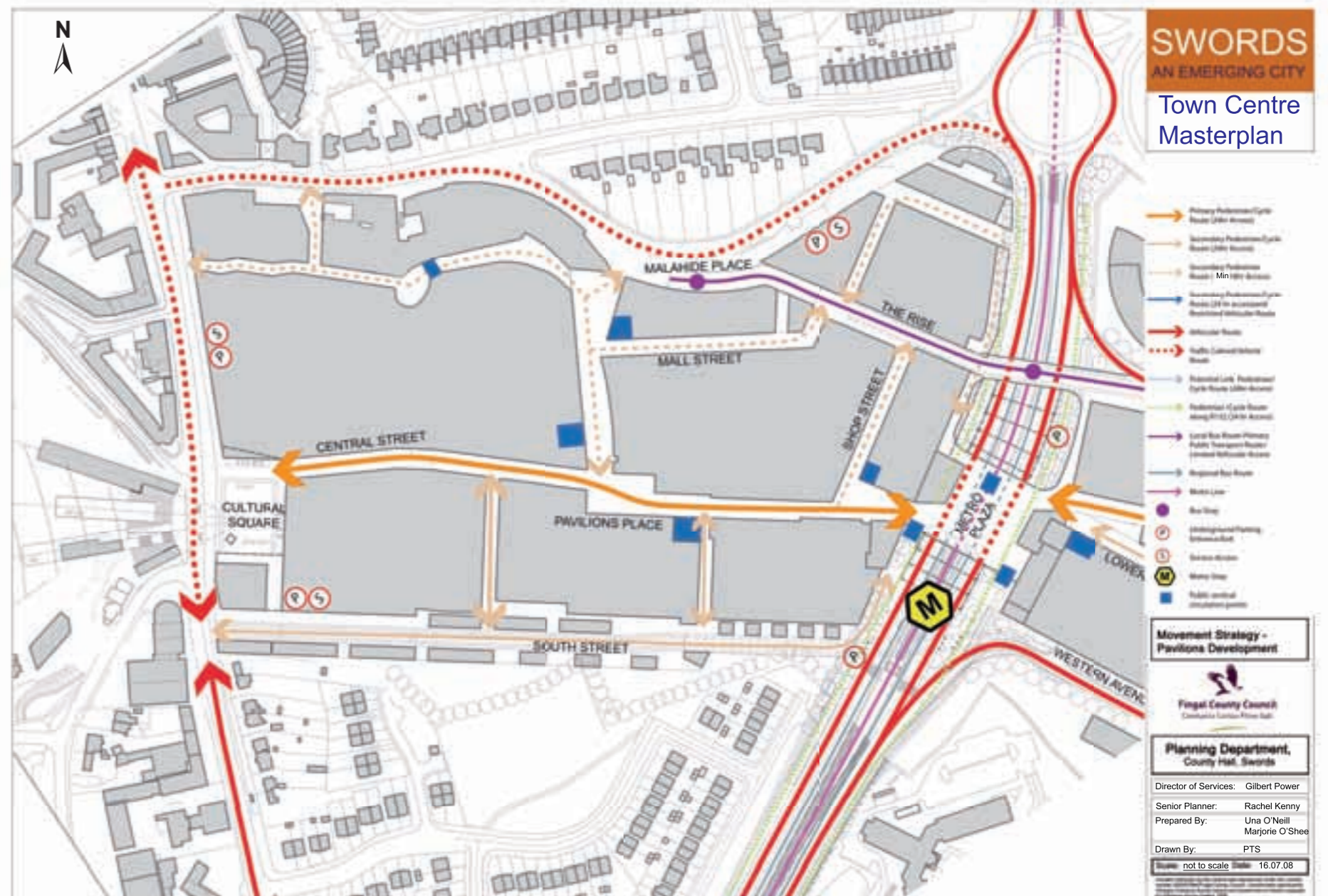
- **The Rise**
The Rise will be an inclined street, which rises at a slope of less than 1:20 from existing ground level on the Malahide Road to level +1 at the Metro Plaza, returning to ground level within the Barrysparks site. This 24 hour route will accommodate public transport connecting Main Street to the Barrysparks lands and interchanging with the Metro Stop via the Metro Plaza.
- **South Street**
To the south of the scheme, there will be a 24 hour residential street, 'South Street'. This street traverses the site east-west and is accessed from Dublin Street/Road and may in the future connect over the R132 to the lands on the east. The street slopes from level 0 at Dublin Street/Road to +1 within the site. The street will be well landscaped to ensure enhanced residential amenity for existing and future residents in this area.

Side Streets:

A number of north-south connecting side streets are proposed to support the east-west permeability provided by Central Street, The Rise and South Street.

Metro Plaza:

The Pavilions site will be designed and developed to integrate with the Metro Quarter. The Metro Quarter comprises a Metro plaza/podium at the upper ground level and a bridge structure over the R132, connecting the Pavilions and Barrysparks sites to each other and to the Town Centre. There will be 3 vertical circulation nodes to facilitate multi-modal private and public movement at this location [see Section 7 addressing Metro Quarter]. The Metro plaza/podium will provide a well designed destination node and potential meeting place for those arriving by Metro or bus to Swords Town Centre.



Specific Local Objectives:

- Development should provide for integration with the Metro Plaza and Metro Box*. These connections shall be provided on a phased basis, to complement and serve future development in Swords.
- The provision of the Central Street connection to the Cultural Quarter shall take place in conjunction with the development of the Cultural Quarter located east of Dublin Road/Street.
- Central Street, The Rise and South Street shall facilitate 24 hour pedestrian and cyclist public access.
- Shop Street, Mall Street and existing internal streets (within the Pavilions existing building) shall support longer public access (minimum 18 hours).
- The Rise shall be a 24 hour street for pedestrian and public transport movements linking Malahide Road, through the northern section of the Pavilions development area, and ultimately over the R132, and connecting with the Barrysparks lands.
- The Rise, Central and South Streets shall be constructed at a gradient of less than 1:20 in order to accommodate the proposed level and nature of public access.

VERTICAL CIRCULATION WITHIN THE PUBLIC REALM BETWEEN 0 AND +1

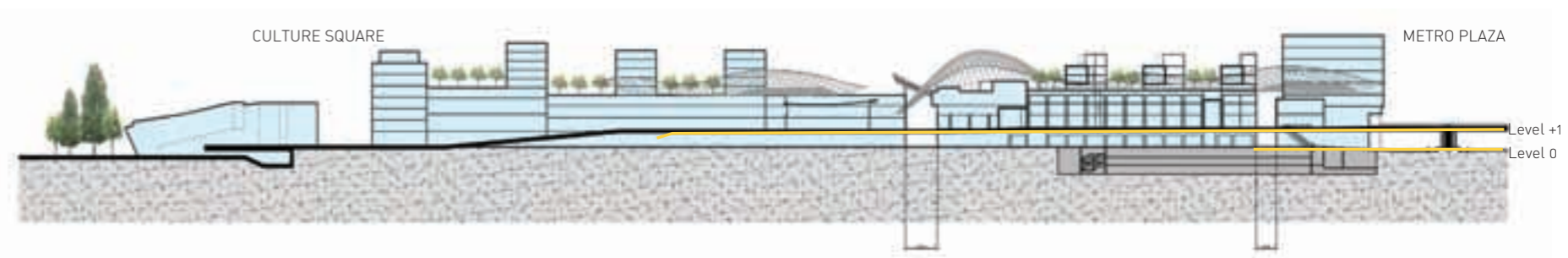
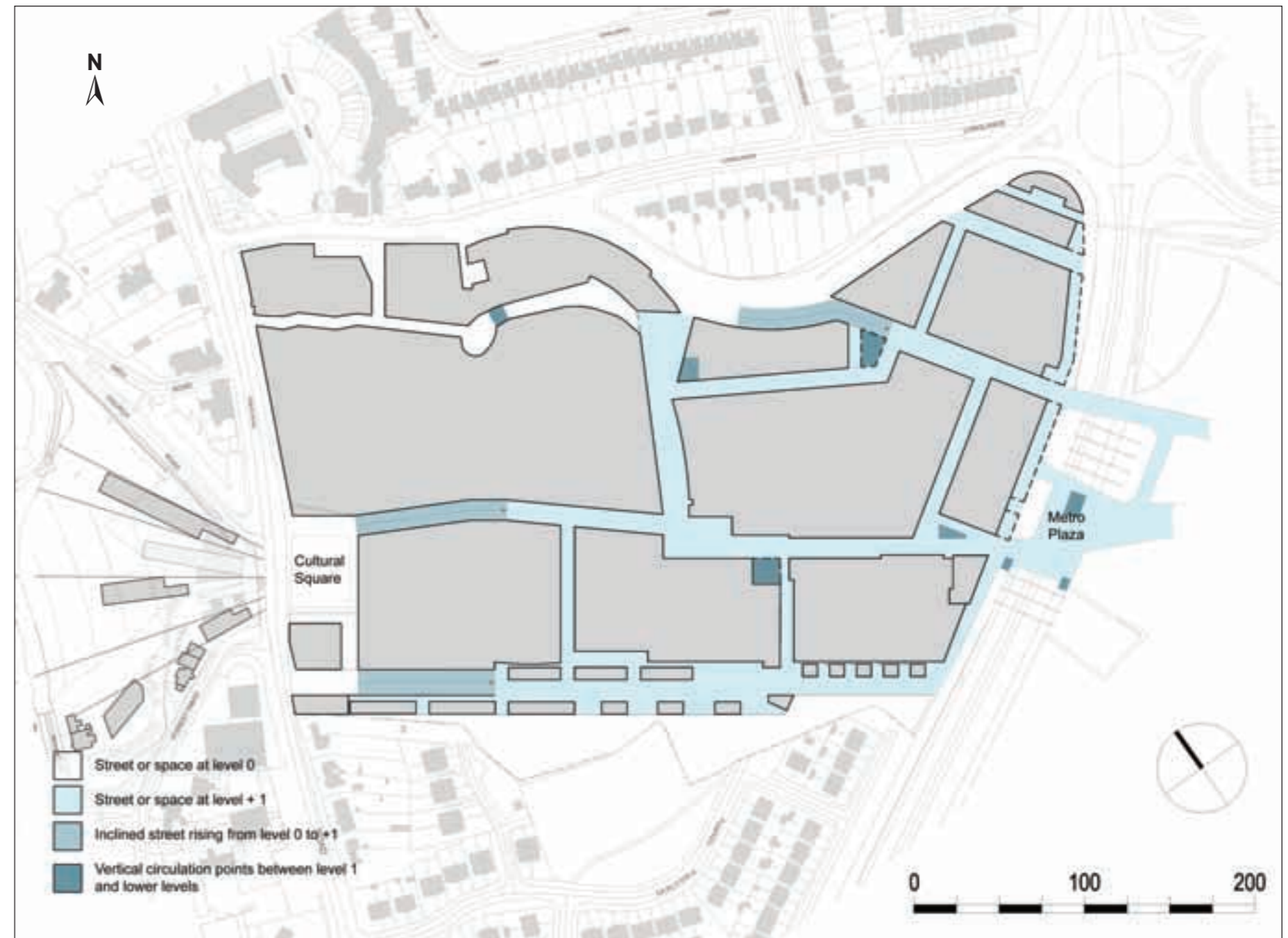
The creation of a multi level public realm will present opportunities for different forms of vertical movement. The vertical movement will be expressed as:

- Inclined streets, which will be pedestrian friendly, rising from the Cultural Quarter eastward to the upper ground level (Level +1) along Central Street and South Street.
- Inclined street from the Malahide Road, which continues as The Rise toward the Metro Plaza, which will cater for public transport.

Direct vertical circulation between level 0, level +1 and levels -0 will be in the form of stairs, lifts and escalators which will connect between upper and lower levels. These circulation cores are located adjacent to entrances to streets and public spaces. Circulation cores shall not be positioned within the general width of the street. Visual connection between the two primary street levels (at 0 and +1) shall be provided at each circulation node in the form of apertures with no more than 30% of their plan area occupied by stairs, escalators and lifts. They allow for the integration of existing levels 0 and +1 of the existing Pavilions buildings within the new streetscape.

* Metro Box is an underground vehicular connection across the R132, associated ramps, and necessary realignment of the R132 locally, in order to provide for access to lands zoned 'Swords Town Centre' east and west of the R132, as shown in the Fingal Development Plan 2005 to 2011. This plan was adopted under Part 8.

LEVELS / MOVEMENTS



INDICATIVE SECTION THROUGH 'CENTRAL STREET' SHOWING LEVEL CHANGES FROM CULTURE SQUARE TO METRO QUARTER

6.4 Density and Mix

LAND USE STRATEGY

Objectives

- To promote a mixed use, primarily retail led, development, which will integrate with the evolving city of Swords, adding to its vitality and sustainability. The mix of uses shall include retail, retail services, residential, office, hotel, social, and cultural uses.
- To ensure the provision of residential units at strategic locations overlooking 24 hour streets.
- To promote the creation of active ground floor frontages, such as restaurants, cafes, bars and retail services, particularly at public places/squares and along 24 hour streets.
- To support access to the Town Centre by sustainable modes of transport and accommodate appropriate levels of parking within 800m distance of the Swords Metro Stop.

The land use mix within this urban extension area will relate to retail, retail services, residential, office, hotel, social, community and cultural uses. This mix of uses and the quantum of this mix is important in the overall vitality and viability of the Town Centre, as is the urban design framework, street network, form and scale of development [see map Uses Diagram].

RETAIL DEVELOPMENT

In terms of the retail mix, it is deemed appropriate that the majority of new retail provision be in the form of comparison shopping, with expansion in large scale convenience retail more appropriately located outside of the Town Centre in proximity to the local population it serves. Retail anchors are proposed within the Pavilions site: 2 anchors are to be located within the eastern section of the site to the north and south of the Metro Plaza; 1 smaller anchor will be located adjacent to and supporting the Cultural Quarter; it is anticipated the anchors within the existing scheme will remain in their current locations. The anchors will primarily be located adjoining the two 24 hour streets – Central Street and The Rise. All retail streets, primary and secondary, will support active frontages with frequent entrance points along the street. Long inactive frontages will not be supported.

Retail services

Retail services (including non-retail uses such as Cafés, Restaurants, Smoothie Bars, Sandwich Bars, Confectionary and Coffee Shops etc) are required particularly around key urban spaces to act as focal points of attraction, with active frontages addressing the spaces. Such uses will in particular be supported at the Cultural Quarter, Pavilions Place, Malahide Place and onto the Metro Plaza to ensure day and evening activity.

RESIDENTIAL

Residential provision within the scheme will be primarily in the form of apartments and shall be located so as to achieve vibrancy/activity and overlooking along the main 24 hour streets, namely Central Street, The Rise, corner location of Main Street/Malahide Road, corner of Malahide Road/R132, and South Street.

Apartments shall be provided over Central Street and The Rise to ensure activity along these 24 hour streets.

In terms of complimentary edge uses, South Street shall be a residential only street to the southern edge of the Pavilions site, adjoining the existing Carlton Court/Paddocks residential areas. This street may also support limited community facilities, such as a crèche.

400 units – 600 residential units will be accommodated within the development. These units shall comply with National and County Development Plan Standards in terms of size, layout, and private/public amenity open space.

OFFICE DEVELOPMENT

Office development is proposed primarily at the Metro Plaza. Ground floor uses fronting the Metro Plaza shall be active in nature and evening activity promoted. Parking requirements will reflect the location of this use adjoining the Swords Metro Stop. The integration of high density employment generating uses adjoining the Metro is in support of sustainable development.

CULTURAL QUARTER

The Cultural Quarter will be integrated with the Pavilions development and will support a mixture of cultural, retail and retail services/uses within the setting of a focal urban space. An anchor retail unit on Central Street will abut the cultural square and act as a draw to this area. Located onto the square will also be a theatre, library, galleries and arts centre, hotel and a range of restaurants and café facilities. The public square itself will be capable of supporting outdoor performances, outdoor dining, weekly market stalls and meeting/seating spaces. The square will have a strong landscape element including formalized tree planting, a water feature, and permanent commissioned sculptural and lighting art installations

PARKING PROVISIONS

Parking shall be located primarily underground within the Pavilions scheme, facilitating the development of a pedestrian friendly environment. County Development Plan parking standards will be considered within the context of the location of this site beside Metro North, which is a high-quality public transport facility, and the potential for interlinking of trips between the various uses.

A Traffic Impact Assessment and a Mobility Management Plan will be required to be submitted as a part of a planning application for this area. This Plan will consider the proximity of development to the Metro Stop, interlinking of trips for those visiting the area and potential catchment area. The Swords traffic model undertaken by FCC shall be used to assess the implications of the proposed development.

Specific Local Objectives:

Retail

- Active frontages shall be provided along retail streets within the scheme and shall be dominant in particular fronting all urban spaces, particularly at Pavilions Place, Malahide Place, Metro Quarter and Cultural Quarter.
- Shopfront signage shall integrate with the character of the streetscape and promote the formation of a legible urban environment.
- Parking shall be facilitated below ground level and a Car Parking/Traffic Management Strategy for the area shall be submitted at planning application stage.
- A Retail Impact Assessment shall be submitted at planning application stage.

Residential

- A range of 400-600 residential units shall be provided within the scheme to ensure vibrancy and activity along the network of 24 hour streets.
- Residential units shall be provided along Central Street, The Rise, corner of Malahide Road/Main Street, corner of Malahide Road/R132, and South Street.
- South Street shall be a residential street and shall be designed to protect the amenity of residential units in the adjoining Carlton Court and Paddocks developments. The minimum standards set down in the Masterplan document in this regard shall be adhered to and a Visual Impact Assessment shall be submitted as part of the planning application.
- All residential units shall be assessed at planning application stage in terms of height, massing, sunlight and daylight impacts, visual impact, overlooking, wind impact, acoustic privacy, and provision for private and public open space.
- Private open space and semi-private open space shall meet national and development plan standards. Privately accessed toddlers' play grounds shall be accommodated on-site. Larger public open space requirements may be addressed within the neighbouring Ward River Valley Park, and proposed regional park in Swords, subject to agreement with the planning authority.
- Parking provisions shall be assessed within the context of the Fingal County Development Plan provisions, proximity to Swords Metro Stop and public transport interchange and potential for inter-linking of trips between uses.

Office

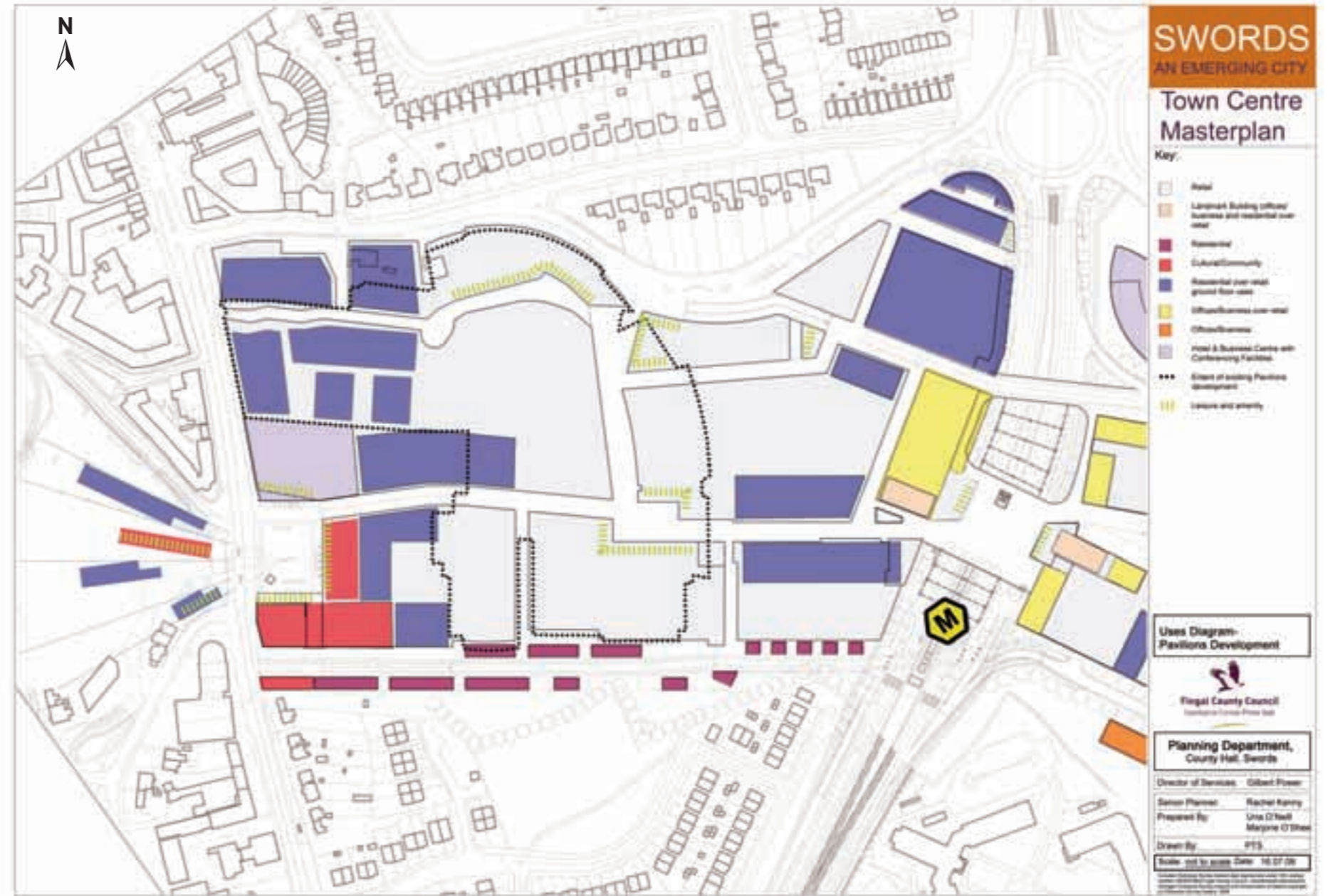
- Office Development shall be facilitated along the R132. Additional small scale office units shall be permissible within the development area to support the mix of uses at this location.
- Office development shall be assessed at planning application stage in terms of Fingal Development Plan standards, and in particular shall be assessed in terms of height, massing, sunlight and daylight impacts, visual impact and wind impact.
- Parking provisions shall be assessed within the context of the Development Plan provisions and proximity to Swords Metro Stop/public transport interchange.

Traffic, Mobility and Parking Management

- A Traffic Impact Assessment shall be submitted at planning application stage.
- A Mobility Management Framework, including Mobility Management Plan and a Car Parking Management Strategy, shall be submitted at planning application stage.
- Innovative approaches, such as the development of car clubs and car sharing will be considered by the Planning Authority.

Indicative Quantum of Proposed Uses:

- 80,000 – 100,000 sqm gross of comparison retail, in addition to existing retail provision.
- 8,000 – 10,000 sqm gross convenience retail.
- 20,000-40,000 sqm gross office space.
- 400-600 number of apartments.
- 20% of the overall scheme will comprise retail services.
- Cultural Quarter, including provision for a small retail anchor and a theatre, library and arts centre, hotel, cafés, galleries, etc.
- Metro Quarter: commercial uses, transport interchange and ancillary infrastructure.



6.5 Heights and Massing

Building heights and massing will be controlled within a height strategy for the Pavilions area as a whole and as part of an overall Swords Town Centre Heights Strategy.

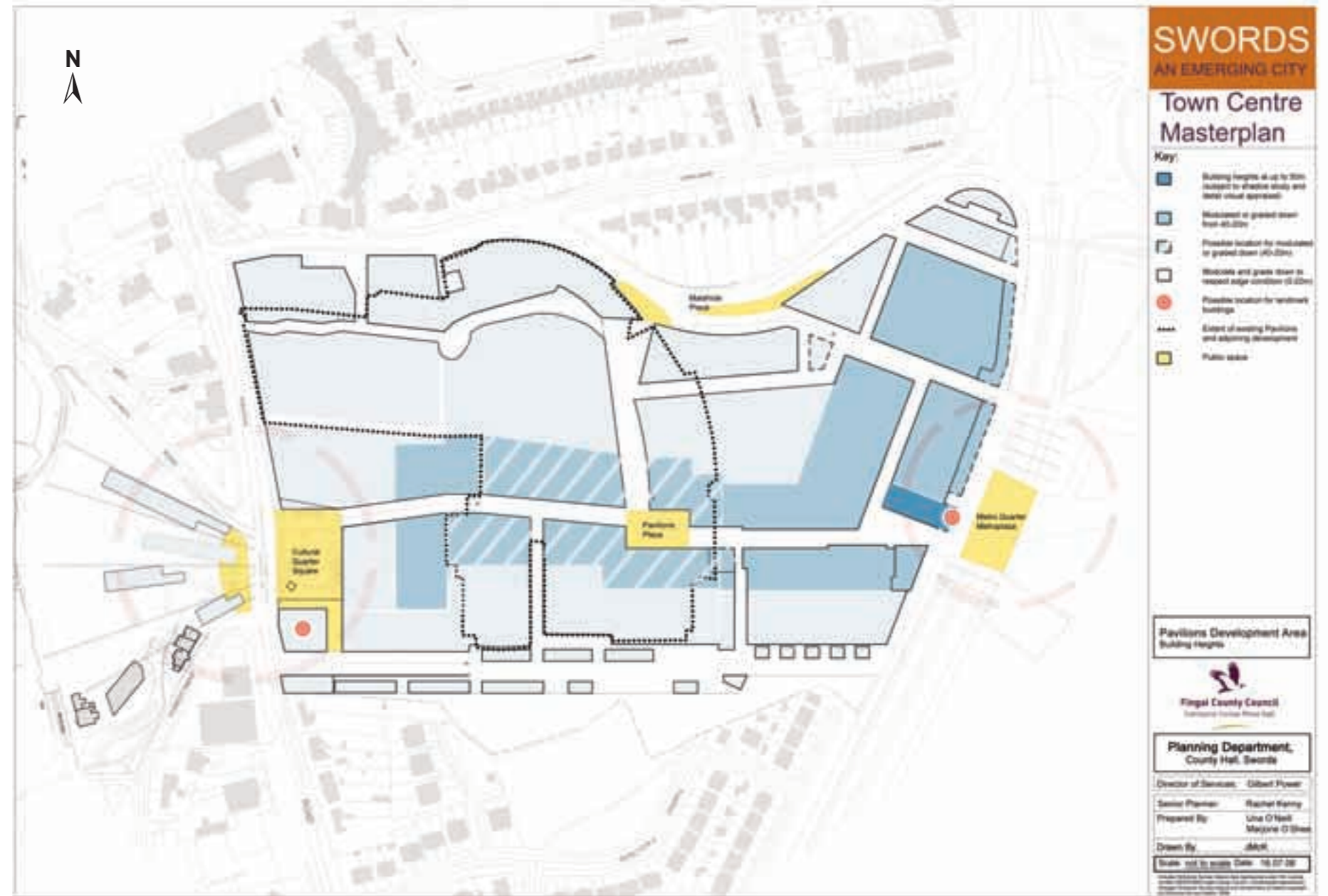
This strategy responds to:

- (i) The existing context and neighbouring areas:
Building height will be controlled to respond sensitively to both Carlton Court along the southern edge of the development area, and Longlands along the Malahide Road to the north. With regard to massing, there is a specific requirement for a set back of building massing to below a 30 degree (from horizontal) angle from the current boundary wall of Carlton Court within a distance of 45m. The Carlton Court boundary condition requirements are set out in the following Edge Conditions section and Landscape Strategy.
- (ii) The creation of 2 major public spaces of significant size, at the proposed Cultural Quarter and Pavilions Place, and the need to create an appropriate scale of enclosure.
- (iii) Need to provide a modulated or graded profile away from the buildings about the major public spaces which indicates a hierarchy of space.
- (iii) Identified locations within the Town Centre Heights Strategy, where landmark forms can be accommodated.

Building heights are indicated as a series of height ranges which respond to the height strategy, uses within each building, the need to create a modulated building profile and to minimise any potential of overshadowing [see map 'Pavilions Development Area: Building Heights'].

The following key elements are important in terms of building heights proposed within the development area:

- Building heights are provisional and subject to verification that adequate sunlight and daylight levels are achieved within the development and there is no adverse impact on adjoining residential areas. Shadow studies will be conducted to ensure that the height and mass of buildings are appropriate and do not have a negative impact within the site or on the surrounding areas.
- Building heights are indicated above ground level. The tallest building heights are indicated at 50m, marking the eastern edge of the Metro Quarter about the Metro Plaza and marking the gateway and arrival point to the expanded Swords Town Centre.
- Building heights grade down in stepped ranges from 40m to 20m south, north and west of the Metro Quarter.
- Building heights grade down further in lower ranges from 0 to 20m toward the sensitive northern and southern edges.
- Building heights are indicated within 10-15m range along edge to Carlton Court [see Edge Conditions 3.0 Southern Edge]. Heights are subject to the provision of suitable landscape screening to this edge.
- Building heights along the Malahide Road are to retain a consistent parapet line with existing Pavilions Building (15-17m) while responding to the existing residential character of Longlands to the north. At the eastern end of Malahide Road, near the Malahide Road roundabout, there is potential for building height of up to 20m.
- Building heights grade down from 30-20m about the future Cultural Quarter to create appropriate enclosure about the eastern edge of this future public space.



EDGE CONDITIONS

The north, east and south boundaries are considered as edges and shall be coordinated with neighbouring lands to respond to the existing scale, use and character.

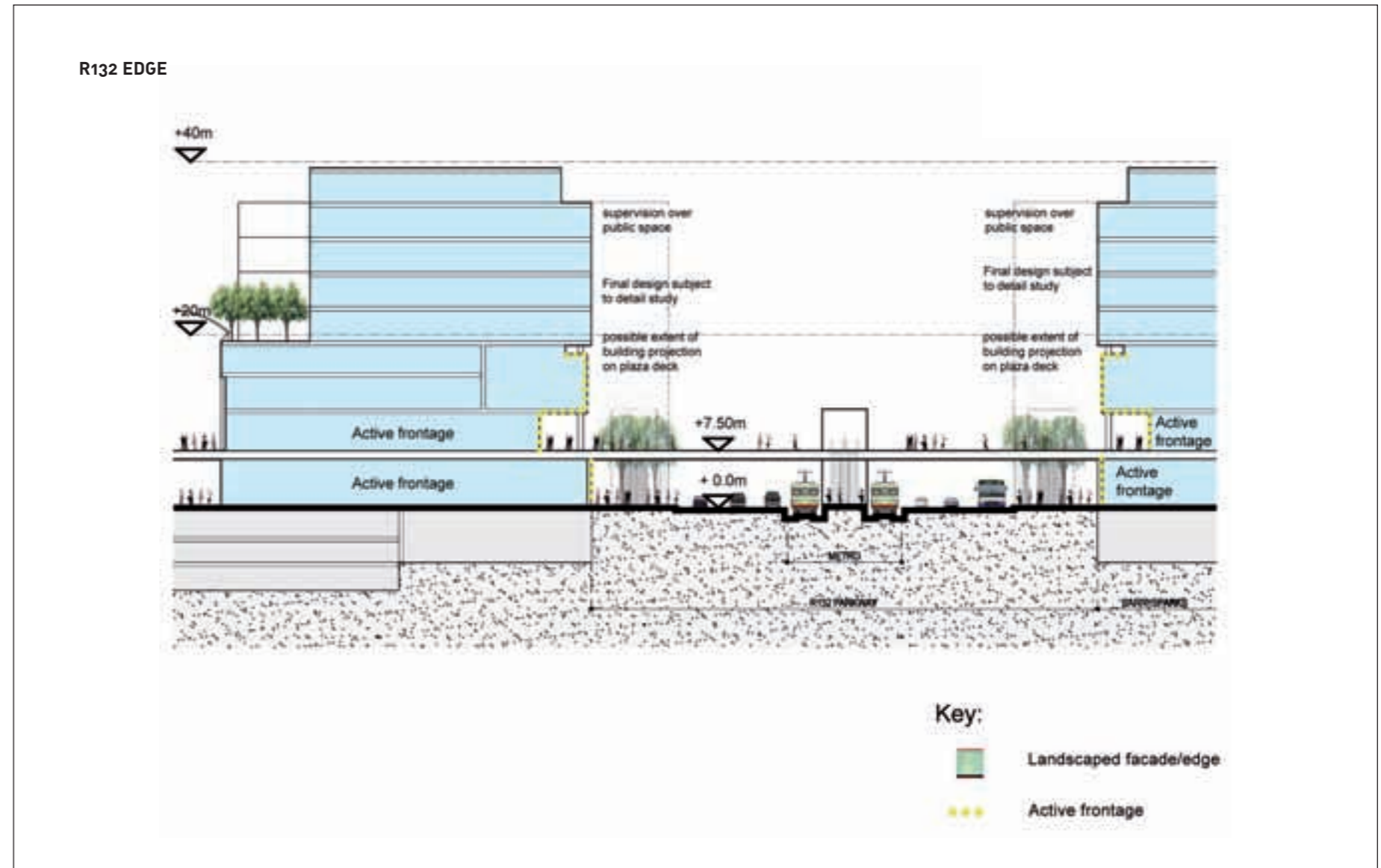
1. R132 [see Cross Section]

CHARACTER

- The building forms and heights along the R132 parkway, express the importance of the R132 as a major route, and also form the enclosure of the Metro Plaza which will be a major new public space and gateway to Swords from the Metro Stop.
- This edge shall comprise a series of prominent focal buildings.
- The general building line on either side of the R132 shall be set back a minimum of 13.5m from the road edge to permit the existing parkway/landscaped condition to continue through the Metro Quarter and facilitate the necessary car park ramp access to the adjoining developments.
- The height of buildings along this edge will be 20-40m at Metro Plaza, grading down to approximately 10-15m to integrate with adjoining edges/developments.
- The proposed landmark buildings on the R132 shall have a height up to 50m subject to an assessment of architectural merit.

DESIGN

- The façades along the R132 are an integral component of the Metro Quarter identity and their design should reflect this status.
- Façades should be broken down into discrete clearly separate elements to avoid the appearance of a monolithic wall of buildings. Single façades should be no more than 20% of the site length along the edges.
- Buildings shall have active frontages at ground level below the Plaza, where possible, and at +1 level onto the Metro Plaza. Building at level 0 and +1 will connect into the R132 and Metro Plaza (when developed) respectively. Active frontages will also be supported at , level +2 to provide further activity and life along the Metro Plaza.
- Buildings facing onto the Plaza shall have well-designed and large foyers and addresses accessed from the Plaza.
- The 'plinth condition' caused by the approximate 7m rise required to cross the R132, is to be treated as a landscape element integral to the R132 public realm. The 'plinth' design form and qualities will be set by the Metro Quarter detailed design.
- Buildings shall be designed to cater for phased delivery of Metro North and Metro Plaza to include at grade connections and access.
- The height of the buildings shall provide an appropriate scale of enclosure to the Plaza.
- The buildings fronting the R132 shall include an arcade treatment and active frontage at level +1 along the R132 to allow movement along this edge until the arrival of the Metro Plaza. These uses at level +1 will be capable of extending to a new alignment along plaza, and filling the arcade space, in the event of such being created.



2. Malahide Road [see Cross Sections]

CHARACTER

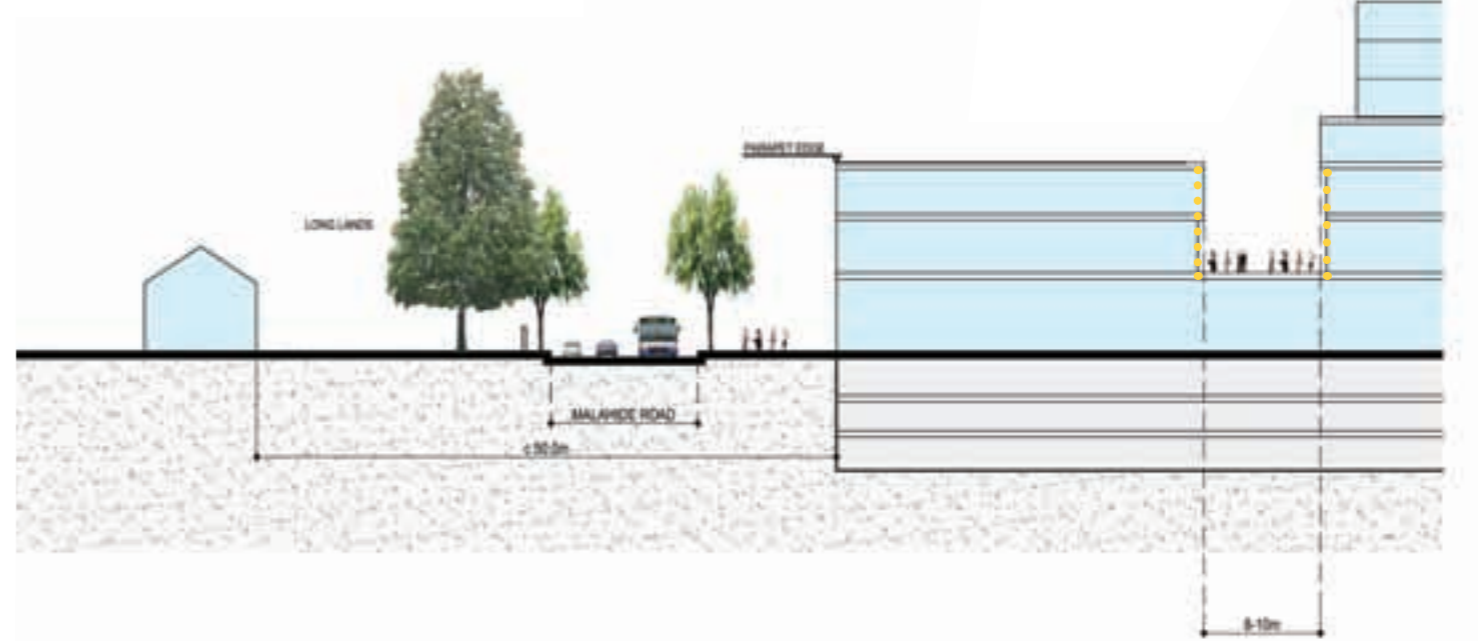
- The northern edge is formed by the busy Malahide Road which as part of the Pavilions Development shall contain a series of routes, access points, set down areas and a junction with the inclined street, The Rise.
- A landscape corridor shall adjoin the Pavilions Development edge, linking from the R132 via Malahide Road to Main Street.

DESIGN

- New buildings will have a parapet height along the Malahide Road which will be consistent with the existing Pavilions height at c.15-17m
- Building above this height shall be set back and stepped in profile to avoid any overlooking, overshadowing or excessive visual dominance of neighbouring residential lands.
- Semi mature tree planting will occur along the length of the Pavilions site facing onto Malahide Road. Such trees will be planted with an initial height of 6m at 8m centres and positioned within prepared tree pits.



INDICATIVE EDGE SECTION 1.0 – MALAHIDE ROAD



INDICATIVE EDGE SECTION 2.0 – MALAHIDE ROAD



INDICATIVE EDGE ELEVATION 3.0 TO MALAHIDE ROAD



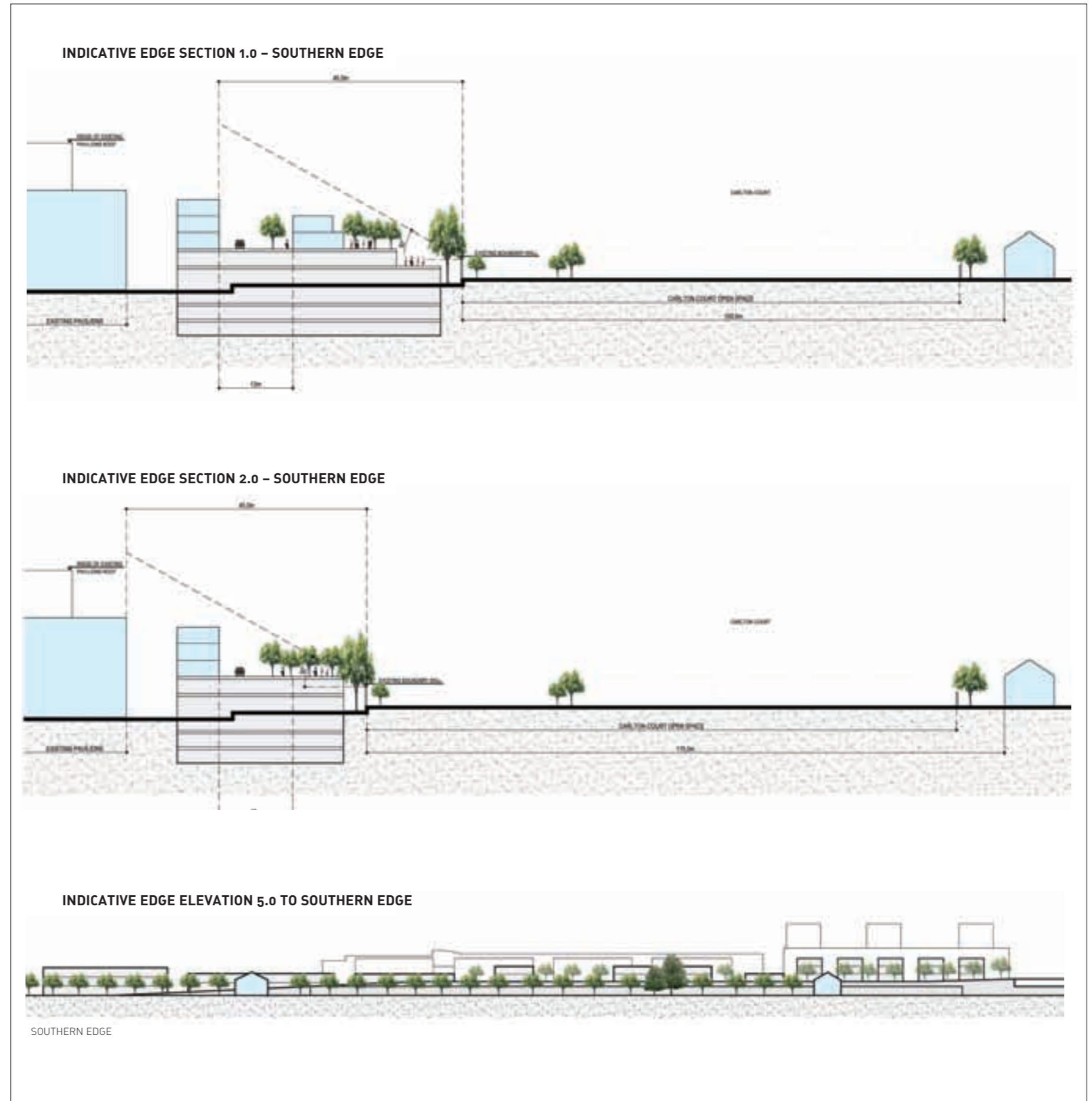
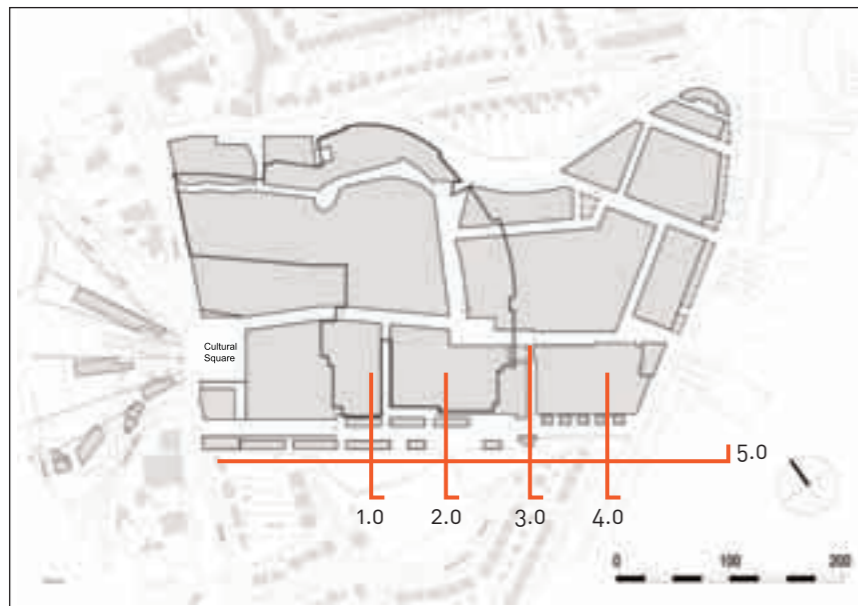
3. The Southern Edge [see Cross Sections]

CHARACTER

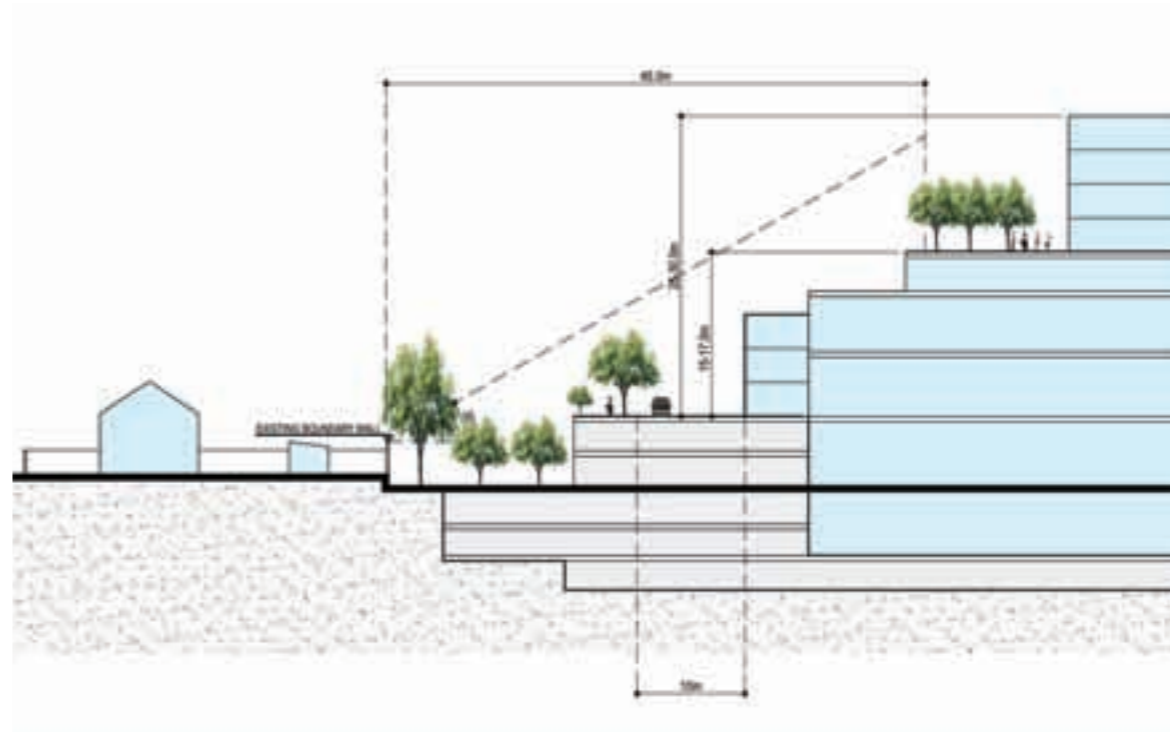
- The southern edge will be a domestic scale residential street, South Street, mediating between the suburban scale of the existing Carlton Court and the urban scale of the Town Centre.
- The impact of the change in level between the proposed South Street and Carlton Court should be mitigated by screening with trees along the boundary wall line and supplemented with similar scale trees at the South Street level.

DESIGN

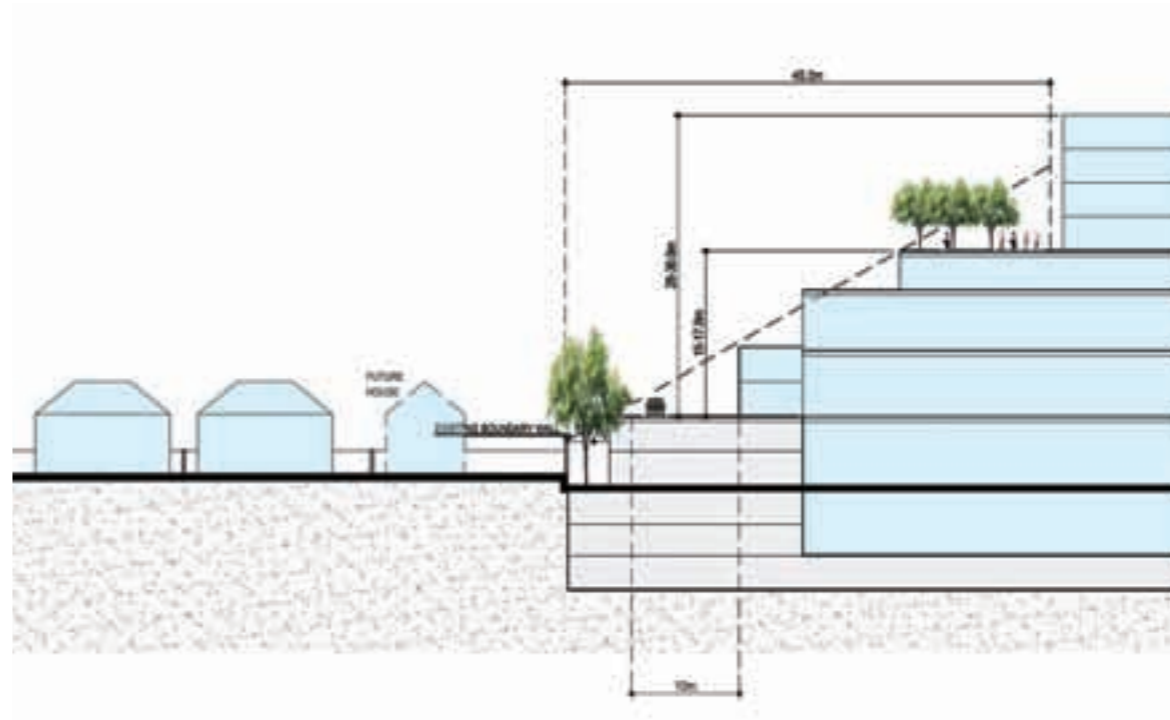
- The building form and heights along the southern edge shall be constrained to respond to the existing scale and character of Carlton Court, to avoid overlooking and overshadowing and significant visual dominance.
- The existing boundary wall shall be retained.
- Buildings shall be set back and profiled within an angle of incidence of 30° for a distance of 45m from the boundary wall.
- A minimum 3 metre wide continuous densely planted tree screen and landscape corridor shall be provided adjoining the edge with the residential development. The trees shall be semi mature, with an initial height of 6m, and these intermediate sized trees shall be spaced and inter-planted to create a continuous screen above the current boundary wall. The planting shall be wholly at ground level and into natural ground with suitable root space to allow the trees to grow to maturity.
- The massing of the residential elements along South Street should be broken up by inter-planting street trees to the same scale as Malahide Road. Trees should be planted in recessed planting pits with appropriate irrigation systems provided.



INDICATIVE EDGE SECTION 3.0 - SOUTHERN EDGE



INDICATIVE EDGE SECTION 4.0 - SOUTHERN EDGE



6.6 Urban Form – Streets

LINKING THE TOWN CENTRE TOGETHER

Objectives:

- To provide for a clearly defined street hierarchy. Each street will be designed according to the core principle that the street is a space, a place of containment and activity rather than simply a movement and access route. The scale and character of each street, its width, and the height of buildings along it shall relate to this hierarchy. A high degree of permeability will be provided throughout the street network, with provision for 24 hour streets and minimum 18 hour streets.
- To create a strong central 24 hour pedestrian street, 'Central Street' through the Pavilions site connecting the Cultural Quarter, Metro Quarter and Barrysparks lands.
- To create a strong 24 hour public transport route, 'The Rise', through the Pavilions site connecting the historic Swords Town Centre, Metro Quarter and Barrysparks lands.

STREET NETWORK

The proposed network of streets will be distinguished in character via:

- Width to height proportions, which will reflect the primacy of the streets within the scheme. Urban design and daylight criteria will govern the width-height relationship.
- Primary use, for example retail, residential access, office use etc, will result in differing street widths and materials.
- The Streets will take the form of a mix of sheltered streets with glazing or unsheltered streets. The purpose of the sheltered streets is to protect shoppers against inclement weather conditions, while at the same time facilitating permeability and a network of 24 hour movement within the scheme which is more akin to a Town Centre environment than a shopping mall.
- All streets will be designed with passive security in mind, including the following features:
 - Overlooking by residential and other uses on the upper floors
 - Clear and unobstructed street lines without recessed or hidden areas for easy surveillance at ground level
 - Residential and office accommodation should have street addresses with overt street level entrance lobbies distributed along all 24 hour routes.
 - All routes should have alternative access points so cul-de-sacs are avoided.
 - Clear street naming signage should be used throughout the area in a consistent manner to a Swords Town Centre standard design.
 - No element of the buildings adjacent the streets shall project over the street width, except intermittent bays less than 10% of the street width.



STREET HIERARCHY
STREET COMPARISON

SHELTER STRUCTURE [see map Shelter Structure]

To improve the amenity of the new streets, a sheltering glass canopy structure is proposed. This canopy will span over the streets protecting them from inclement weather. The design envisages a glass and metal structure that flows continuously in a vaulted form that maintains the daylight and sunlight levels experienced in any open street.

The sheltered streets will relate to the

- (i) Main route from the west of the Cultural Quarter along Central Street to the Metro Plaza
- (ii) The Rise, linking Malahide Road with the Metro Plaza
- (iii) Important retail frontages of Mall Street and Shop Street.

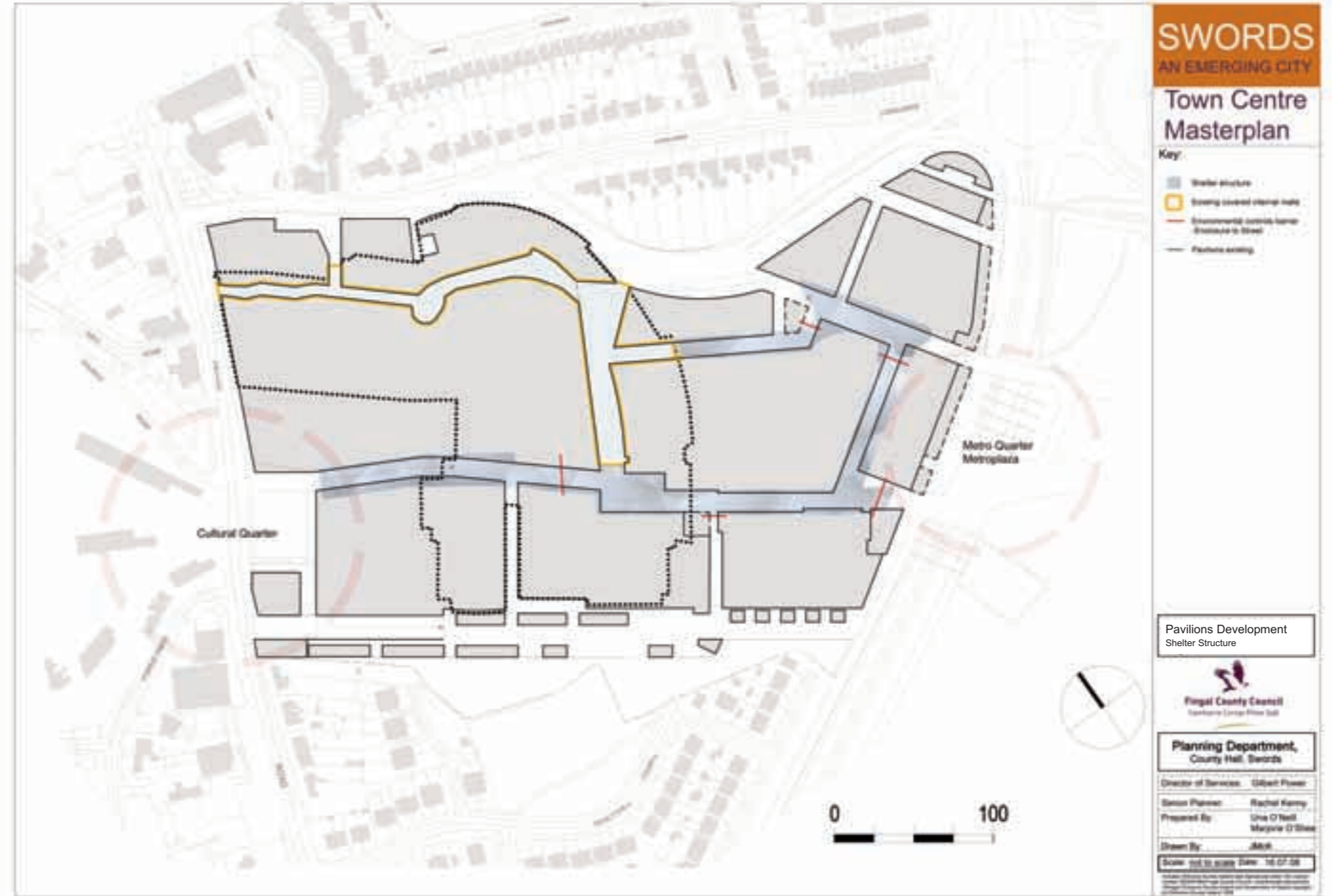
2 key design criteria will define the quality and technical requirements for the shelter structure and its relationship with the streetscape:

(i) Ability to 'read' streets and buildings

A key objective of the masterplan is the creation of a legible street pattern at street level (level+1). The introduction of a glazed canopy shelter structure should not prevent the clear reading of the street along parapet lines and/or building facades rising above. The location of the shelter structure above the street parapet level and its support beyond the street edges (parapets) will remove any structural and edge elements from view.

(ii) Transparency

The level of transparency of the glazed canopy shelter structure will help the reading of the streetscape. The covering over the street will be totally glazed and no opaque panels are to be incorporated in the roof. The net percentage of the roof which is clear glazing, after structure has been taken into account, is c. 70%. The glazed court of the British Museum has a comparable degree of transparency.



STREET HIERARCHY – URBAN DESIGN PARAMETERS AND OBJECTIVES:

Central Street *[see Plan and Cross Sections]*

CHARACTER

- Central Street shall be the main 24 hour public pedestrian thoroughfare linking all public spaces, from Cultural Square, Pavilions Place, Metro Plaza, to 'Upper Street' within the Barrysparks development area.

DESIGN

- The width of the street shall be 12-14m.
- The retail street shall have a continuous façade height between 12 and 15m.
- This street shall be covered by a glazed canopy shelter structure.
- Building façades above the retail street shall be set back to create a stepped profile to the street to allow the glazed canopy to bear onto the roof at this point.
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing.
- Appropriate mechanisms/environmental controls shall be developed in order to ensure that a suitable environment at street level is maintained having regard to the sheltered nature of the street and the need to interface with the existing shopping centre. The environmental controls shall be attractively designed and doors shall not be utilised to enclose the street.
- Materials and finishes to the street shall be designed to reflect the character of an external street.

USES

- Active ground floor uses shall be required.
- Foyers and entrances to larger office buildings shall be facilitated along Central Street.
- Main circulation routes connecting all levels shall be located off Central Street and shall not interfere with the free flow of people along this pedestrian street.
- Uses over the retail street shall form part of street façades creating points of focal interest and passive supervision of the street. The location of residential/office units above shall be coordinated to ensure daylight/sunlight to the street and minimise overshadowing.
- Retail units shall occupy maximum 2 floors (taken at +1 street level) with residential and office units above.
- Landscaped courtyards may be visible providing opportunity for the landscaping to become part of the street character.

The Rise *[see Plan and Cross Sections]*

CHARACTER

- The Rise shall be the main 24 hour public thoroughfare linking Malahide Road to Metro Plaza over the R132 and connecting to the Barrysparks development area. This street shall cater for cyclists and public transport.
- The Rise shall be designed as a sloping street with a bridge over the R132 with a gradient of less than 1:20 (subject to confirmation of detailed traffic engineering design).

DESIGN

- The width of The Rise street will be 12-14m (width subject to engineering requirements for bus access).
- The street shall have a continuous façade height of between 12 and 15m.

- This street shall be covered by a 'shelter structure'. The shelter structure shall not be sealed off at either end by an enclosing element ensuring continued permeability and accessibility within this area.
- Street façade extends above parapet height as a series of intermittent gables and landscape elements.
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing.
- Materials and finishes to the street shall be designed to reflect the character of an external street capable of taking vehicular traffic.

USES

- Active ground floor uses shall be required.
- Foyers and entrances to larger office buildings shall be facilitated.
- The Rise shall facilitate pedestrian, cyclist and public transport access from Malahide Road to the R132 and into the Barrysparks development area.
- Uses over the retail street shall form part of street façades creating points of focal interest and passive supervision of the street. The location of residential/office units above shall be co-orindated to ensure daylight/sunlight to the street and minimise overshadowing.
- Landscaped courtyards will be made visible, providing opportunity for the landscaping to become part of the street character.

Shop Street and Mall Street *[see Plan and Cross Sections]*

CHARACTER

- Shop Street shall link the primary streets of Central Street and The Rise and will have restricted accessibility (minimum 18hr).
- Shop Street shall be a pedestrian only street.

DESIGN

- The width of Shop Street shall be 10-12m.
- The street shall have a continuous façade of 13 to 15m height.
- The street shall be covered by a shelter structure.
- Building façades above the retail street shall be set back to create a stepped profile to street.
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing.
- Materials and finishes to the street shall be designed to have the character of external street.

USES

- Active ground floor uses shall be required.
- Main circulation cores connecting all levels and residential entrances shall be accommodated along Shop Street.
- Uses over the retail street shall form part of street façades creating points of focal interest and passive supervision of the street. The location of residential/office units above shall be co-orindated to ensure daylight/sunlight to the street and minimise overshadowing.

Side Street *[see Plan and Cross Sections]*

Three streets identified on the plan are given the generic name 'Side Street'. The guidance hereunder refers to the basic parameters governing the design of each of these streets.

CHARACTER

- Side Street is a small scale public route linking primary streets with north and south edges. Side Street shall have 24hr accessibility and shall be primarily pedestrian and catering cyclists.

DESIGN

- The width of Side Street shall be 8-10m.
- The profile of the street shall have a continuous façade, while street parapet heights may vary but shall be smaller in scale than primary streets, circa.9-15m.
- This type of street shall be open (unsheltered).
- Building height, orientation and width shall ensure daylight and sunlight within the street while minimising overshadowing.
- Materials and finishes shall be designed to reflect the use as an external street.

USES

- Active ground floor uses incorporating small scale retail service uses and 'own door' suites shall be accommodated.
- Residential entrances, casual recreation and specialist uses shall be accommodated.
- Uses over the retail street shall form part of street façades creating points of focal interest and passive supervision of the street. The location of residential/office units above shall be co-orindated to ensure daylight/sunlight to the street and minimise overshadowing.

South Street *[see Plan and Sections]*

CHARACTER

- South Street shall be a residential street, mediating between the suburban scale of Carlton Court and the urban scale of the Town Centre.
- South Street shall be a public street with 24 hour accessibility which will cater for pedestrians, cyclists, taxi and vehicular traffic. This street shall not be covered.

DESIGN

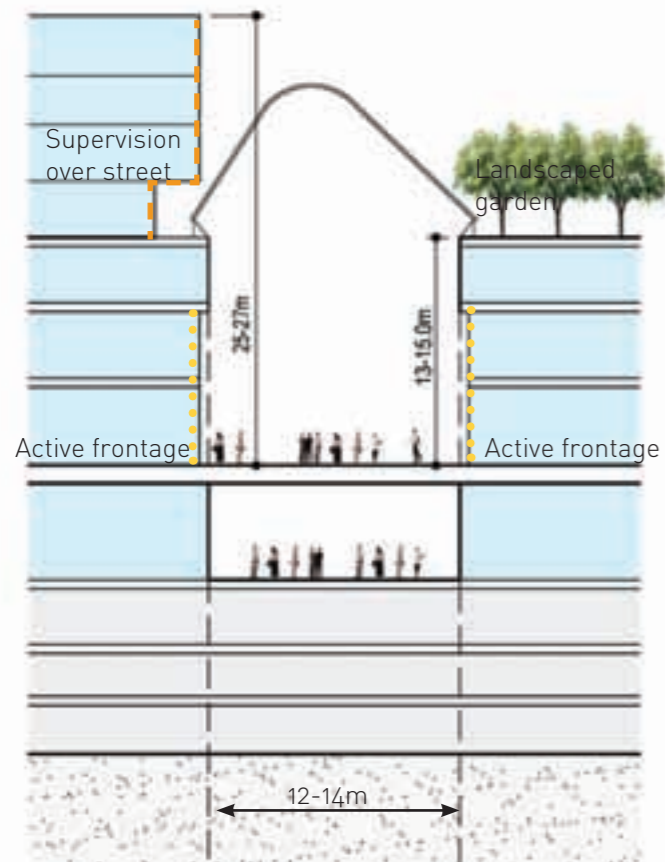
- The width of South Street shall be 13m.
- The profile of the street and height of buildings shall be constrained by the need to create an edge treatment which responds to existing scale and character of the existing adjoining and permitted residential development within Carlton Court.
- The heights and layout of buildings will be restricted within parameters set out in Edges Conditions: Southern Edge.
- A minimum 3m wide continuous densely planted tree screen and landscaped corridor shall be provided at the edge with Carlton Court residential development *(see Edge Conditions: Southern Edge)*. The layout of buildings will provide opportunity for integrating tree planting into streetscape creating a green corridor while creating a softer landscape edge to existing Carlton Court.
- The residential street shall act as an active edge and screen to the retail development behind.
- The residential units shall be 1, 2 and 3 storey and shall reflect the scale and form of existing and permitted neighbouring housing.

USES

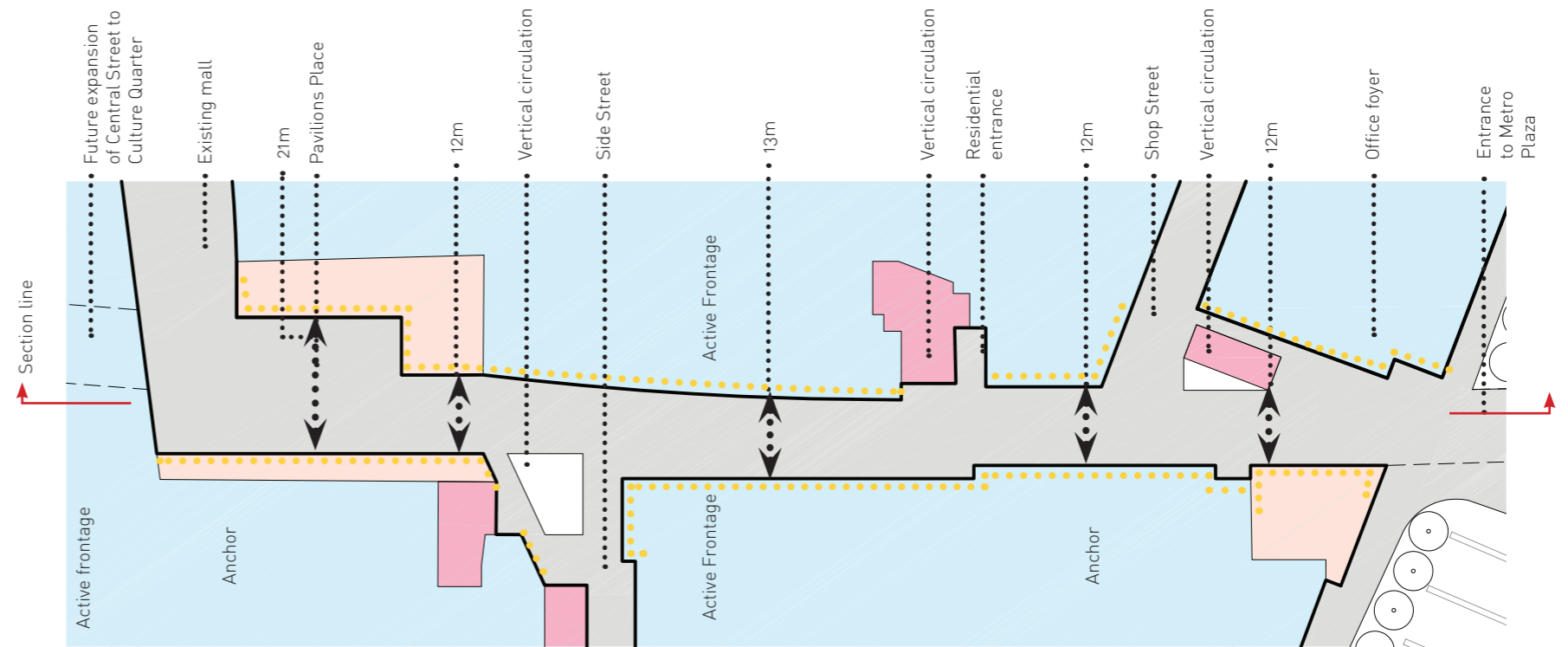
- A mix of own door unit types shall be provided, with provision for community facilities, such as a crèche.

STREET HIERARCHY
PRIMARY STREET: CENTRAL STREET

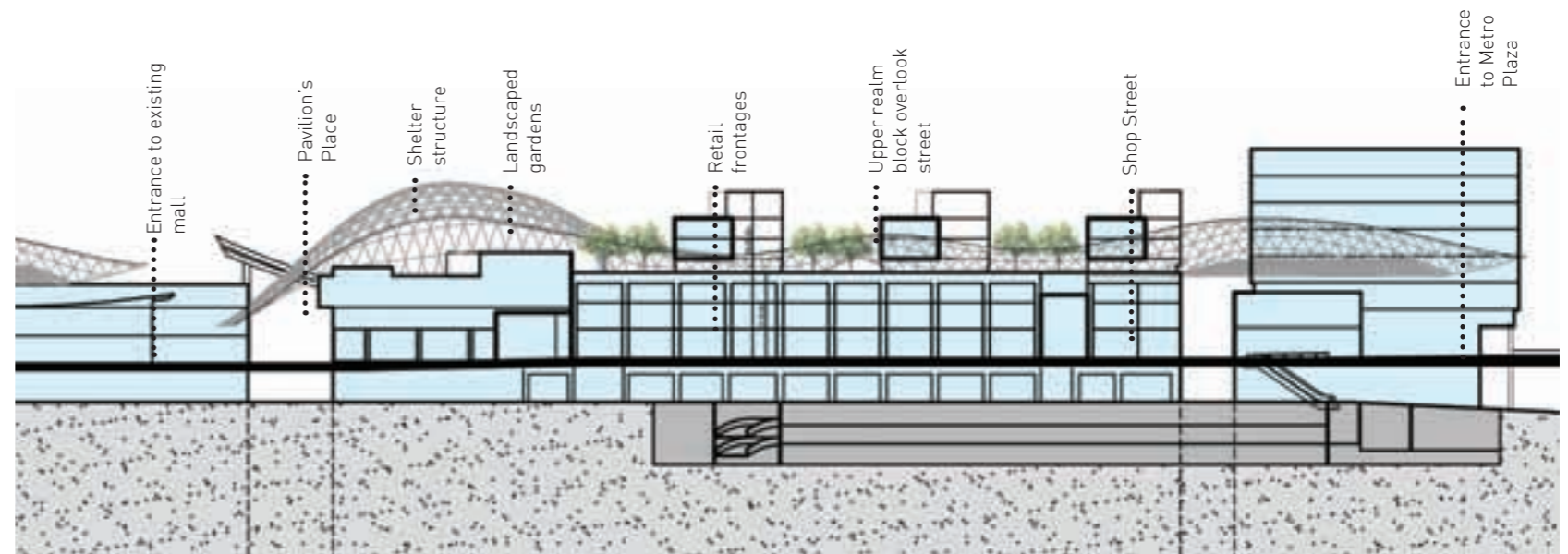
INDICATIVE STREET SECTION 1



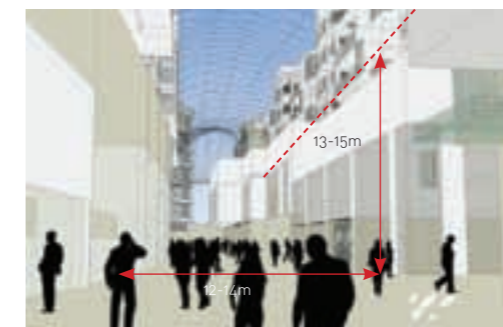
PLAN OF CENTRAL STREET 1:1000



SECTION THROUGH CENTRAL STREET



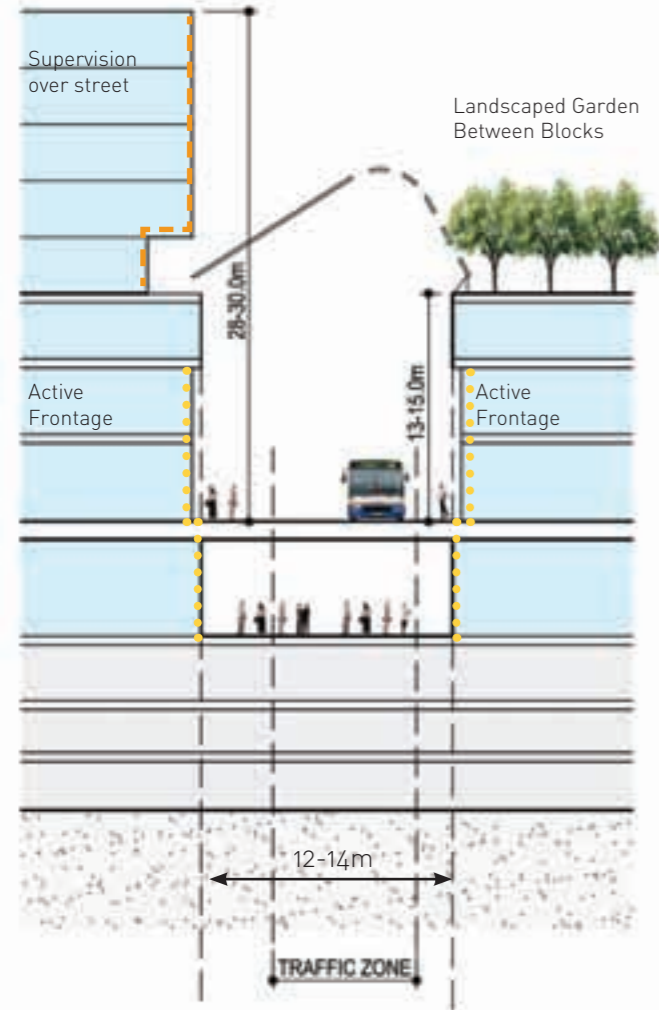
INDICATIVE VIEW IMAGE INDICATING CHARACTER AND SCALE OF STREET



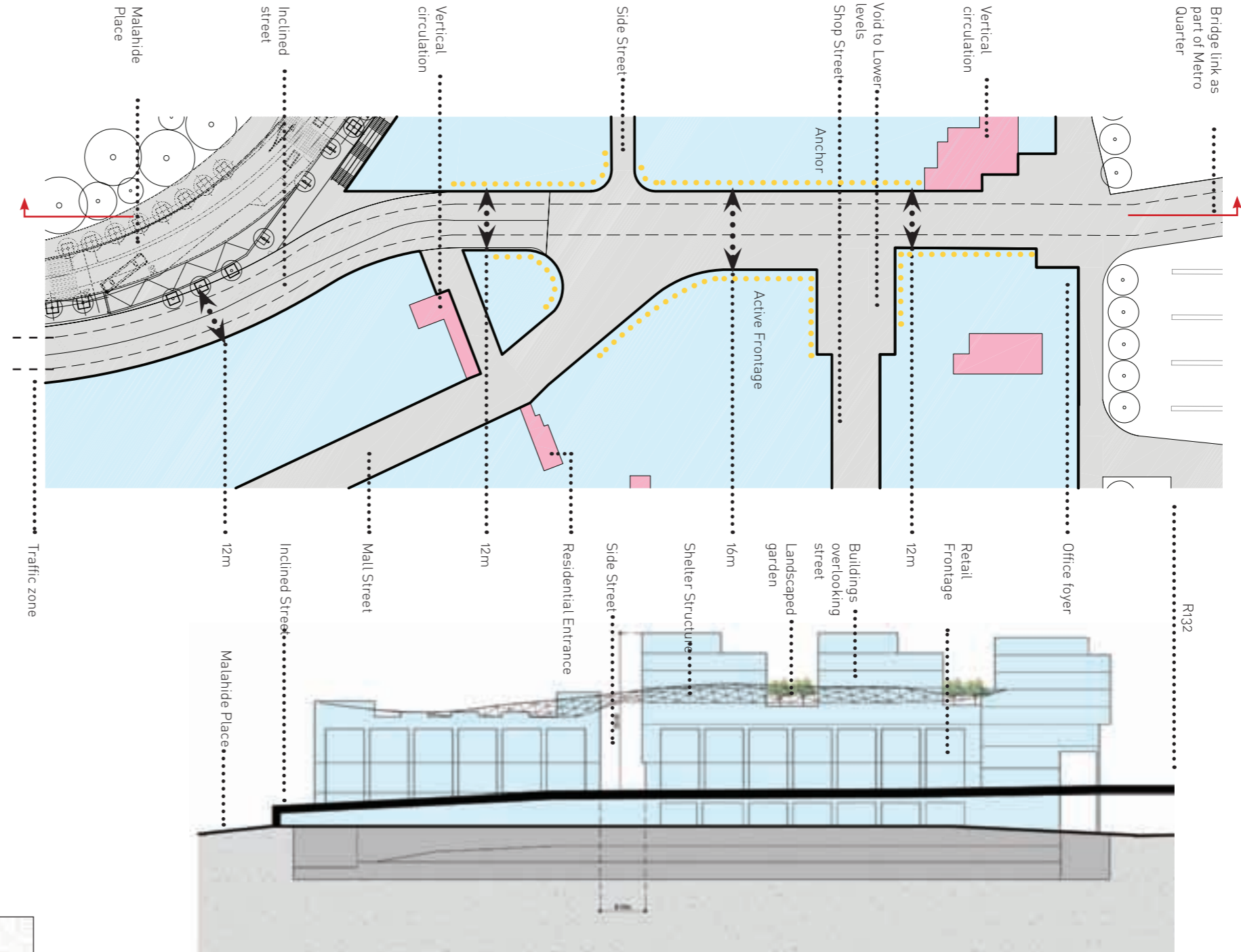
CENTRAL STREET FROM PAVILIONS PLACE TO METRO QUARTER

STREET HIERARCHY
PRIMARY STREET: THE RISE

INDICATIVE STREET SECTION 2



PRIMARY ROUTE WITH VEHICULAR
ACCESS STREET - THE RISE

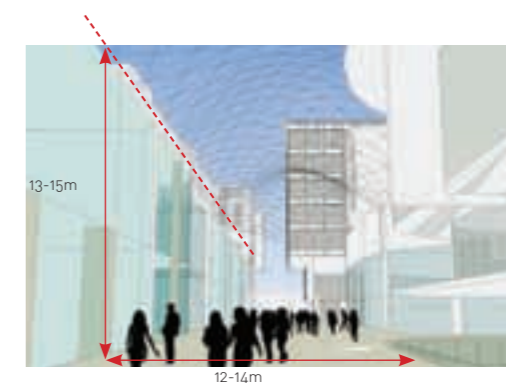


PLAN OF THE RISE 1:1000

SECTION THROUGH THE RISE 1:1000



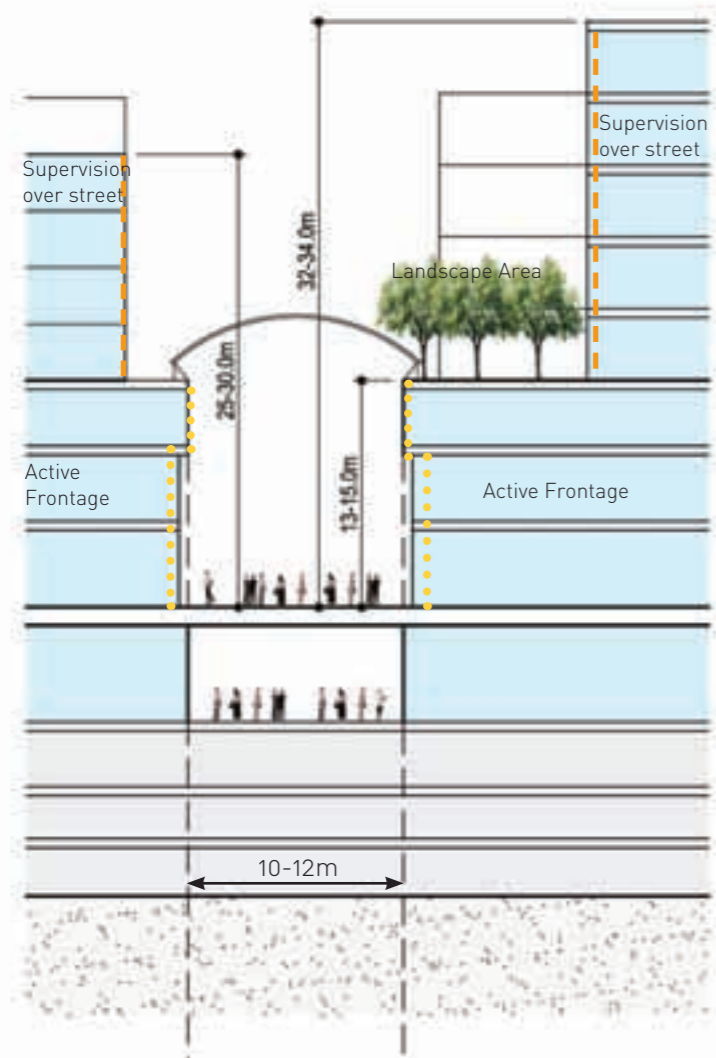
IMAGE INDICATING CHARACTER AND SCALE OF THE RISE



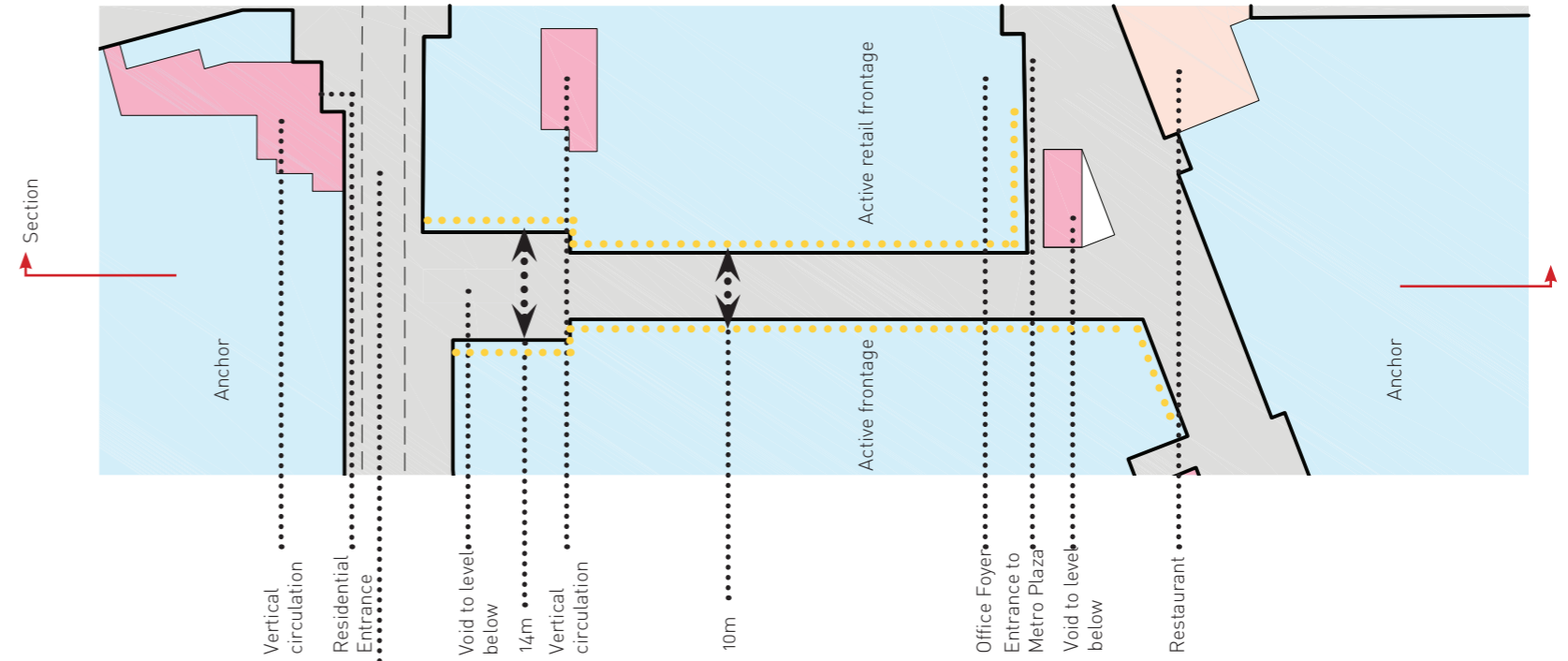
THE RISE

STREET HIERARCHY
SHOP STREET AND MALL STREET

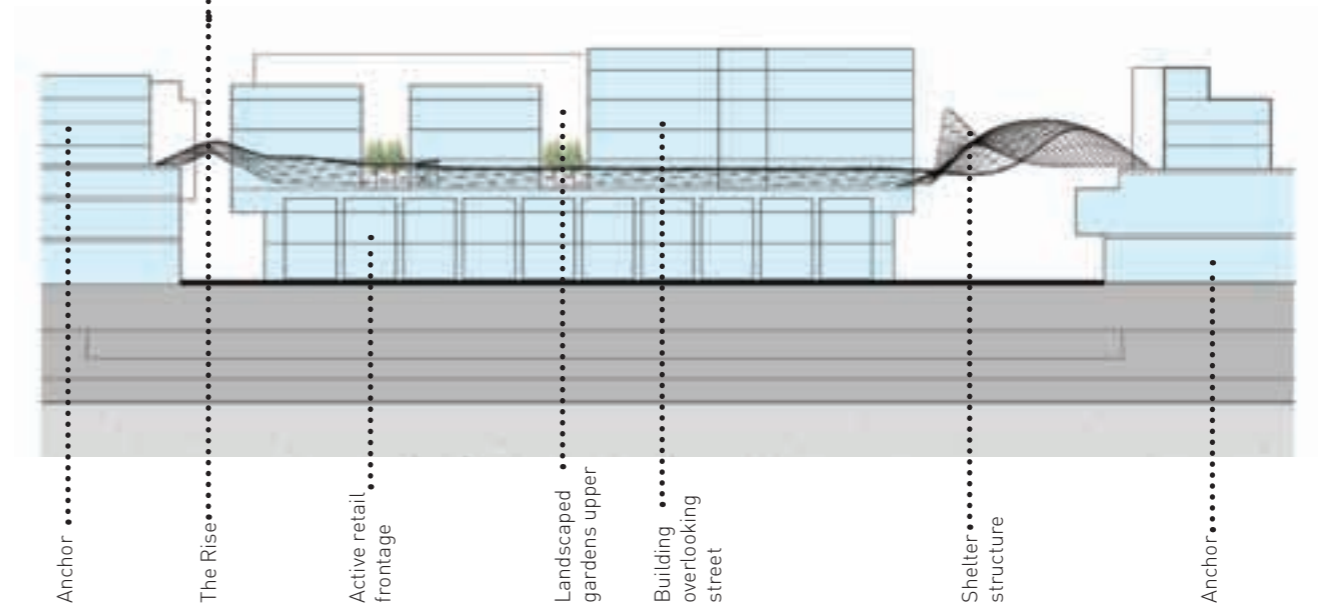
INDICATIVE STREET SECTION 3



SECONDARY STREET - SHOP STREET, MALL STREET



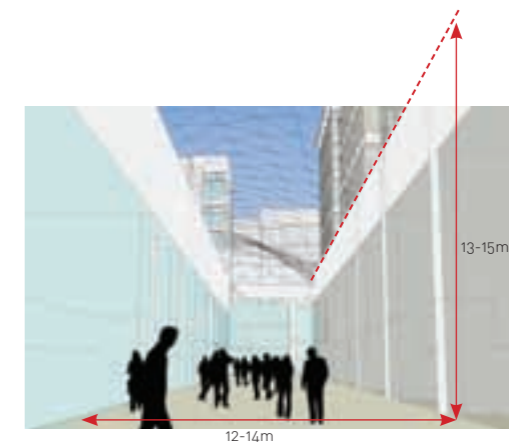
PLAN OF SHOP STREET



SECTION THROUGH SHOP STREET



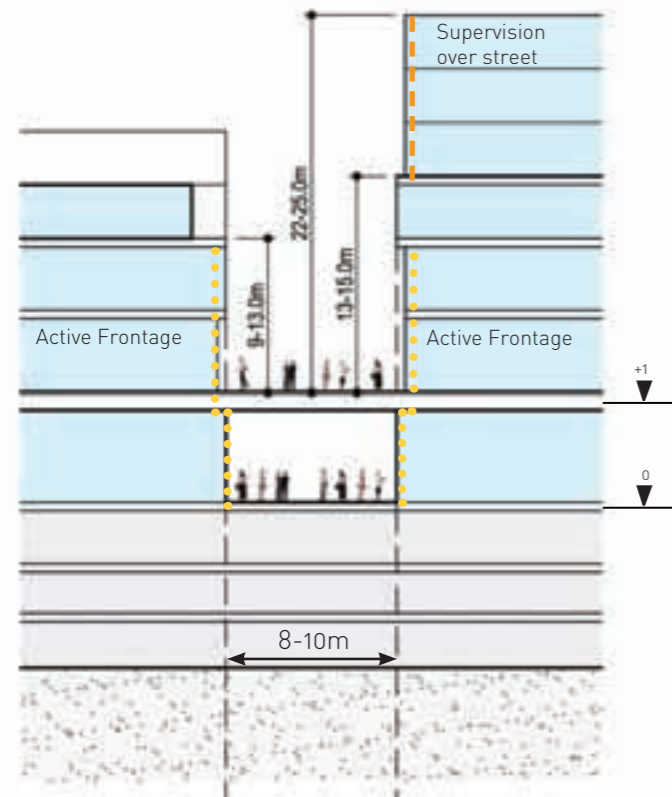
IMAGE INDICATING INDICATING CHARACTER AND SCALE OF STREET



SEQUENCE THROUGH SHOP STREET

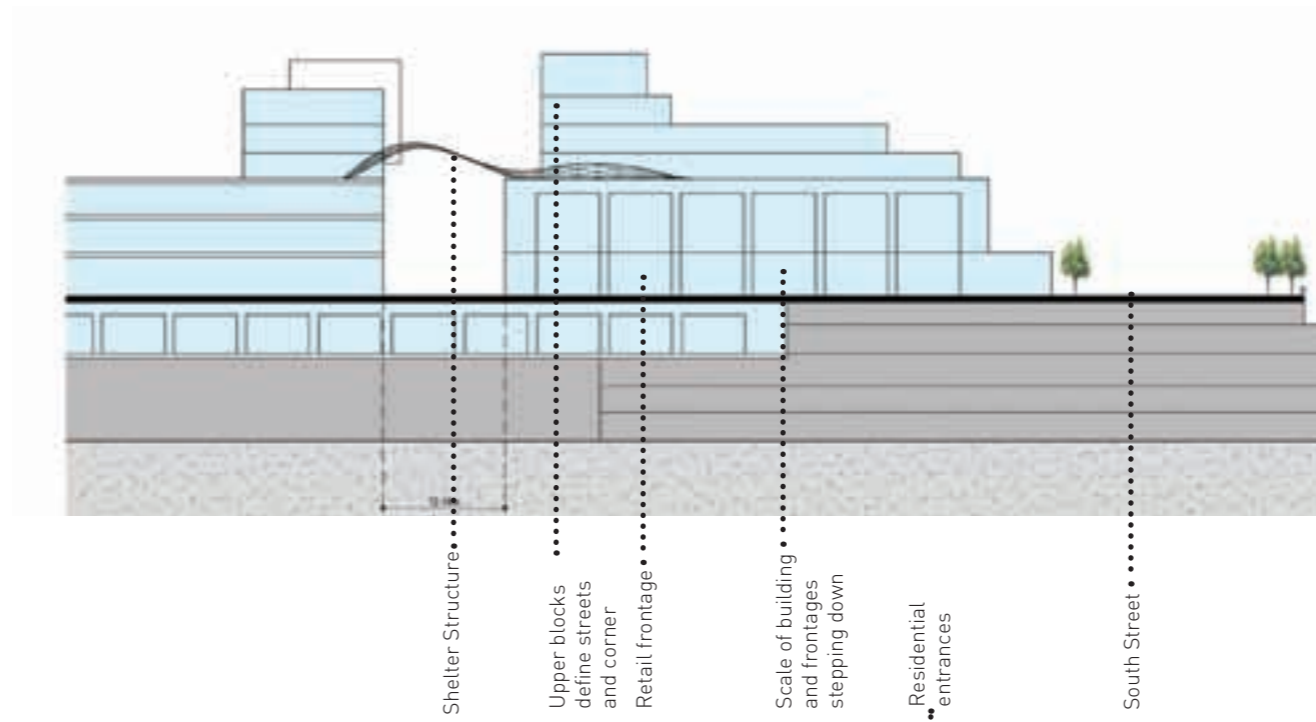
STREET HIERARCHY
SIDE STREET

INDICATIVE STREET SECTION 4

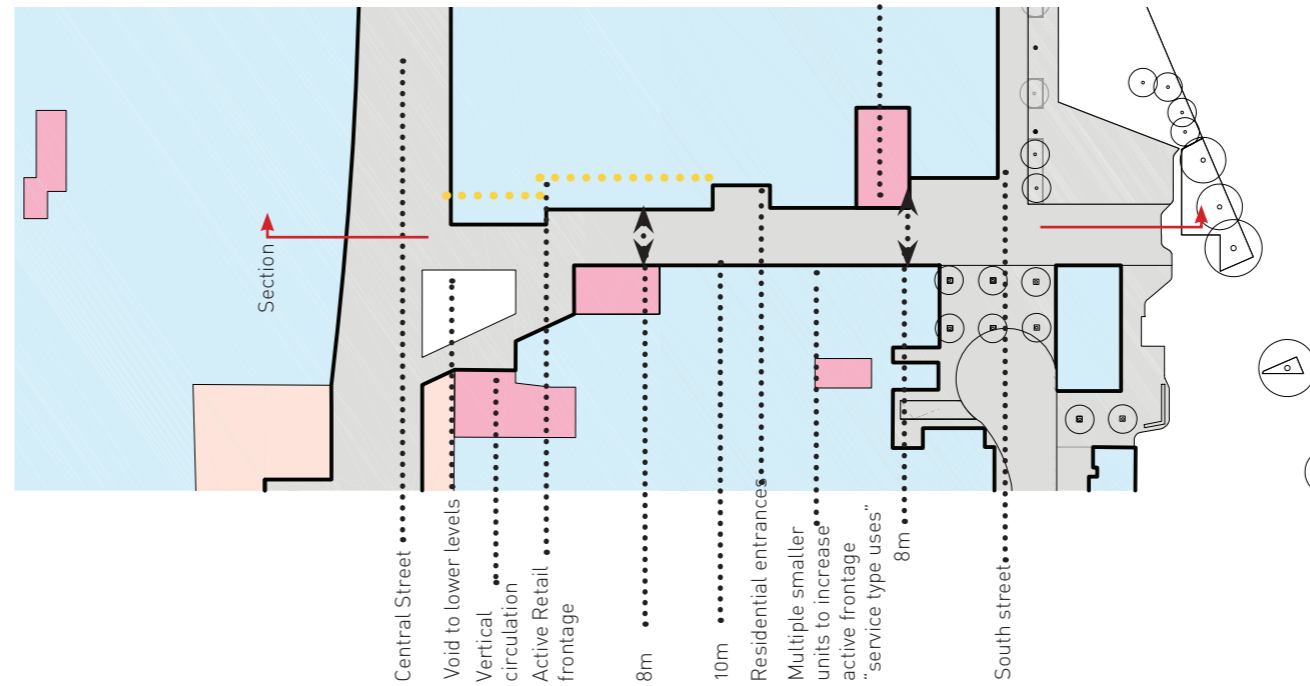


SIDE STREET - SOUTH LINK STREET, NORTH LINK STREET

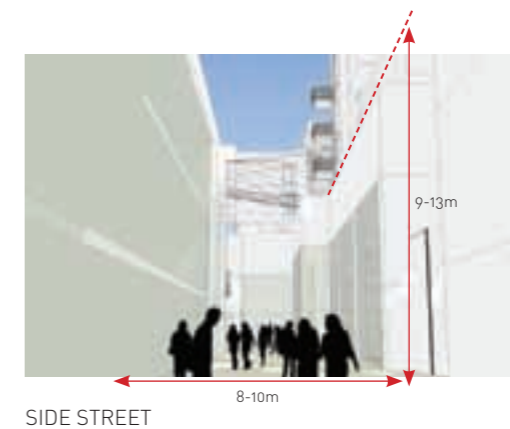
SECTION THROUGH SIDE STREET (SOUTH)



PLAN OF SIDE STREET (SOUTH)



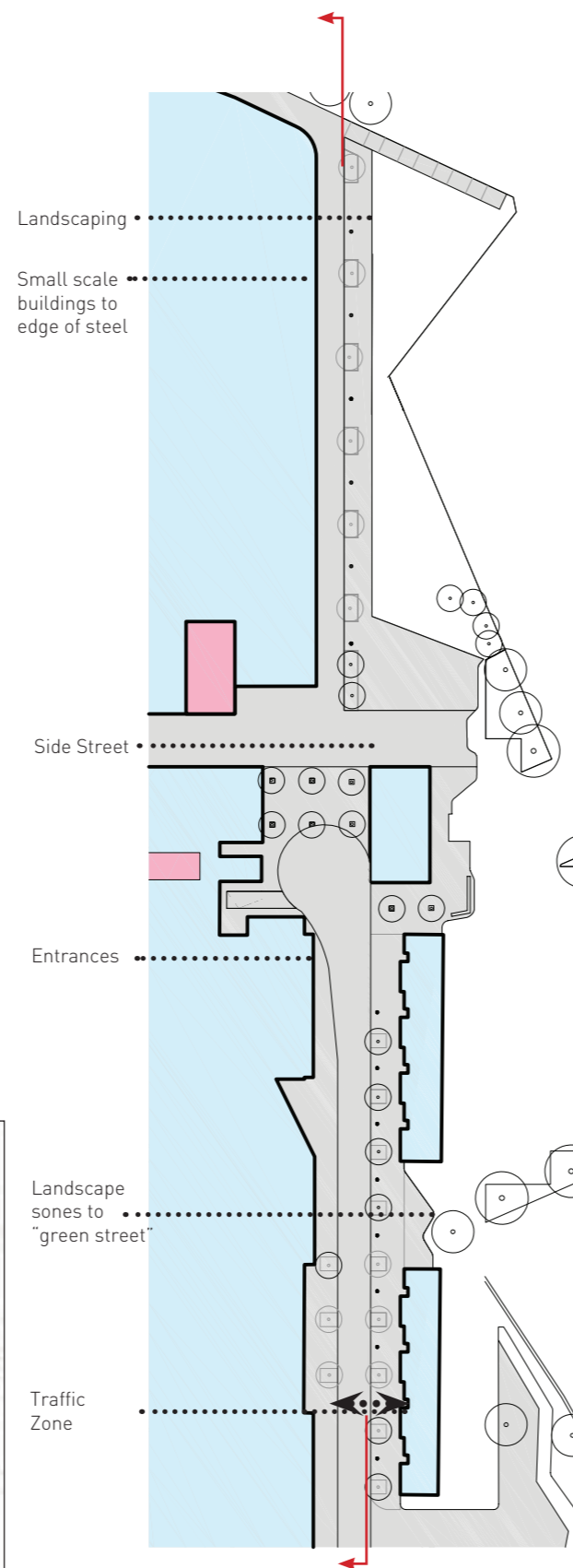
INDICATIVE VIEW - IMAGE INDICATING CHARACTER AND SCALE OF SIDE STREET CONNECTIONS TO CENTRAL STREET



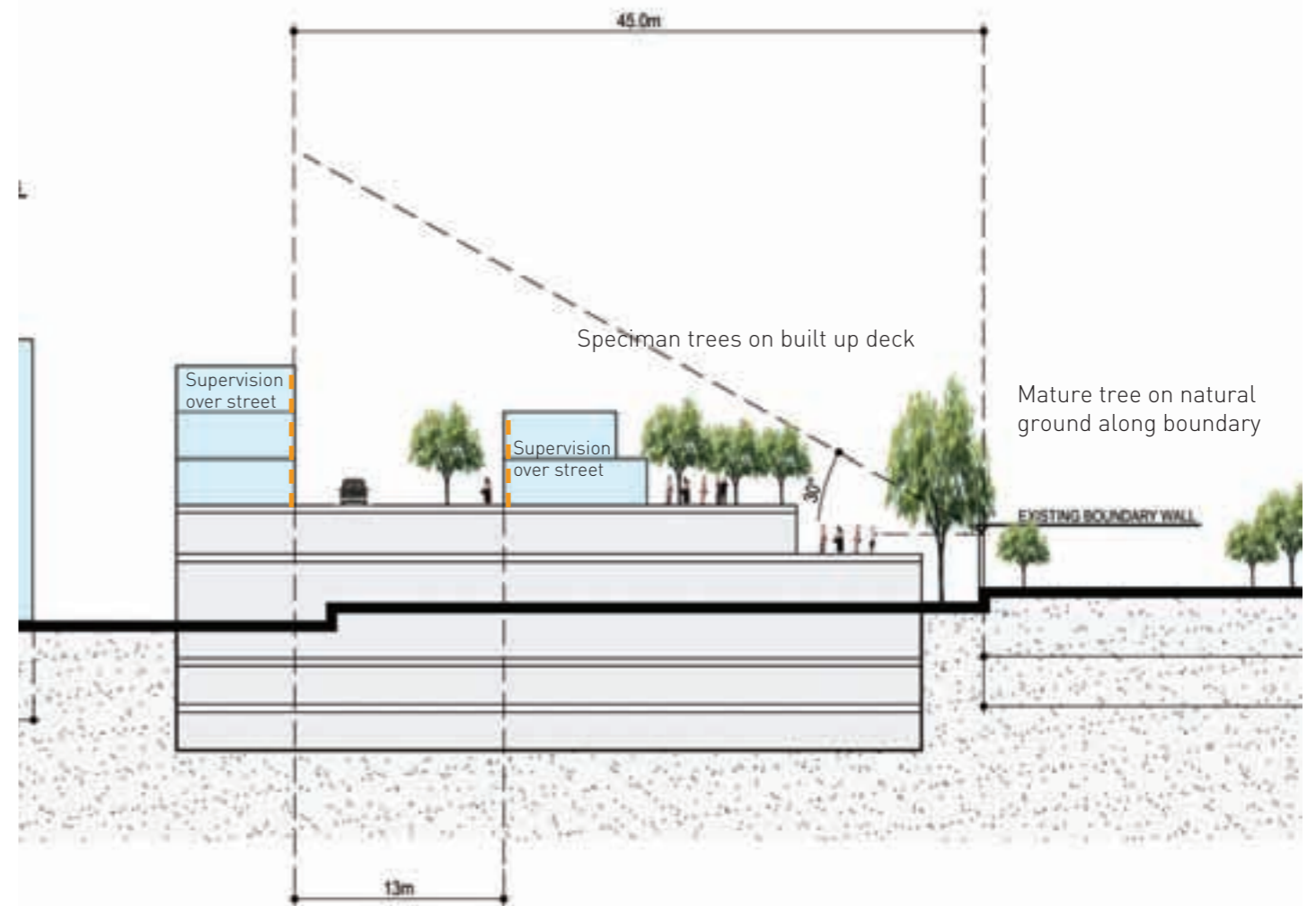
SIDE STREET

STREET HIERARCHY
RESIDENTIAL STREET – SOUTH STREET

PLAN OF RESIDENTIAL STREET 1:1000

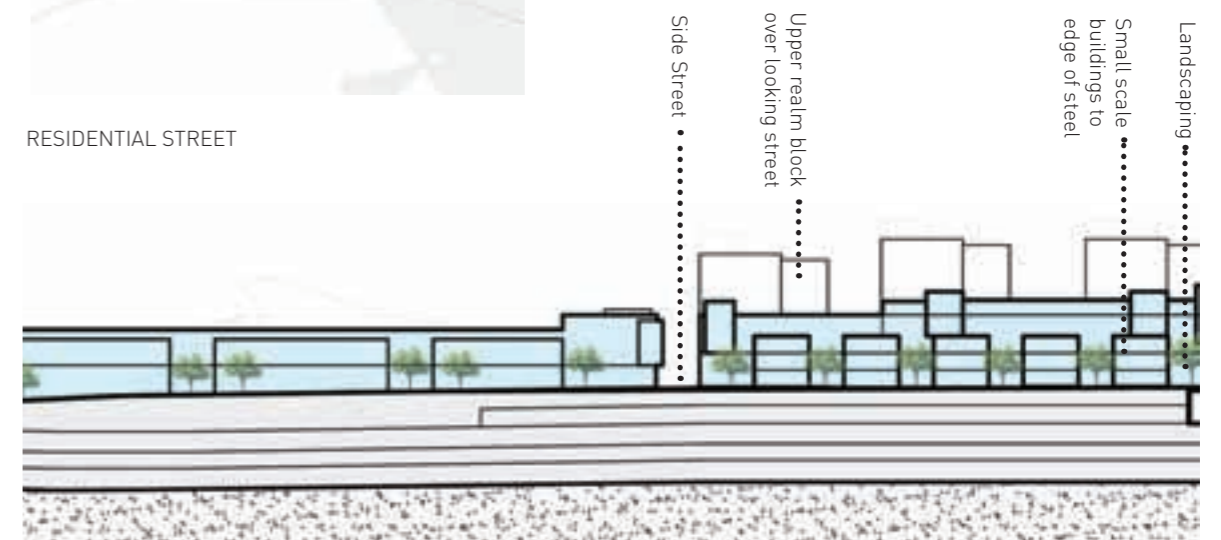


INDICATIVE SECTION 5



RESIDENTIAL STREET

SECTION THROUGH RESIDENTIAL STREET 1:1250



6.7 Open Space

6.7.1 PUBLIC SPACES/ACTIVITY NODES

Places for People

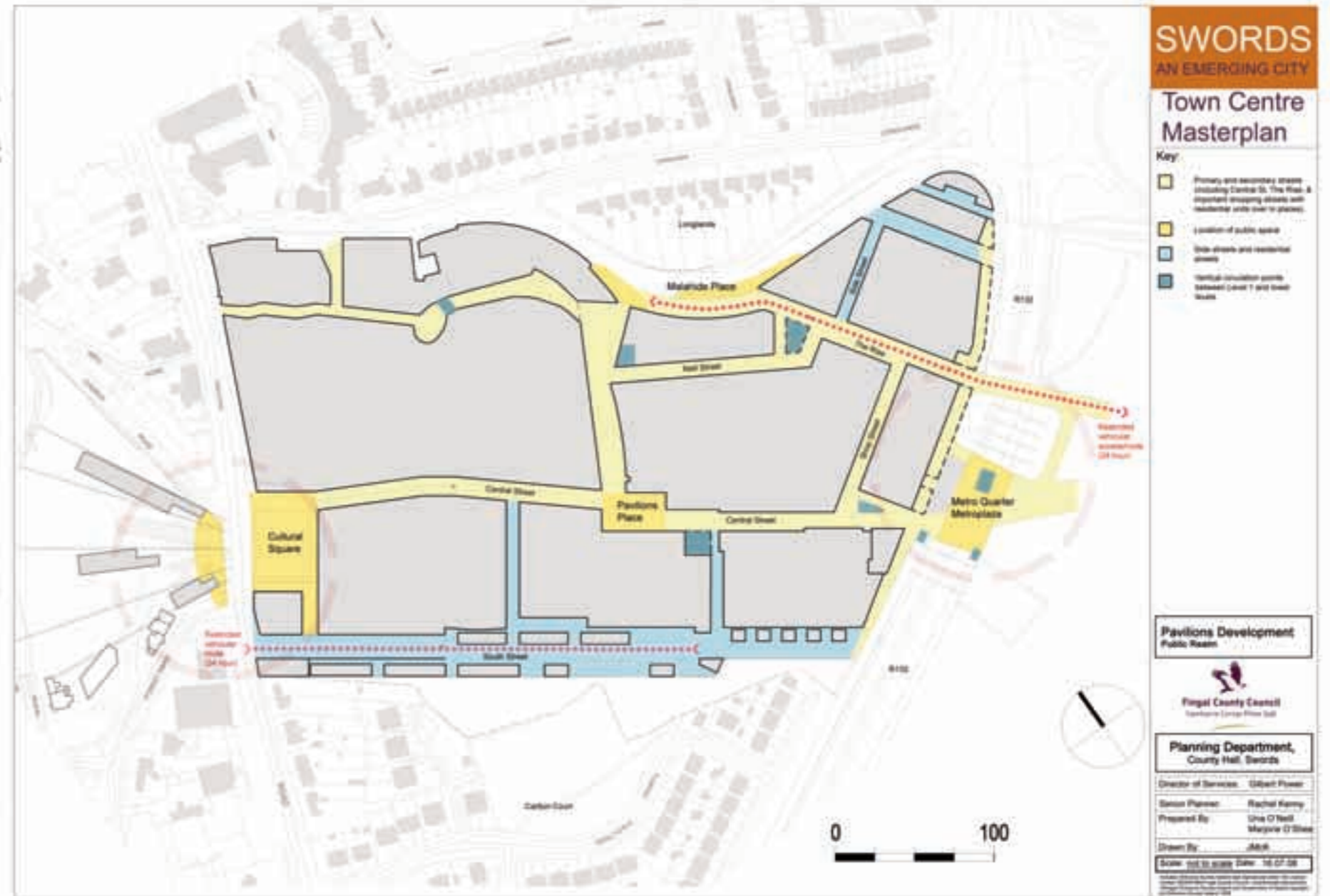
Objectives:

- To provide for a network of high quality public spaces and urban squares/civic spaces within the Pavilions development area linking with the Cultural Quarter and Metro Quarter.
- To ensure that the public realm is characterised by a high standard of design, quality finishes and treatments, including hard and soft landscaping.
- To provide for a number of 'landmark' buildings to give a 'sense of place' and punctuate key 'places' within the area.

Within the proposed development of the Pavilions site, new urban spaces are required to interconnect with the proposed street network, to support vibrancy and activity and add interest to the area. Significant focal nodes will be created at the interfaces with existing Pavilions building ('Pavilions Place'), at Malahide Road ('Malahide Place'), at the interface with the Metro Quarter, and at Cultural Quarter/Dublin Street interface [see map *Public Realm*].

The new public realm will extend from the Cultural Quarter to the Metro Quarter. The new public realm will be planned over 2 levels:

1. At existing ground level (level 0) at the Cultural Quarter (to the west), rising within the Pavilions site toward the existing Pavilions building
2. At upper ground level (level +1) east of the Pavilions building towards the Metro Plaza and linking with the Barrysparks land.



The following key public spaces will be developed in accordance with specific urban design objectives and parameters.

PUBLIC REALM URBAN DESIGN PARAMETERS AND OBJECTIVES:

Pavilions Place

CHARACTER

- Pavilions Place shall be a 24 hour major meeting and event space located along the primary pedestrian thoroughfare, Central Street, at the interface with the existing internal mall of the Pavilions Shopping Centre.
- Pavilions Place shall be a significant meeting and event place, measuring a minimum of 40m by 20m clear.
- Ground floor active frontages shall address Pavilions Place making it a vibrant and attractive space. Active frontages to be considered will include uses such as cafes, bars, kiosks and restaurants.

DESIGN

- In terms of urban quality, this key place shall comprise quality public seating and lighting and be coherent in the quality of surface finishes.
- It shall have casual dining areas, meeting seating spaces, small scale services. Located adjacent to this space shall be a vertical circulation core.
- Soft and hard landscaping shall be incorporated to provide a distinct character for this space.
- This urban space shall have a shelter structure which will be modulated and profiled to express the importance of the space and allow for extended use and activity throughout the day and evening.

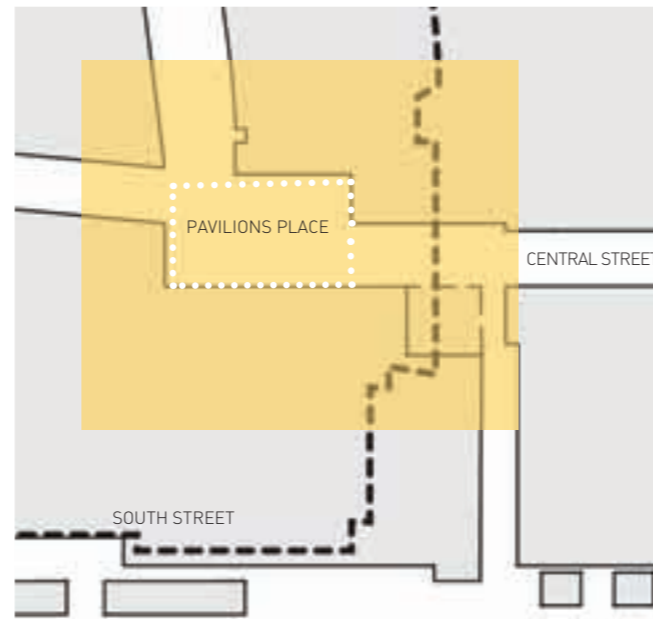


COMPUTER GENERATED IMAGE INDICATING THE CHARACTER AND SCALE OF PAVILIONS PLACE

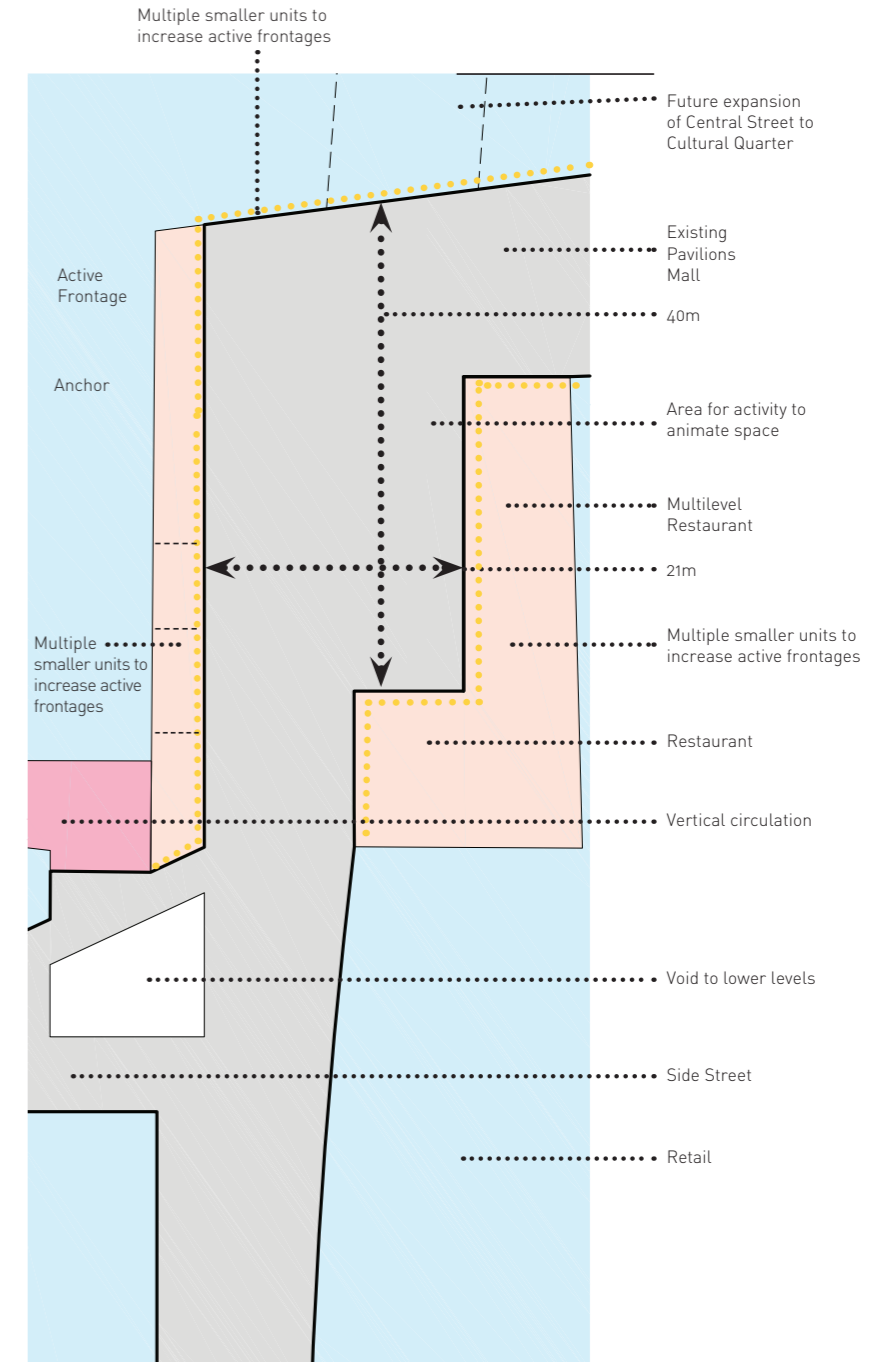


COMPUTER GENERATED IMAGE INDICATING THE NIGHTTIME CHARACTER OF PAVILIONS PLACE

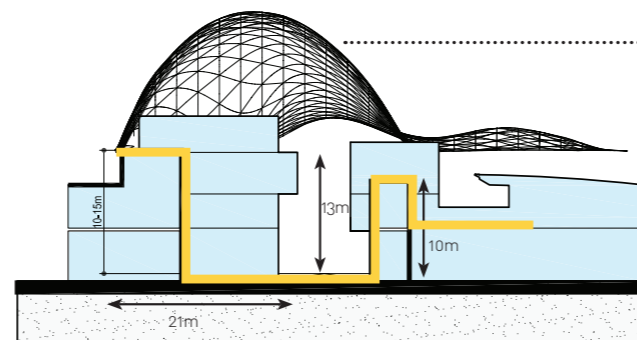
PUBLIC REALM – PAVILIONS PLACE



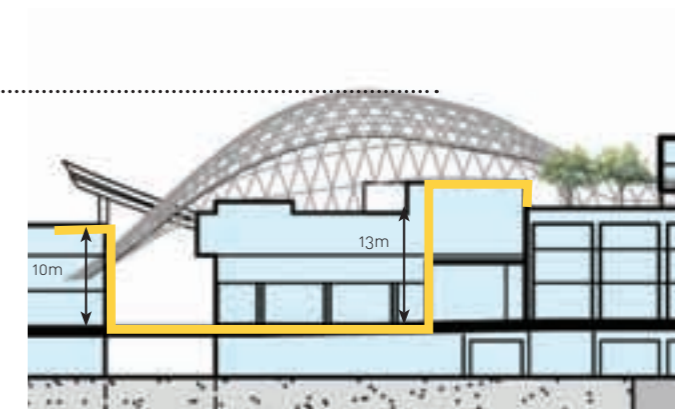
PAVILIONS PLACE



PAVILIONS PLACE PLAN, SCALE 1:500



INDICATIVE CROSS SECTION THROUGH PAVILIONS PLACE



INDICATIVE LONG SECTION THROUGH PAVILIONS PLACE

Cultural Quarter and Metro Quarter

Central Street will commence at the public Cultural Square, traversing through Pavilions Place and terminating at the public square of the Metro Plaza, where it will link in with Upper Street to the east of the R132, within the Barrysparks lands.

Malahide Place

This will be located at the intersection with the Malahide Road and entrance to the Pavilions scheme. This area will be a multi level space which will incorporate parts of the inclined street, The Rise, and incorporate a green corridor between the R132 and the Main Street.

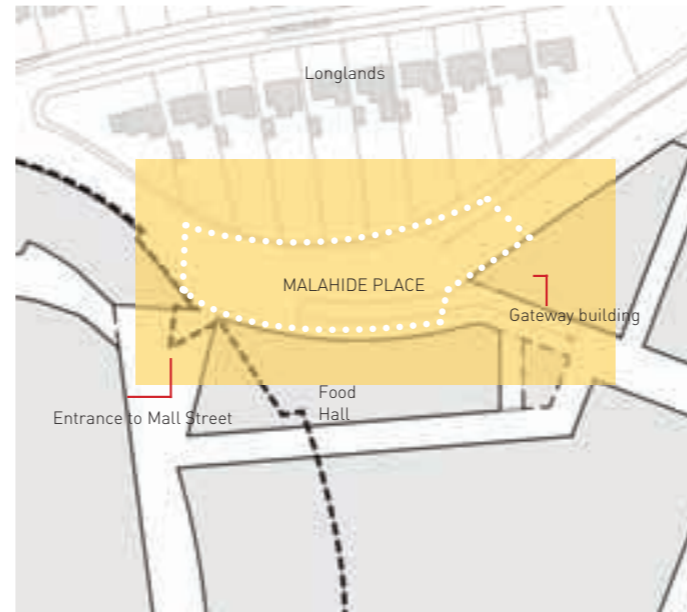
CHARACTER

- Malahide Place shall be a well defined and landscaped urban space, located at the pedestrian and bus entry point to the Pavilions development area from the Malahide Road. This space will be characterised by various forms of movement including cars, buses, cyclists and pedestrians
- The space shall include a uniform surface treatment, to create a sense of place, while rationalising the various forms of movement and traffic moving through this space.

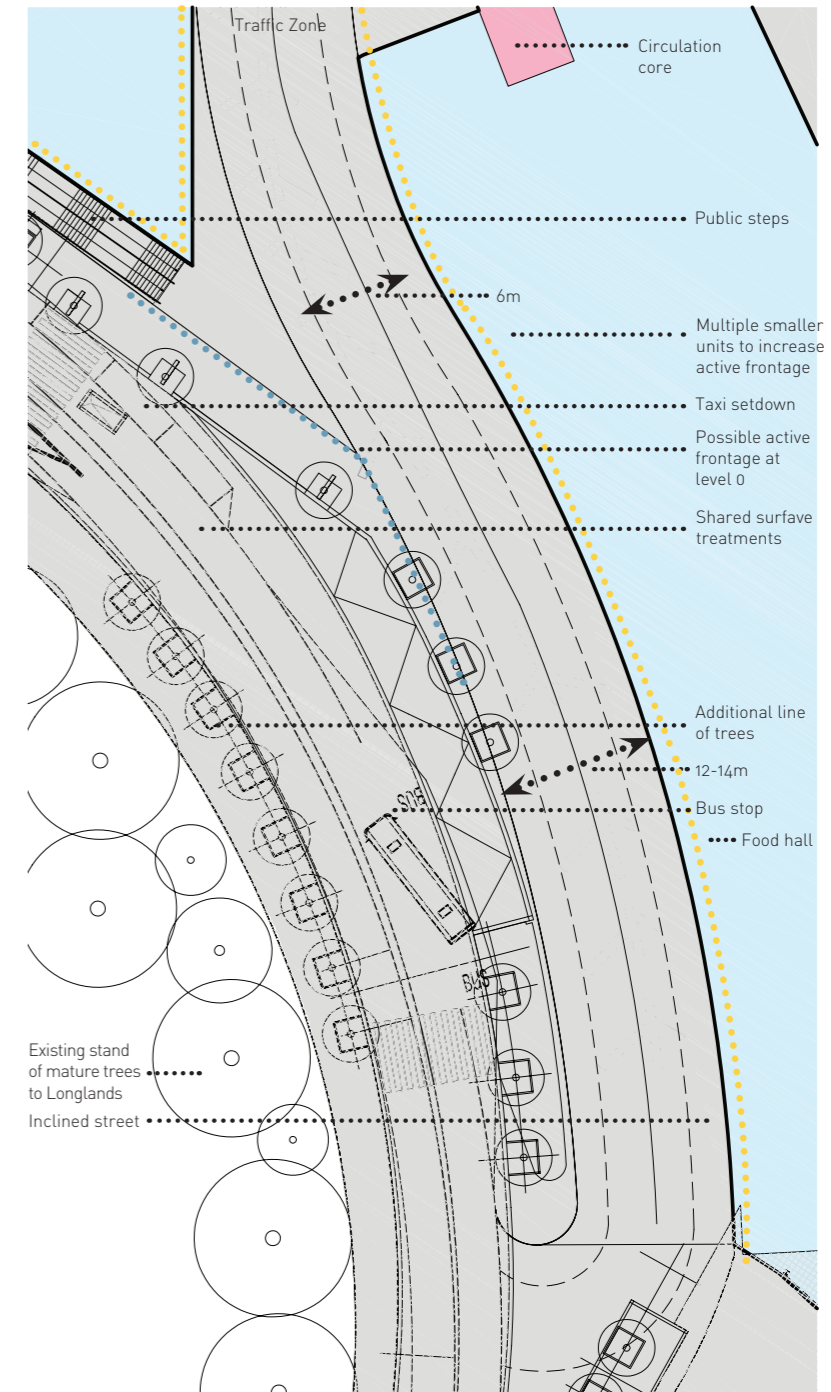
DESIGN

- The Pavilions development facade at Malahide Place shall have active frontages at ground level consisting of café and food related activities including a number of kiosk units along the inclined street to aid in the achievement of an animated and active façade throughout the day and into the evening.
- As part of the landscape strategy the planting of trees shall aid to further define zones of movement and re-enforce the existing mature stand of trees along the western edge of Malahide Road.
- It shall contain taxi and bus stop; the connection for bus and pedestrian movement up The Rise and a major entrance to Mall Street (at Level 0) while extending to the main service and parking entry further east along Malahide Road.

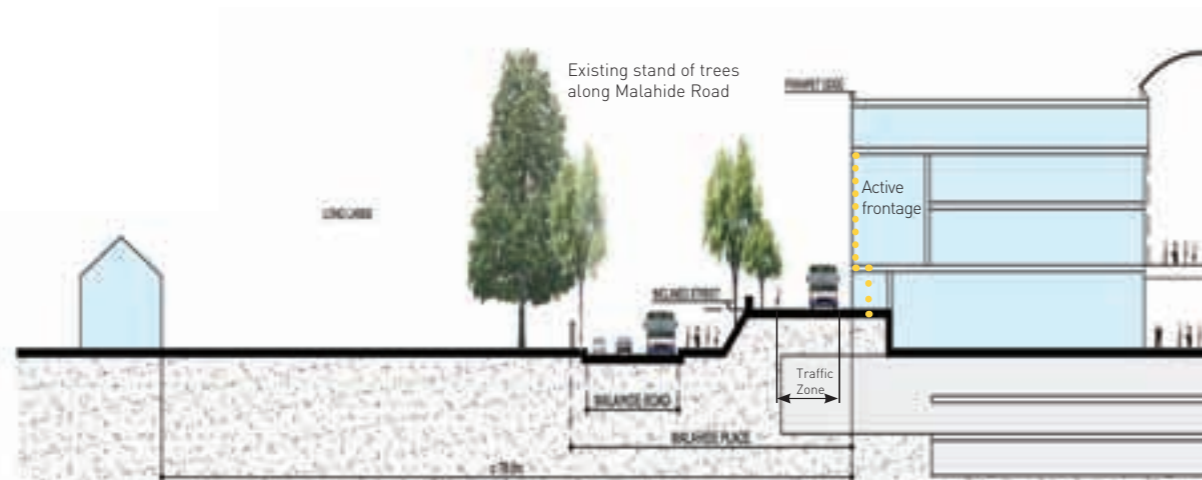
PUBLIC REALM - MALAHIDE PLACE



MALAHIDE PLACE



MALAHIDE PLACE PLAN, SCALE 1:500



INDICATIVE SECTION THROUGH 'CENTRAL STREET'



COMPUTER GENERATED IMAGE INDICATING THE NIGHTIME CHARACTER OF MALAHIDE PLACE



COMPUTER GENERATED IMAGE INDICATING THE CHARACTER AND SCALE OF MALAHIDE PLACE

6.7.2 LANDSCAPE STRATEGY

The overall Landscape Strategy [see map Landscape Strategy] for the area is prescribed by several essential factors:

- **Site Context**
 - Proximity to the existing Town Centre.
 - Access to the adjacent sites and parkland – Barrysparks to the East of R132 and future connections to Ward River Valley Park.
- **Route-ways and permeability**
 - Development of additional routes and access points through the proposed development and along the adjoining areas.
- **Strategic Transport Corridor (R132) and Metro North**
 - Response to engineering requirements; such as the development and alteration of the R132 and Metro North.
 - Response to Metro Plaza.
- **Landscape Corridors bounding the edge of the site area**
 - R132 Parkway – Extension and development.
 - Dublin Road.
 - Malahide Road.
 - Southern Boundary.
- **Parkway Experience and Arrival**
 - Visual appearance and arrival sequence by cars along the R132.
 - Arrival sequence for Metro North users.
- **Pedestrian Comfort and Usability**
 - Where the road network comes in contact with the pedestrian realm, it is essential that the landscape treatment responds in such a way that the public space is adapted or treated in a manner that protects the users of the development, while avoiding an unnecessary interruption to the vehicular flow.

The Landscape Strategy in relation to the Pavilions Development area, R132 Edge, Malahide Edge, and Southern Edge, is as follows:

- Create a series of landscaped corridors between R132 and Main St./Dublin Road along the Malahide Road; and along the Southern Edge with Carlton Court.
- Create a series of landscape character areas within the landscape framework.
- The treatment of the R132 edge is proposed as a continuation of a 'Parkway' route including substantial landscaping and tree planting. It is proposed to extend this treatment onto the eastern end of the Malahide Road.
- A landscape corridor is proposed along the southern boundary to improve the definition of existing landscape spaces within Carlton Court and improve the quality of the existing edge parking areas. (See Section Edge Conditions: Southern Edge).

- A landscaped corridor and buffer is proposed along the Malahide Road, within the new multi-level public space incorporating new tree planting to compliment the existing stand of mature trees to the south of Longlands.
- The Masterplan will make a connection to the Ward River Valley Park at the Cultural Square, linking this valuable amenity back to Swords.
- A distinct landscape intervention is needed for each of the public spaces within the area.

Residential

- A network of connected landscaped gardens shall be located at residential level.
- There shall be a mix of private and communal amenity spaces.



INTERNAL LANDSCAPE TREATMENT

Nodes and Markers:

While the streets shall remain simple and uncluttered at floor level, several large landscape interventions will be targeted as part of the development of meeting points and development of a clear street identity. These shall be in the form of large specimen trees grown between floors to emerge at first floor level, which shall be seen as nodal points along the street level.

Event Space

Event space is provided at the confluence of the existing shopping centre development and the extension of the Central Street – Pavilions Place. When providing for such a space, it is essential that alternative active uses are considered for such an adaptable stage area. This shall facilitate the interim use of the space as a part of the integrated Town Centre experience, perhaps as part of a combined dining function and public seating space.

Civic Values

A series of usable public spaces is developed along the logical structure of the street layout. This series allows for the development of a sequence of squares and functional nodes that run from the Metro entrance level, through the Central Street where Pavilions Place is located, and continuing West as far as the future Cultural Square development.



ELEMENTS, MATERIALS AND EXPRESSION

STREET: SPACE, APPEARANCE AND USAGE

- Artwork – A series of artwork and bespoke urban elements, such as seating, lighting and paving patterns will be used at appropriate locations.
- Lighting – Lighting shall be used as an integral design element while also acting as a functional safety installation.
- Uncluttered and Uninterrupted – Provision of an unfussy floor plane that responds to the street type requirements, while at the same time providing seating and relaxation/outdoor dining functions. In this manner, patterns of usage are considered and catered for.
- Vista and Termination – Consideration will be given to the provision of a green terminus in the form of tree nodes and views to green areas to the boundary.



MATERIAL CONTINUITY:

The continuation of a uniform quality paving material shall provide an apparent extension and integration of the future development into the existing Town Centre. A key factor for the integration of the development shall be the similarity/ matching of the external and internal landscape element.

A selection of lighting shall form a continuous appearance around the boundary of the site, while also marking the primary street connections.

Specimen Tree selection:

The continuity of tree form and appearance shall provide a legible boundary along the R132 which shall curve around and onto Malahide Road. Tree selection at the Metro Plaza will be more urban in character (see section 7.0 Metro Quarter).

UPPER REALM RESIDENTIAL LANDSCAPE STRATEGY

- To achieve the appearance of a balanced and proportional landscape treatment to roof terrace levels, it is critical that there is a sufficient layering of planted material and build-up to allow for the installation of large tree species and associated planting.
- To achieve the build-up and planting depth for the upper realm, landscape will be carefully coordinated with the engineering requirements to take into consideration the loading, drainage and irrigation issues. The availability of an interstitial layer at below the residential levels allows for the development of a much more dynamic and proportioned landscape treatment than a standard 'loaded' roof terrace.
- The residential roof plan will allow the opportunity for a sequence of interconnected spaces. It is intended that these landscaped areas are visible from street level and add to the character and animation of the streetscape.
- A selection of spatial requirements will be included within the confines of the upper realm levels. Semi public play area, barbeque and outdoor eating areas, green space and shelter/screen planting are combined to create an animated living environment.

SOUTHERN BOUNDARY – MATERIAL EXTENSION:

South Street shall be treated as a connected extension of the Central Street; while distinctly changing the character from that of a primary pedestrian thoroughfare, to that of a greened residential extension of the Town Centre.

This street shall provide a 3m wide continuous tree planting screen, increasing the buffer provided by the adjoining Carlton Court public open space. Large specimen tree planting at the ground floor level along the southern boundary shall be required to reduce the impact of the development on the existing residents to the south. The trees shall be semi-mature, with an initial height of 6m. These trees shall be spaced and inter-planted to create a continuous screen above the current boundary wall. The planting shall be wholly at ground level and into the natural ground, with suitable root space to allow trees grow to maturity.

While South Street will provide a residential component to level +1 Masterplan of the scheme, the landscape shall respond to this by providing usable areas such as the Southern square where the residential and Town Centre streets meet. These shall appear in the form of public and semi public outdoor play areas for children, outdoor 'Trim Trails' for adults and children alike, and the development of a pedestrian-friendly street.

7.0 Metro Quarter

Introduction

The Metro Quarter is the proposed new gateway to Swords and is built around the opportunity created by the arrival of Metro North. Swords Town Centre will be served by a Metro North stop located centrally on the R132 in proximity to the Pavilions and Barrysparks development areas. The Metro Quarter will permit and promote multi-modal connections at Swords Metro Stop and will create a hub for movement within Swords as well as providing rapid access to and from the airport and Dublin city to the south.

The Metro Quarter will be the catalyst for growing Swords identity in the 21st Century. This presents a unique opportunity to demonstrate on a national and European stage the ambitions for the town, its people and its future. The development of the Metro Quarter recognizes this importance and will become established as a vital orientation node for those arriving in Swords.

Urban design guidance and objectives are put forward within this Masterplan, which will address the key issues of: movement, public realm, height and massing, and landscaping.



7.1 Characteristics of Metro Quarter

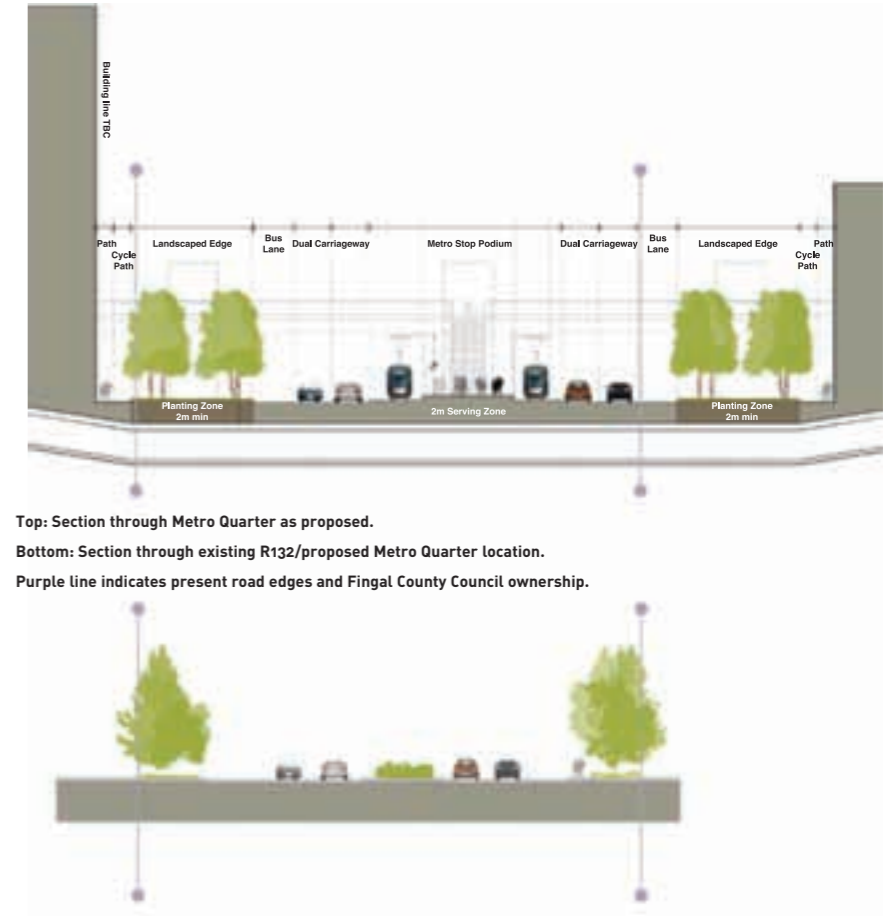
The Metro Quarter comprises the following key elements:

- A high quality well designed public space, 'Metro Plaza' (+7.5m) above the Swords Metro Stop and the R132.
- A well designed architectural bridge over the R132 linking with the Metro Plaza and Swords Metro Stop. This bridge will accommodate priority bus services and pedestrian/cyclist movements and public transport over the R132 and ensure connectivity between Town Centre/MC zoned lands east and west of the R132.
- A vehicular road network under the R132/Metro North providing access to underground car parks east and west of the R132. This network is known as 'Metro Box'* and access is via ramps directly from the R132.
- High quality, architecturally significant gateway buildings east and west of the Metro Stop and Metro Plaza.

7.2 R132 Characteristics

The Metro line and Swords Stop has been fixed in an optimal location, given the constraints of the alignment of the existing R132 and desire to provide a high standard of access to existing Swords Town Centre. This alignment generates the core setting out of the realigned R132 running either side of the Metro and fixes the Swords Stop platform position. The quality of the realigned R132 transport corridor has been enhanced to provide bus corridors, cycle lanes (integral to the bus lanes) and footpaths within an enhanced tree-lined landscape setting in both directions through Swords: the aim being to provide a high level of accessibility at this level to the multimodal transport interchange.

* Metro Box is an underground vehicular connection across the R132, associated ramps, and necessary realignment of the R132 locally, in order to provide for access to lands zoned 'Swords Town Centre' east and west of the R132, as shown in the Fingal Development Plan 2005 to 2011. This Metro Box plan was adopted under Part 8.



Top: Section through Metro Quarter as proposed.
Bottom: Section through existing R132/proposed Metro Quarter location.
Purple line indicates present road edges and Fingal County Council ownership.



7.3 Movement Strategy

The Metro Quarter is located around the Swords Metro Stop. Its extent runs along the R132 as far north as the Malahide roundabout and as far south as the future slip roads accessing the underground car parks and 'Metro Box'*. To the east and west, the Metro Quarter will abut the adjacent development areas of Pavilions and Barrysparks, however it will have an influence on all approaches into the quarter from these developments.

The Metro Quarter will be the multi modal (multi level) transport hub and focal point of movement to and from the town. An enhanced network of roads, cycle/footpaths and public transport provision is designed to feed this hub and promote reduced car dependency within the town as a whole. In addition to this, Metro Quarter fulfils a vital role in connecting seamlessly between the existing Town Centre and new development areas whilst overcoming the severance of the R132. The development either side of the R132 will facilitate the bridging over the roadway and metro line at the Metro Quarter. These bridging elements will be in the form of public streets and will permit a new dedicated bus route to be established, thus forming effective links between east and west areas of Swords.

Connectivity between the Metro Quarter and local/regional areas will be supported in the following way:

- Metro Stop located in the central median of the R132 providing access to Dublin City, Dublin Airport and the future development lands at Lissenhall.
- Bus stops located along the R132 will be retained for national, regional and local services.
- Future bus stops will be located along "The Rise" on the Metro Plaza level (+7.5m) serving local east west routes. This route may also accommodate taxis.
- Cycle routes and provision for bike parking will be located along R132. East-west cycle routes and parking will be provided at the Metro Plaza level.
- In order to maintain operational capacity, left in – left out access ramps will be provided to underground car parks in both directions along the R132.

Connectivity between the Metro Quarter and surrounding developments will be supported in the following way:

- At the Metro Plaza level, at-grade connections will be provided to "Central Street" and "The Rise" on the Pavilions side (see Section 6.0), as well as "Upper Street" and "Eastern Avenue" on the Barrysparks side (see Section 8.0).
- At the R132 level, at grade access will be provided into the Pavilions and Barrysparks developments to the east and west.
- Pedestrian footpaths will be located along the R132 and two pedestrian crossing points between the path and the Metro Stop will be located at each end of the platform.
- Vertical access between the R132 level and the Metro Plaza will be provided in three locations:
 - At the end of the metro platform there will be two escalators and one lift between the two levels.
 - On each side of the R132 there will be stair access along with a lift between the two levels.

* Metro Box is an underground vehicular connection across the R132, associated ramps, and necessary realignment of the R132 locally, in order to provide for access to lands zoned 'Swords Town Centre' east and west of the R132, as shown in the Fingal Development Plan 2005 to 2011. This Metro Box plan was adopted under Part 8.

Specific Local Objectives:

- The Rise and Western Avenue will be between 12 and 14 metres wide (subject to confirmation of detailed traffic engineering design requirements) and at a slope of less than 1:20 to accommodate buses in both directions, in addition to pedestrian and cycle movements.
- Dedicated cycle routes and cycle parking will be provided in both directions along R132 and east-west at Metro plaza level.
- Vertical circulation of high architectural quality will be provided on both sides of the R132 as well as at the Metro Platform to access the Metro Plaza.

Part XI Scheme currently on public display





Part XI Scheme currently on public display

7.4 Public Realm

A sense of place will reflect the quarter's status as the point of arrival and departure for Swords. The development around the Metro Stop will celebrate and exploit the new connectivity brought by increases in density and form.

The scale and standards of the public realm, plus the quality and type of the surrounding buildings will do much to reinforce the importance of this new place in Swords and mould the identity of Swords in Ireland and the wider world of international trade, culture and tourism.

The new public realm will be planned over two levels:

1. R132 level
2. Metro Plaza level (+7.5m)

Direct vertical circulation will be provided between these two levels in the form of stairs, lifts and escalators. The success of the Metro Quarter is dependent on having these connections in place and as such all vertical circulation elements should be of high architectural quality and be part of an integrated design solution.

The following key public spaces will be developed in accordance with specific urban design objectives and parameters.

- Metro plaza
- Metro platform
- R132 and Metro setting.

The urban design quality of the area will be set at an international level of excellence with a particular emphasis on the open spaces and landscape setting commensurate with the 'Emerging Green City' identity being forged in the FCC Strategic Vision for Swords.

7.4.1 METRO PLAZA

A public place will be formed around the transport interchange at the level of the new streets crossing over the R132 (+7.5m). This will be called Metro Plaza and is intended as the primary arrival and departure point from the town via public transport. Buildings will respond in their uses and appearance to this public space.

CHARACTER

- Metro Plaza will be a 24 hour major meeting space located at the crossing point of several key routes through the Pavilions and Barrysparks developments. It will be the Town Centre's key bridging point of the Metro line and the R132.
- The space will accommodate a major transport interchange, operating over 2 levels serving metro, bus, taxi, cyclist and car users.
- A point of orientation, the Plaza will be a busy and well used space which people will pass into and through. It will also be a meeting place and a space that will become a focus for minor events.
- Animation and activity may be provided through the introduction of a series of small kiosk type buildings located on the plaza, containing such uses as flower shops, newsagents, coffee take away etc. The number and location of these kiosks must be well considered and not serve to clutter the place.

DESIGN

- The main function of the space is to access public transport services. Direct access to both the Metro and all bus links must be facilitated directly from the plaza.
- The buildings surrounding the Metro Plaza will be significant in scale and presence. They will have active uses to upper ground level (+7.5m) which will directly address the plaza.
- The buildings will include an arcade/collonade treatment at level +1 along the R132 to allow movement along this edge between the bus link and shopping streets of the adjoining lands until the arrival of the Metro Plaza.
- The adverse effects of noise and pollution from traffic should be minimized through the introduction of measures that might include walls of running water, bands of tree planting and/or acoustic shelters.
- The space, its edges and enclosure will have a civic presence. Façades and surface treatments including lighting and furniture will reflect the civic quality.
- Elements of hard and soft landscaping will be integrated into this urban space to ensure the continuity of the 'parkway' character of the R132 and to act as screens to control environmental conditions.
- A 'branding and public art' programme is required that sets out the range and location of possible interventions by artists working in the public domain.
- Signage, in the form of custom designed illuminated freestanding pillars, will orientate and inform visitors to the Quarter as part of an overall

branding strategy for the Town Centre.

- Illuminated glazed canopies, and Metro and bus signage will be provided to mark the locations of access points to the metro and bus stops below the plaza.

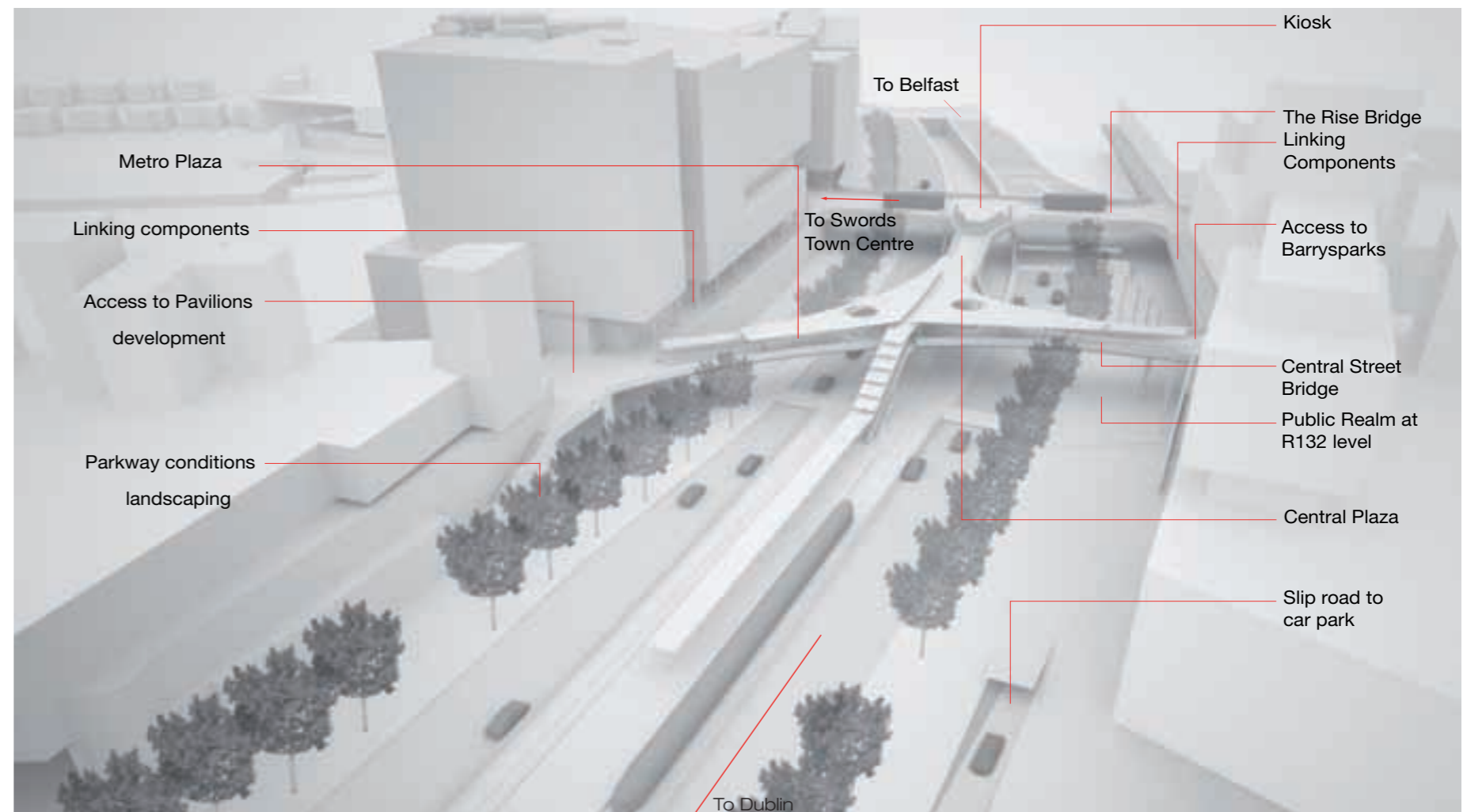
7.4.2 METRO PLATFORM

CHARACTER

- Measuring 10m x 100m the platform's main role will be to wait for and access the Metro as well as buying metro tickets.
- The space shall include a uniform surface treatment while rationalising various forms of movement.

DESIGN

- The platform will have a sheltering canopy and screens to protect it from the elements and to discourage crossing at any point other than dedicated crossing points.
- The platform will have ramped access at either end leading directly to a safe pedestrian crossing point.
- The north end of the platform will provide direct vertical circulation to the Metro Plaza 7.5m above.
- Adequate seating will be provided along the platform length.



7.4.3 BUS INTERCHANGE

CHARACTER

- Local feeder bus services to be developed that will extend the effective catchment area of the Metro. The delivery of a QBC from west Swords into Main Street and the Swords Metro Stop (via The Rise) is an important element of proposed improvements to the bus network.
- In addition to the bus lay-by proposed at grade adjacent to the Stops, at the Swords Stop bus interchange facilities shall also be provided at the Plaza level.
- The bus stops in Metro Quarter will be located to provide convenient interchange with Metro-North. The character will place a large emphasis on amenity and public realm. It is not envisaged that the bus interchange will be a bus terminal.

DESIGN

- The bridge over the R132 will be level to allow for buses to stop and pick up passengers arriving by metro.
- The bus stops will be set down only. There will be no terminus provided at this location. Road engineering should discourage buses from stopping for prolonged periods of time.
- Adequate shelter will be provided for passengers waiting on buses. These shelter facilities should not adversely interrupt the flow of pedestrians moving along this bridge.
- Interchange facilities are to be designed in such a way as to make way finding between bus and metro as well as between transport modes and the Town Centre a priority.
- Adequate signage, bus route information and digital waiting time displays to be provided.
- The Metro plaza/podium and the vehicular conduit under the R132 should ideally be constructed by the time Metro North is commissioned, having regard to practical constraints associated with Metro North construction.
- Independent of arrival of Metro North these elements of infrastructure will be required when development east of R132 takes place.
- The completed plaza and bus-only route should be constructed in tandem with Barrysparks development to ensure links east and west.

7.4.4 R132 AND METRO SETTING

CHARACTER

- The existing landscaped/parkway character of the R132 is to be maintained.
- This area will provide 24 hour access to adjoining areas. It will be distinguished from all other stretches of the R132 as being part of the Metro Quarter and a gateway to Swords.
- The space will include a uniform landscape treatment to both sides of the road tying into the Metro Quarter as a whole.
- The development lands adjacent the Metro Stop require a dedicated vehicular access system. The major impact on the Metro Quarter is the addition of twin vehicle access/egress ramps on either side of the R132 and their associated slip roads.

DESIGN

- Both Pavilions and Barrysparks development façades shall have active/transparent frontage at R132 level around the Metro Plaza. In all other areas

the landscape will be brought up the plinth to continue the landscaped setting.

- The public realm at the R132 level will be naturally lit. Crossing points at the upper level are required however the creation of an “undercroft” situation will, as far as possible, be avoided.
- Bus stops on each side of the R132 will provide adequate shelter and seating and allow for an easy transition to other transport modes.
- An uninterrupted cycle route will be provided running alongside the footpath.
- A landscape strip of minimum 13.5 metres (including footpath and cycle lane) will separate the R132 from the façades of the adjoining developments. The parkway condition of the R132 will become more formalised in the Metro Quarter to reflect its civic role.
- Vehicle access ramps will, as far as practically possible, be hidden within landscape strips.
- Appropriately scaled landscape and built form is envisaged to mitigate the impact of the infrastructure and avoid a highway or functional road engineering logic creating a place inhospitable to pedestrians.

Specific Local Objectives:

- Metro Plaza will be directly accessible to all public transport modes.
- Building façades should be set back a minimum of 13.5 metres from the R132 to achieve a meaningful parkway condition.
- Vehicle access ramps to be hidden, as far as possible, within landscape strips.
- All buildings addressing the Plaza must provide direct active frontage to the plaza.
- Plinth condition along R132 will carry landscape up the building façades as far as Metro Plaza height (+7.5m).
- Semi mature trees, 10m in height when planted, will be used along the R132 to create an immediate continuity of the parkway condition.
- Arcade treatment will be provided along plaza level until arrival of plaza.
- Metro Plaza and Platform will be protected from adverse effects of noise and wind.
- Adequate shelter is to be provided on the Metro Platform.



7.5 Land Use

The Metro Quarter will provide opportunity for active and recreational uses as well as public gatherings, about a major public space. The land use mix within this area will reflect the civic importance of the space as a major transport interchange. Furthermore, there will be opportunity for extended hours of use and activity.

TRANSPORT

The main land use in this area is a multimodal transport interchange at the Swords Metro Stop location. This will form the focus of an integrated public transport system for the town as a whole, involving the establishment of a network of safe and efficient pedestrian, cycle and bus routes all connecting to the Metro.

RETAIL

Retail frontages with frequent access points will be provided along the east and west side Metro Quarter at R132 level and Plaza level to provide active frontage.

Non retail services (including cafes, sandwich bars etc) are required to act as key focal points of attraction. These may be in the form of small kiosk type buildings or be part of the active frontage adjoining the metro quarter.

OFFICE DEVELOPMENT

It is envisaged that there will be significant office development around the Metro Quarter. Lobbies fronting the Metro Quarter will be active in nature and promote evening activity. The integration of high density employment generating uses adjoining the metro is in support of sustainable development.

PARKING PROVISION

Parking will be primarily located underground within the Pavilions and Barrysparks schemes.

Specific Local Objectives:

- Metro Quarter will provide for fully accessible interaction between all forms of public transport.
- All buildings addressing the Metro Plaza must provide direct active frontage to the Plaza and well designed frontage onto the R132 and Metro Platform.
- Shopfront signage shall integrate with the character of the Metro Plaza and promote the formation of a legible urban environment
- Parking shall be primarily located below ground level.
- Office development will be facilitated within Metro Quarter area. Offices must provide generous active lobbies to Metro Plaza and onto R132 level as appropriate.

7.6 Heights and Massing

Building heights and massing around the Metro Quarter will be controlled within a strategy which responds to the size of the adjoining space (Metro Plaza or R132) and will provide a modulated building profile to help mitigate any potential overshadowing.

Buildings around the Metro Plaza will be 35-40 metres high to reflect the scale of connectivity. This is in keeping with the desire to locate higher density land uses around public transport nodes. The heights will reduce as one moves north and south along the R132.

Shadow studies will be conducted to ensure that the height and mass of the buildings are appropriate and do not have a negative impact within the site or on the surrounding areas.

LANDMARK BUILDINGS

There are opportunities to create landmark buildings on both sides of Metro Plaza. These buildings will complement each other and form gateway entrances to the Pavilions and Barrysparks sites as well as Swords Town Centre.



7.7 Landscape Strategy

The landscape strategy for the area is prescribed by several essential factors.

- **Site context:**
Proximity to the adjacent development sites of Barrysparks and Pavilions and associated new routes and access points.
- **Parkway experience and arrival**
Visual appearance and arrival sequence by metro and car along the R132. This strong tree lined approach is part of the defining landscape for the town.
- **Strategic Transport Corridor (R132) and Metro North**
Response to engineering requirements such as the development and realignment of the R132, Metro North, Metro Box* and its associated slip roads.
- **Pedestrian comfort and usability**
Where the road network comes in contact with the pedestrian realm, it is essential that the landscape treatment responds in such a way that the public space is adapted or treated in a manner that protects the users of the development, while avoiding an unnecessary interruption to the vehicular flow.

The general landscape strategy in relation to the Metro Quarter area comprises the following elements:

- Building on the inherent strengths of the existing landscape of Swords is a core aim of the Masterplan for the town. The Metro alignment along the R132 parkway corridor offers an opportunity to use the landscape setting as a signature for the town. This is driven by both the need to replant the existing tree belts and the redefinition of the route as a series of arrival/departure points rather than a bypass condition.
- The current R132 planting is a highly effective green screen, successfully addressing the potentially adverse impact of the road and creating a strong and distinctive character to the main transport corridor through Swords. The construction of Metro North will mean the loss of a number of existing trees along the R132 and landscape analysis has identified that the long term retention of these qualities requires a planned replacement of trees with more durable species. The replacement trees will be planted in a landscape strip of at least 5 metres wide (not including 1.5 metre footpath along each side) to protect the residential amenity as well as reinforcing the identity of Swords. Tree planting in the Metro Quarter will be more formal in nature reflecting the area's importance.
- The planned mitigation measures associated with the integration of the metro alignment within the R132 corridor include the re-provision of the existing planting along the entire southbound road boundary. The opportunity therefore exists to address the whole parkway condition by considering an appropriate replanting scheme that unifies all landscape elements into a single vision reflecting the varying significance of the location. There are 3 levels of response proposed within the Metro Quarter:
 - A. General Approach areas along R132
 - Semi naturalistic planting replicating a woodland setting.
 - B. Swords Metro Quarter
 - Semi mature tree planting in formalised landscape design, lighting and artwork.
 - C. Metro Plaza
 - Planting on Metro Plaza will express the continuity of the parkway condition to the north and south.

ELEMENTS, MATERIALS AND EXPRESSION

Artwork – elements such as sculpture, art and paving patterns will be used at appropriate locations throughout the Metro Quarter.

Lighting – Lighting will be used as an integral design element while also acting as a functional safety feature. There will be several distinct types of lighting employed throughout the Metro Quarter which will form part of the holistic lighting strategy for Swords Town Centre. These are as follows:

- R132 – column mounted lanterns to illuminate vehicle routes
- Pedestrian areas – mix of column mounted lanterns and building mounted lanterns. Mounting heights will be fully integrated with the form of the architecture and provide human scale.
- Amenity – additional lighting around public spaces such as Metro Plaza and platform.
- Architectural – Illuminating building forms can help reinforce the identity of a place. This form of lighting may be internal or external.
- Bespoke illuminated signage pillars with way-finding maps and local information, including Metro and bus timetables and active display screens for actual arrival times. These should be co-ordinated with other such elements installed throughout the Town Centre.
- Landscaping – The lighting of both hard and soft landscaping will be carefully integrated into the whole landscaping proposal.
- Event and art – temporary and permanent use of light, particularly in a dynamic and creative way, will be employed in the Metro Quarter.
- Security and safety – Any specific lighting for security will be carefully controlled. Safety lighting will be considered wherever hazards may occur.

Vista and termination – Particular attention will be paid to the vistas from Metro Plaza. It is envisaged that tree nodes and green areas will form the terminations of views looking up and down the R132 as well as looking down the streets linking into adjacent development sites.

Material continuity – The continuation of a uniform quality paving material shall provide an apparent extension and integration of the Metro Quarter into the future development. The Metro Plaza itself will have a distinguished surface treatment to highlight its importance as the major transport interchange in the town.



8.0 Barrysparks Town Centre Expansion

The Barrysparks development area falls within the boundary identified in the Fingal Development Plan as requiring a Masterplan. Prior to development on this site, however, there is an additional requirement to meet the following Objective and Local Objective as identified in the Fingal Development Plan 2005-2011:

Objective SWORDS 10

- To prepare a Local Area Plan for 'MC' zoned land to the east of the Swords By Pass for the integration of major Town Centre lands east of Swords By-Pass with the existing Town Centre, for a transportation hub, retail and employment uses and the Swords Masterplan.

Map based Local Objective relating to the Barrysparks MC zoned development area:

200: To prepare a Local Area Plan for the integration of major Town Centre lands east of Swords By-Pass with the existing Town Centre including for transportation hub, retail and employment uses and the Swords Masterplan.

The Masterplan will establish an Urban Design Framework for the development of the Barrysparks area, addressing the key issues of: movement, street hierarchy, public realm, height and massing, and landscaping. Any development in this area is, however, subject to the preparation of a Local Area Plan. And the UDF may be reviewed in this context for Barrysparks.

Prior to the development of the Barrysparks development area, a Local Area Plan shall be prepared. An Appropriate Assessment and Strategic Environmental Assessment shall be undertaken as part of this Local Area Plan.

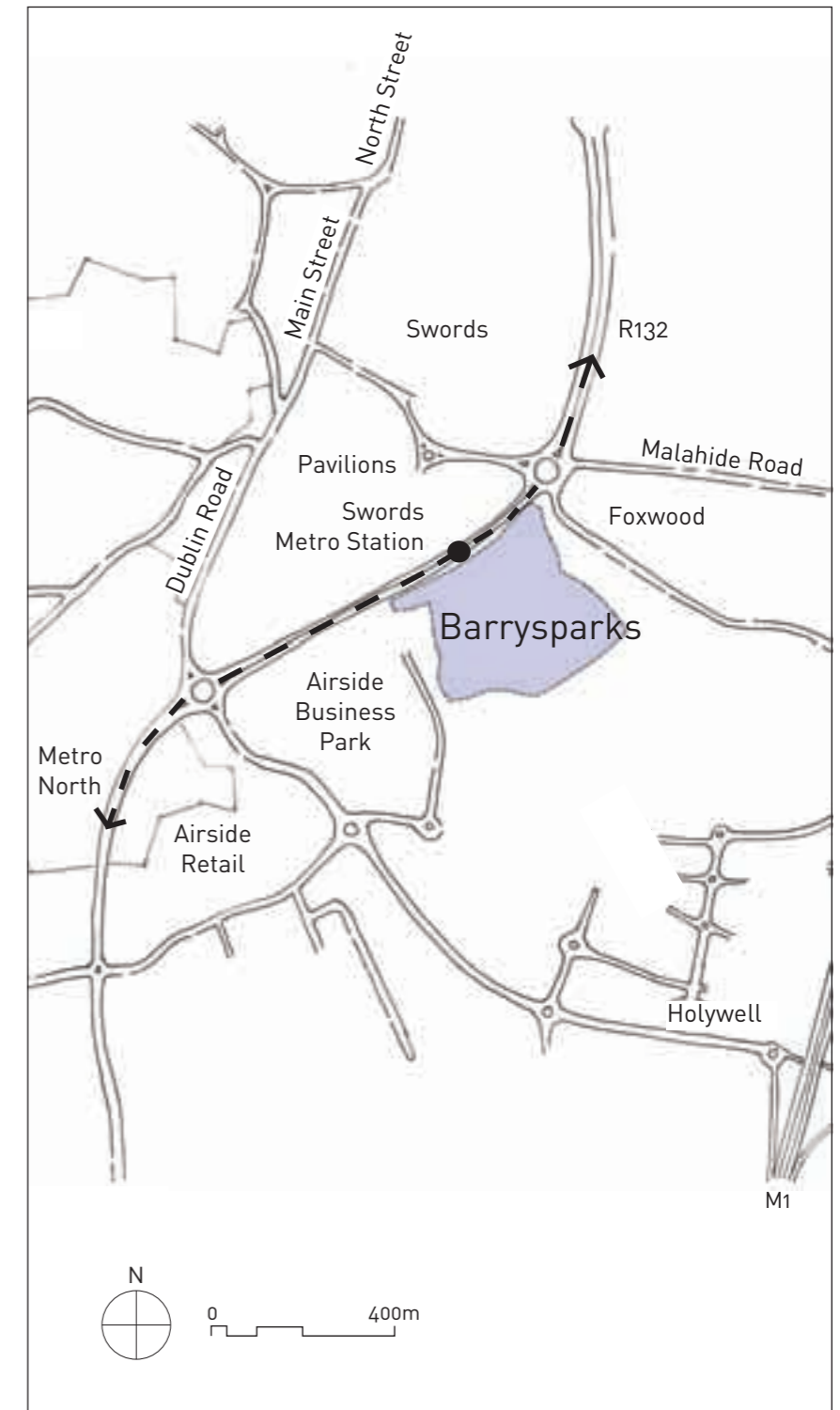
8.1 Barrysparks Development Area: Constraints and Opportunities

Existing Constraints

- Compatibility of future development with existing residential development adjoining the area.
- Compatibility of future uses with the adjoining Airside Business Park.
- Potential barrier of R132 and Metro North line between MC zoned lands east and west of the R132.
- Impact of development on streams within this development area, which flow into the Broadmeadow Estuary. The Estuary is designated as a SPA, cSAC, pNHA and a Ramsar site.
- Airside/Drynam link roads are not yet constructed.

Existing Opportunities

- Integration with Metro Quarter and improved connectivity with the Town Centre by all modes of transport, particularly bus, pedestrians and cyclists.
- Existing landscape corridor setting along the R132.
- Development of mixed-use retail, office, commercial and leisure elements, further enhancing the attractiveness of the Town Centre.
- Direct linkage with the Metro Stop and Metro Plaza, ensuring compact and integrated growth.
- Opportunity for a commercial leisure node to the southeastern edge of the ST1 lands which would support the developing community in this area and also contribute positively to the legibility of Swords by becoming part of the identified network of nodes within the Town Centre.
- Development of landmark and high quality architectural and environmental standards.



8.2 Movement Strategy

Objectives:

- To provide for a legible and permeable high quality urban environment, readily recognisable and easy to move around.
- To provide for a network of well-designed safe, direct and attractive pedestrian and cycle routes throughout the development lands and linking these lands to the Swords Metro Stop, Pavilions development area and Main Street.
- To ensure the Barrysparks development integrates with the surrounding transport network, particularly with the Metro North Stop at ground and first floor level, with the vehicular road network below ground level (Metro Box*), with the vehicular route/transport interchange over the R132 and vehicular connections to the proposed Airside/Drynam Link Road to facilitate future development.

INTEGRATION

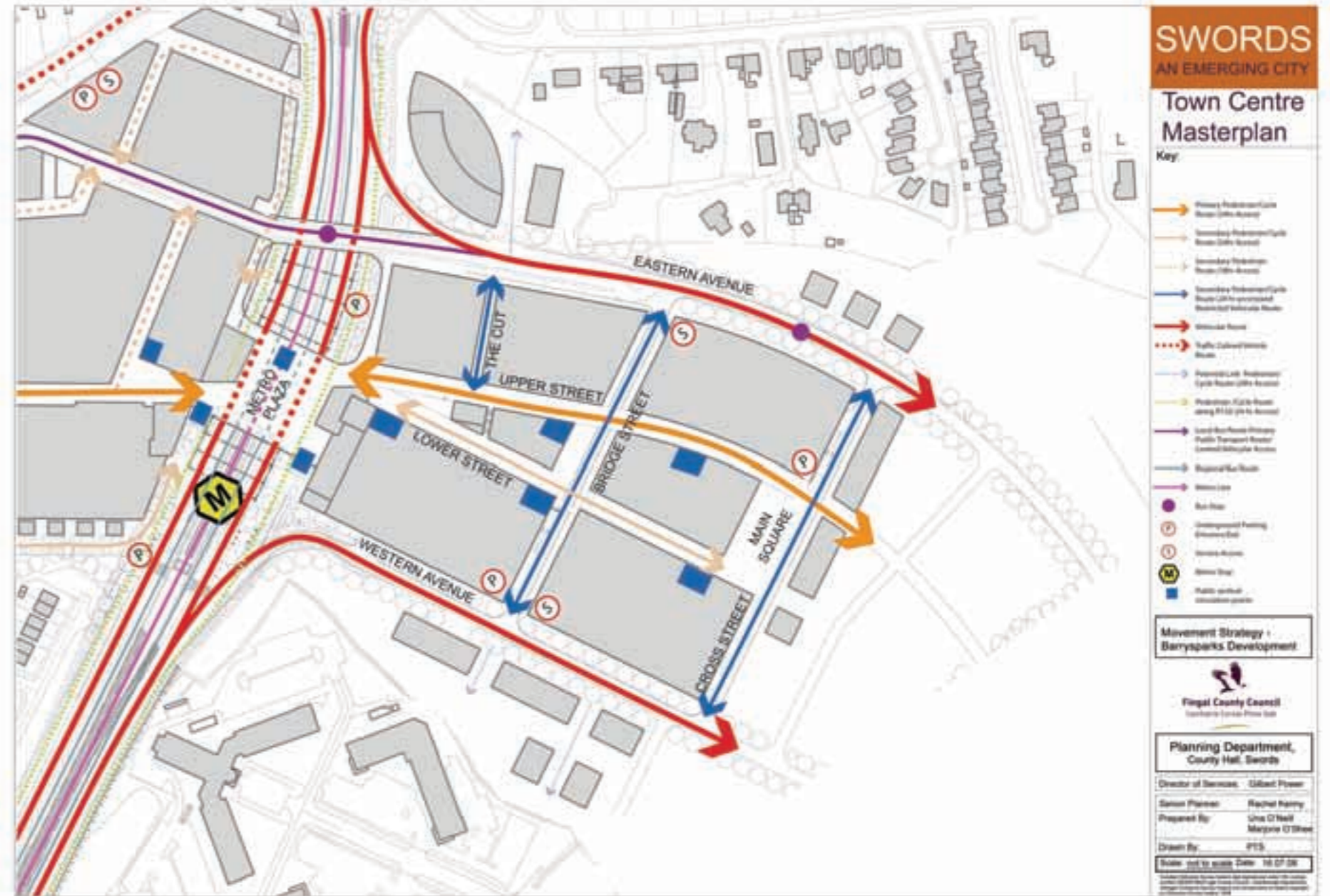
Barrysparks is located to the southeast of Swords Main Street and the Pavilions development area. It is an undeveloped green-field site within an increasingly urban setting, adjacent to the R132 to the northwest and adjoining the proposed Swords Metro Stop. The adjoining land is residential to the north-east, industrial/manufacturing zoned lands to the east (currently undeveloped), residential to the south-east, office to the west and Town Centre zoned lands to the north (Pavilions). This land bank must integrate seamlessly with Pavilions and Swords Main Street if it is to be a successful Town Centre extension.

CONNECTIVITY

All modes of traffic will be connected with Barrysparks in a number of ways to ensure accessibility and permeability within the site and beyond to the Town Centre and Pavilions development area:

- **Public Transport:** The Metro Quarter will be a multi-modal transport interchange accommodating the Swords Metro Stop adjacent to the site. A bus route over the R132 at Plaza level (facilitating Metro interchange) will connect Barrysparks with the Pavilions, Malahide Road and Main Street.
- **Private Vehicular Traffic and Metro Box*:** As part of the Metro Quarter an underground network will be developed linking Barrysparks and Pavilions beneath the R132. Access to this network will be via ramps adjoining both sites inter-connecting beneath the R132. Barrysparks will connect southwards to the proposed Airside/Drynam local distributor road, facilitating connection to the southeast quadrant of Swords back into the Town Centre.
- **Pedestrian/cyclist permeability** will be accommodated through the site via a network of streets, which will link at ground level and at plaza level/+1 to the Metro North and Pavilions development area and also to the southeast via the Airside/Drynam distributor link road network.

* Metro Box is an underground vehicular connection across the R132, associated ramps, and necessary realignment of the R132 locally, in order to provide for access to lands zoned 'Swords Town Centre' east and west of the R132, as shown in the Fingal Development Plan 2005 to 2011. This Metro Box plan was adopted under Part 8.



PERMEABILITY AND STREET PATTERN

The following series of internal streets [see map Movement Strategy] are proposed within the lands, which will enable permeability and connectivity within the development area and also to the surrounding environment:

The primary street through the scheme is a 24 hour pedestrian street called **'Upper Street'** which traverses the site northwest-southeast. This street connects at +1 with the Metro Plaza, linking up with 'Central Street' within the Pavilions site, and connecting ultimately to the Cultural Quarter/Ward River Valley Park and Swords Main Street.

Lower Street is a 24 hour pedestrian route, which runs parallel to Upper Street and connects to the Metro Quarter to the north and 'Main Square' public space to the south. It links into Upper Street in proximity to the Metro Plaza and also at Main Square.

The Rise will be an important street, offering 24 hour public transport/pedestrian/cyclist connection between Barrysparks through the Pavilions to Swords Main Street and supporting multi-modal interchange with Metro North. The Rise will be an inclined street with an average gradient of less than 1:20 and will be approx 12-14m wide (subject to confirmation of detailed traffic engineering design). The street climbs from existing ground level within Barrysparks, to level +1 at the Metro Plaza, returning to ground level at Malahide Road. This route will play an important part in the public transport system and will allow pedestrian connection at +1 level to the Metro Plaza and the Metro stop.

Western Avenue is an "all vehicle" street traversing the development area (northwest-southeast), linking to the R132 for southbound traffic exiting the site and linking into the proposed Airside/Drynam local distributor road to the south.

Eastern Avenue is an "all vehicle" route which links the R132 at the northern part of the site and proposed Airside/Drynam local distributor road to the south. This route interconnects with 'The Rise' public transport route.

A number of secondary link streets are proposed within the development to support the primary north-south links. **The Cut** is a 24 hour secondary pedestrian route offering a link from The Rise to Upper Street and also provides entrances to housing above.

Bridge Street traverses the site, linking Western and Eastern Avenues/The Rise and is predominately for the use of cyclists, pedestrians and service vehicles.

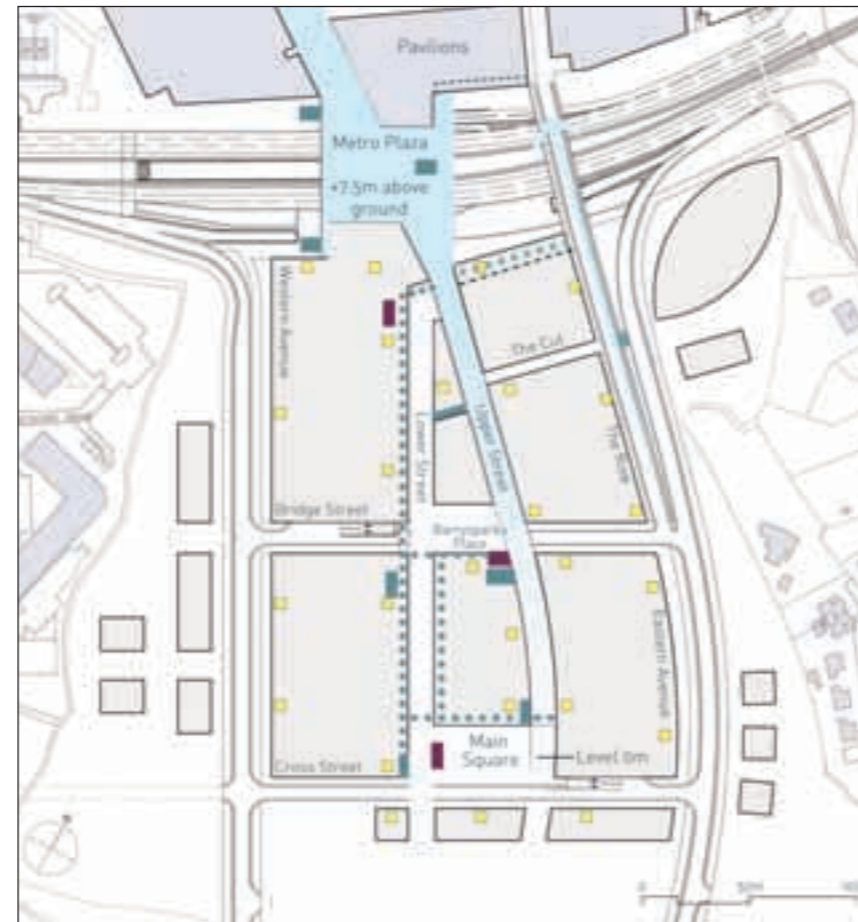
Cross Street also traverses the site, providing access between Western and Eastern Avenues. This is predominately for cyclists/pedestrians modes and for service vehicles. At limited times of the day it may also be used by cars. In the evenings its use will help bring activity to the central parts of the development and the Main Square.

CONNECTIONS [see map Levels/Movement]

To ensure integration with the Metro Plaza and adjoining Pavilions development, Upper Street will slope from at grade at Main Square to +1 at the Metro Plaza (+7.5 m) at a gradient of 1:30. Where Upper Street and Lower Street link to the Metro Stop, vertical circulation between levels is provided. Another stair and lift will be provided at the Main Square linking the ground level to the arcaded routes. Other stairs and lifts will be incorporated along this upper route as required.

The 'Rise' will move from ground level within the Barrysparks lands to +1 over the Metro Plaza and connect through the Pavilions lands to the Malahide Road. This transition will occur over a slope of less than 1:20, capable of accommodating buses and pedestrians/cyclists (subject to confirmation of detailed traffic engineering design). 'South Street' will rise from 0 to +1 towards the western section, with car parking accommodated below this area.

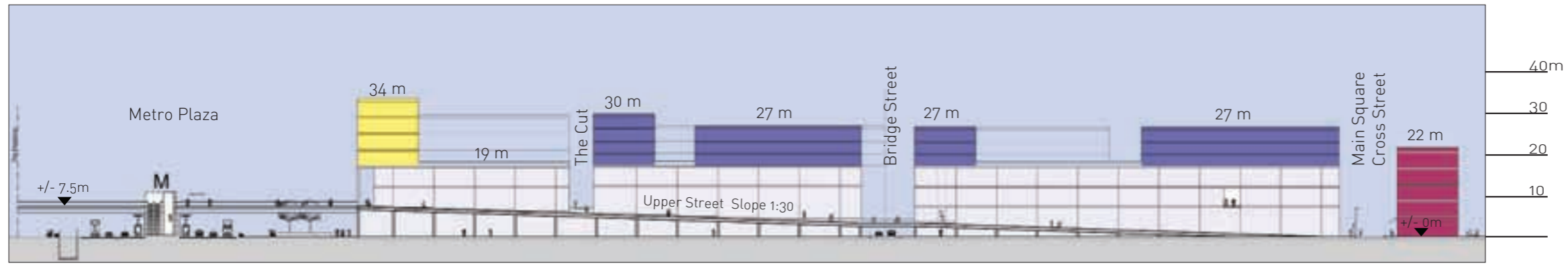
Metro Quarter. There will 2 pedestrian crossing facilities at ground level from Metro Platform to Barrysparks. There will be 3 vertical circulation nodes to facilitate ease of movement at this location between the Metro Plaza and Barrysparks and Pavilions. [see Section addressing Metro Quarter].



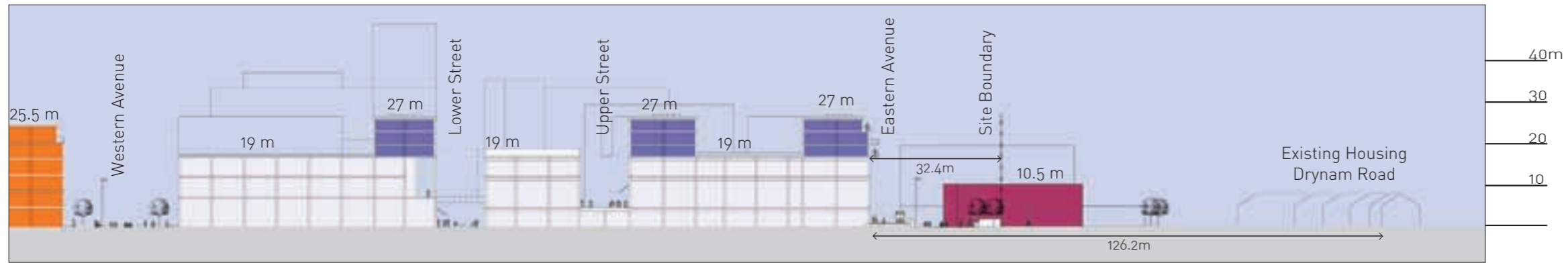
Specific Objectives:

- Development shall integrate with the Metro Plaza (providing +1 level pedestrian connectivity to existing Town Centre and Pavilions development) and underground vehicular network beneath the R132, including access ramps. These connections shall be provided for prior to the occupation of any development on the Barrysparks lands.
- The Rise/Western Avenue and Eastern Avenue shall link into the proposed Airside/Drynam distributor link road. This connection shall be provided in tandem with development on the Barrysparks lands.
- All the streets within the Barrysparks development area shall facilitate 24 hour public access.
- Upper Street shall be the primary pedestrian street linking into the Metro Plaza and shall clearly connect to Central Street within the Pavilions site.
- The Rise shall be constructed at a gradient of less than 1:20 (subject to confirmation of detailed traffic engineering design) and shall be a 24 hour public transport street linking Barrysparks, over the R132, through the northern section of the Pavilions site, and connecting with the Town Centre.

Axonometric View of Upper Street and Lower Street



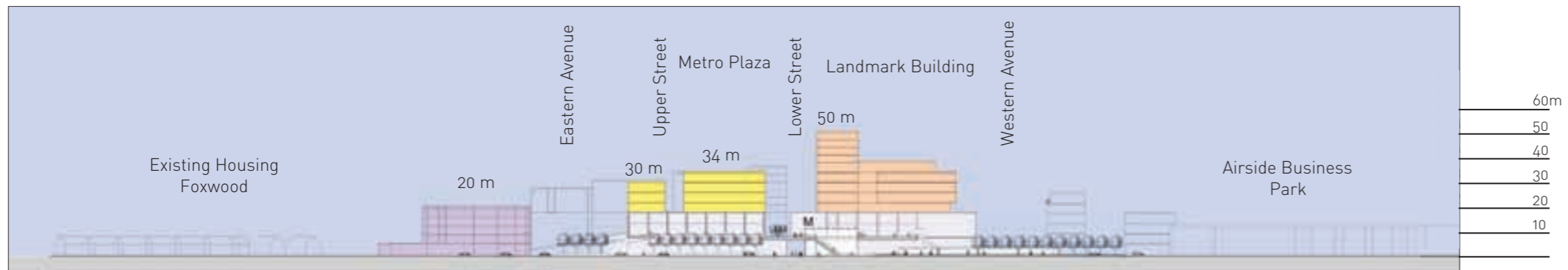
J: Upper Street. Slope 1:30



K: Street Section: Western Avenue, Lower Street, Upper Street and Eastern Avenue

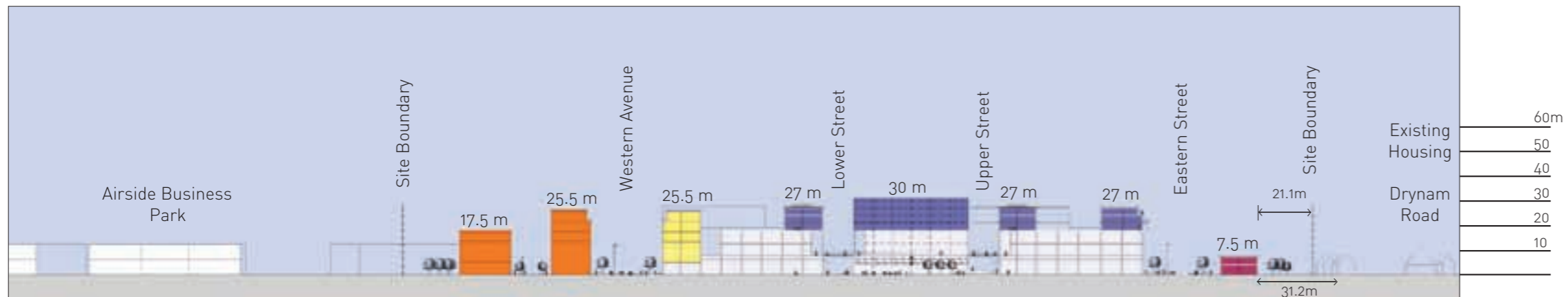


Key Plan

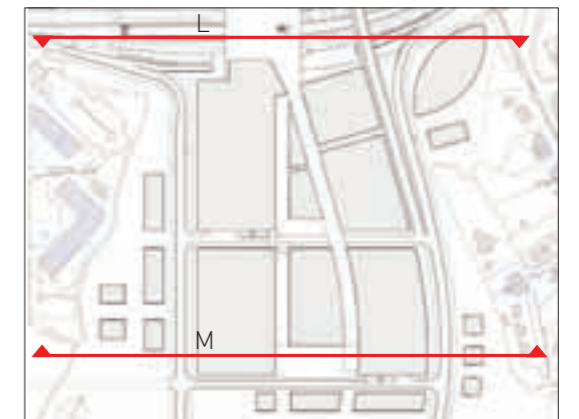


M: Street Section: Eastern Avenue, Upper Street, Metro Plaza, Lower Street, and Western Avenue

- LEGEND
- Retail
 - Residential
 - Residential over Retail
 - Offices/Business
 - Offices/Business over Retail
 - Hotel & Business Centre with Conferencing Facilities
 - Landmark Building (Offices/Business and Residential over Retail)



L: Street Section: Western Avenue, Lower Street, Upper Street, and Eastern Avenue



Key Plan

8.3 Density and Mix

LAND USE STRATEGY

Objectives:

- To develop a high density range of Town Centre uses, including a suitable mix of retail, retail services, residential, office, hotel, social and cultural uses.
- To ensure the location and scale of retail development shall not negatively impact on the compact focus of the core retail area within Swords Town Centre.
- To ensure the provision of active frontages onto the Metro Plaza and public spaces/nodes within the development area.
- To ensure the protection of the amenity of adjoining residential areas.

An appropriate mix of uses is to be accommodated to continue the multi-functional role of the Town Centre. Uses to be facilitated are as follows: retail, retail services, residential, office, hotel, social, community and cultural uses [see map *Uses Diagram*]. The mix of uses and the quantum of this mix is important in the overall vitality and viability of the Town Centre and is addressed here indicatively. The exact mix and quantum of uses will be assessed fully as part of a future LAP and associated Appropriate Assessment and Strategic Environmental Assessment.

RETAIL DEVELOPMENT

A component of the development at Barrysparks will be comparison retail, which will be arranged from the Metro Stop towards the Main Square. It is important to ensure the location and scale of retail development does not negatively impact on the compact focus of the core retail area within Swords Town Centre and Pavilions development area. To this end the proposed retail element will be located within 400m of the Metro Stop and the scale of the development will be such as to ensure the balance is maintained toward Main Street.

The retail at Barrysparks will have a direct link at the upper level with the Metro Stop which will be in the form of an elevated Plaza. At ground level a controlled crossing will allow a pedestrian link across the R132. This will be designed in conjunction with bus stops, a taxi rank and drop-off points. The 2 main retailing streets proposed are Upper Street and Lower Street, which are crossed by Bridge Street and Cross Street. The character of these will vary from pedestrian and cyclist only to restricted vehicle movement. Residential units will be proposed above ground level to ensure viability and vibrancy along these primarily pedestrian routes.

The shops will generally be arranged on three floors with the upper floor occupying a reduced floor area. The gently sloping Upper Street allows a transition from the elevated Metro Plaza to Main Square and provides a series of retail units along its length. The shops at Lower Street are accessed directly from the lower street. Other shops will be accessed along open arcades at first floor level. These arcades will be open to the outside but will have roof protection. Bridges will link the upper streets and arcades at the first floor level maintaining pedestrian links and avoiding dead-ends. The second floor retailing will be accessed from within the individual shops. Each retail 'block' will have its own architectural character and identity to promote variety and interest.

Currently, two anchor stores are included, one adjacent to the Metro Stop and

the other near the Main Square. The positioning of these stores will encourage pedestrian movement along the streets. The Main Square will provide a focus of activity during shopping hours and in the evenings as a location of restaurants, bars and cafés – a place to linger and rest. Other opportunities for food and drink will be identified throughout the development.

Retailing provision may be between 50,000 - 60,000 gross sqm with a range of unit sizes catering for a variety of retailers. Servicing for the majority of retail units will be discreetly organised at ground level in mews type streets or alternatively below ground level – this will be worked out at planning application stage. Of primary importance is the maintenance of a pedestrian friendly and active vibrant place.

Retail services

Retail services (including non-retail uses such as Cafés, Restaurants, Smoothie Bars, Sandwich Bars, Confectionery and Coffee Shops etc) are required particularly around key public spaces to act as focal points of attraction, with active frontages addressing the spaces. Such uses will in particular be supported at the Barrysparks Place, Main Square and onto the Metro Plaza to ensure day and evening activity.

RESIDENTIAL

Located within a transportation corridor, the subject lands are an appropriate location for higher densities subject to qualitative standards.

Residential provision within the Barrysparks area will be located:

- Within the Major Town Centre Zone proximate to the Metro Stop and overlooking key 24 hour routes
- On the eastern edge adjoining the housing off Malahide Road

On the eastern edge development shall be consistent with the established character of the adjoining housing will be provided and with its own separate open space area designated. Development adjoining existing residential development will be assessed at planning application stage in terms of massing, height, visual impact, overlooking, sunlight/daylight, private and public open space provisions, parking arrangements, and landscaping, to ensure protection of adjoining residential amenity.

The form of housing will vary across the site to provide a broad range of housing 'types' to suit single occupiers and families. Approx 600-800 units will be accommodated.

An appropriate range of retail and community facilities will be provided within the Barrysparks area including childcare and healthcare in consultation with the Planning Authority. This will be assessed as part of the LAP for the area. It is anticipated that a primary school will be required for the area.

OFFICE DEVELOPMENT

Barrysparks has the potential to provide a significant quantum of office development of approx 20,000 - 30,000 sqm offering local employment. Office development will be located adjoining the R132 at the Metro Stop location and toward the northwestern boundary adjacent existing commercial development within the Airside Business Park. The buildings adjoining Airside Business Park will generally be five to seven storeys with parking arranged underground.

The total area for office development is approximately 20,000 - 30,000 sqm.

PARKING PROVISIONS

Parking shall be primarily located underground within the scheme, facilitating the development of a pedestrian friendly environment. The level of parking provided shall meet development plan standards. Given the location of this site beside Metro North, which is a high-quality public transport facility, deviation from the county development plan standards will be considered.

A Traffic Impact Assessment and Mobility Management Plan will be required to be submitted as a part of a planning application for this area. This Plan will consider the proximity of development to the Metro Stop, interlinking of trips for those visiting the area and the potential catchment area. The Swords traffic model undertaken by FCC shall be used to assess the implications of the proposed development.

Detailed design and development standards will be adopted within the LAP, however the following specific local objectives should be considered:

Specific local objectives:

Retail

- Active frontages shall be provided along retail streets within the scheme and shall be dominant in particular fronting all urban spaces.
- Shopfront signage shall integrate with the character of the streetscape and promote the formation of a legible urban environment.

Residential

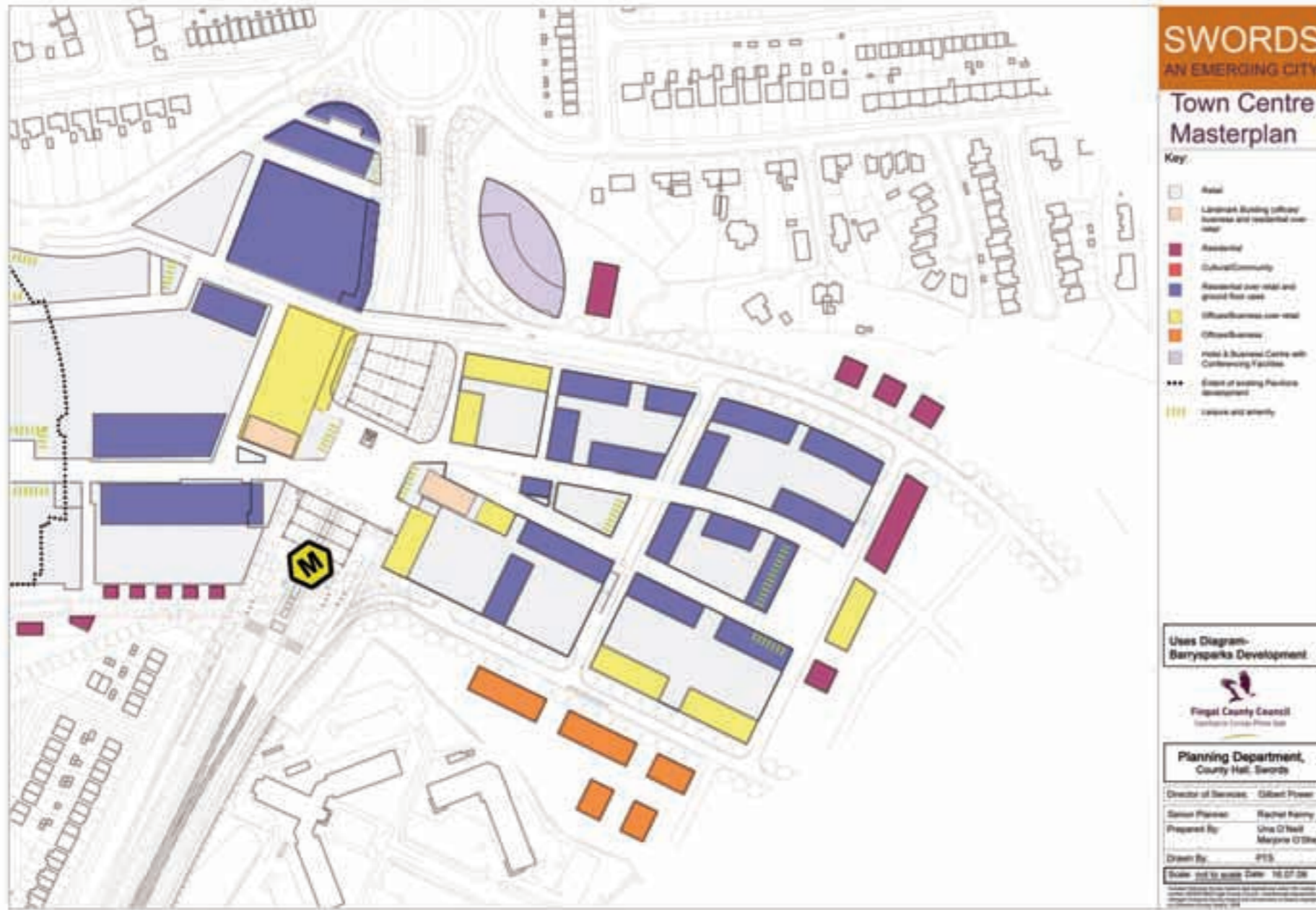
- A range of 600-800 residential units shall be provided within the scheme to ensure vibrancy and activity along the network of 24 hour streets.
- Residential units shall be provided along Upper Street, Lower Street and Eastern Avenue.
- All residential units shall be assessed at planning application stage in terms of height, massing, sunlight and daylight impacts, visual impact, overlooking, wind impact, acoustic privacy, and provision for private and public open space.
- Development adjoining Eastern Avenue shall ensure the protection of the residential amenity of existing adjoining dwellings.

Office

- Office Development shall be facilitated along the R132 and adjoining the Airside Business Park.
- Office development shall be assessed at planning application stage in terms of height, massing, sunlight and daylight impacts, visual impact and wind impact.

Traffic, Mobility and Parking Management

- Parking provisions shall be assessed within the context of the Development Plan provisions, proximity to Swords Metro Stop, and in accordance with the proposed LAP for the area.
- A Traffic Impact Assessment shall be submitted at planning application stage.
- A Mobility Management Framework, including Mobility Management Plan and a Car Parking Management Strategy, shall be submitted at planning application stage.
- Innovative approaches, such as the development of car clubs and car sharing will be considered by the Planning Authority.



8.4 Heights And Massing

Building heights and massing will be controlled within a height strategy which responds to:

- (i) The existing site context and neighbouring areas.
- (ii) The creation of 2 major public spaces of significant size (excluding Metro Plaza) and the need to create appropriate scale of enclosure.
- (iii) Provision of a modulated or graded profile away from the buildings about the major public spaces which indicates a hierarchy of space.
- (iv) Identify locations which could accommodate landmark forms to strengthen spatial hierarchy.

Building heights are indicated as a series of height ranges which respond to the Heights Strategy, uses within each building, the need to create a modulated building profile and to minimise any potential of overshadowing [see map *Barrysparks Heights Strategy*].

A location suitable for a landmark building has been identified at the southern edge of Metro Plaza. This 40m+ building will be a landmark and will provide a sense of arrival for the development. This mixed-use building forms a gateway to Barrysparks.

Surrounding the landmark building, structures of up to 40m are located along the R132 to provide a sense of enclosure for Metro Plaza. This responds to the desire to increase density adjacent to a public transport node.

From Metro Plaza, building heights are gradually reduced into the site. Building heights between Eastern and Western Avenue vary up to 30m. The highest building is located between Upper Street and Lower Street on the northern side of the Main Square. This tall building helps to identify and define this public space.

Apartment buildings along the eastern edge of Eastern Avenue are further reduced in height and respond sensitively to the houses located along the eastern boundary.

Offices/business buildings along the western edge of Western Avenue will be

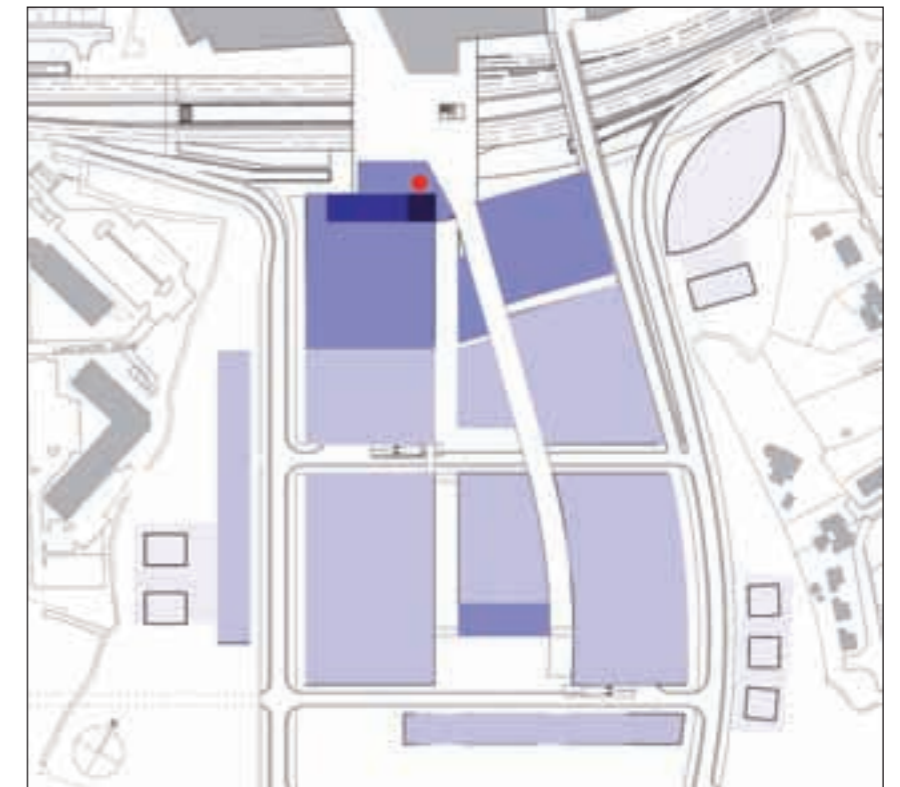
reduced in height to respond to the buildings at Airside Business Park.

Shadow studies will be conducted to ensure that the height and mass of buildings are appropriate and do not have a negative impact within the site or on the surrounding areas.

LANDMARK BUILDING

The proposal for the metro stop at Swords and the desire to extend the Town Centre provides the opportunity to create major building developments at Barrysparks. Allied to this is the need to increase density within close proximity of a public transport node.

A site adjacent to Metro Plaza is identified as a location for a landmark building. This important site addresses the southern side of the plaza and its massing will complement the development on the opposite side at the Pavilions site. This building forms part of a gateway to the Barrysparks development. The building will be a mixed use development on ten floors. Three of those floors will be retail, floors four to seven will contain offices and the upper three floors will be residential.



LEGEND

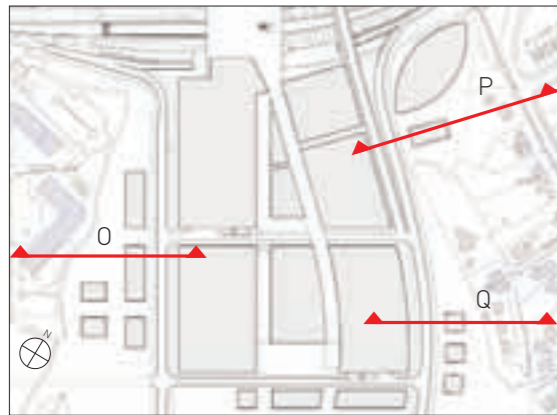
- Existing buildings and The Pavilions proposal
- Location for taller landmark building
- Building height circa 50m (subject to shadow studies and visual appraisal)
- Building height circa 40m (subject to shadow studies and visual appraisal)
- Building height modulated down from 40m
- Building height modulated down from 30m
- Building height modulated to respect existing adjacent offices and houses

EDGE CONDITIONS

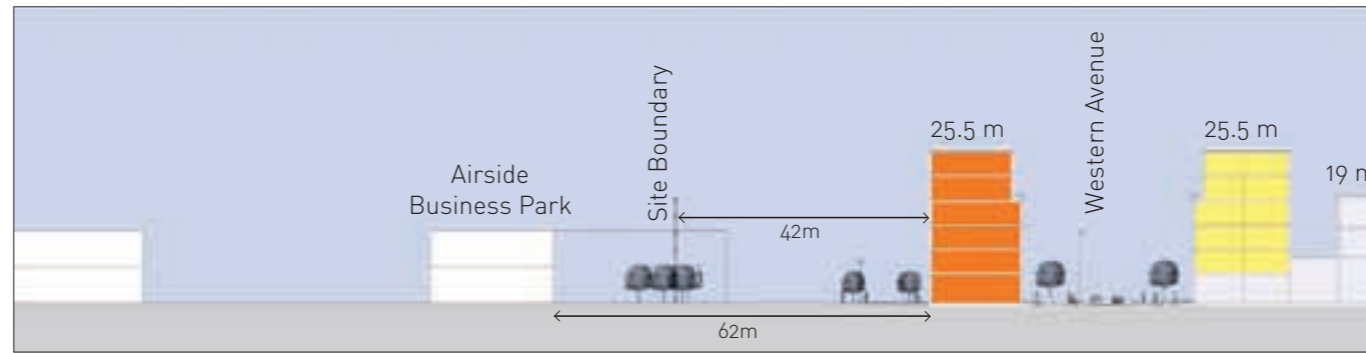
The boundaries with existing residential and commercial developments to the west and east are considered as edges and shall be co-ordinated with neighbouring lands to respond to the existing scale, use and character.

The strategy adopted when dealing with adjacent buildings is to gradually reduce the scale and tier the height and massing of the proposed development. This will help ensure that issues relating to overshadowing and overlooking are mitigated. Shadow studies will be conducted as part of the planning process to ensure that the height and mass of the buildings are appropriate and do not have a negative impact within the site or on the surrounding areas.

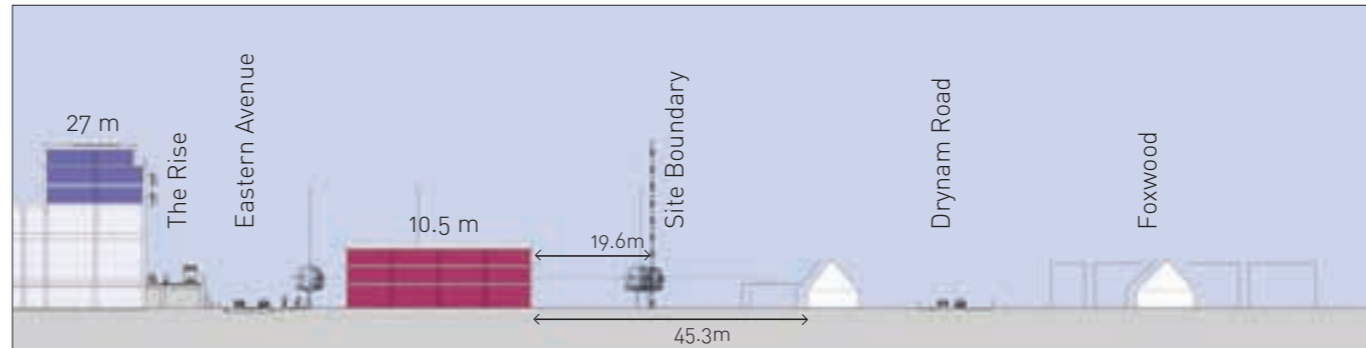
The following cross-sections relate to the edge condition and highlights the need for sensitivity adjoining existing developments.



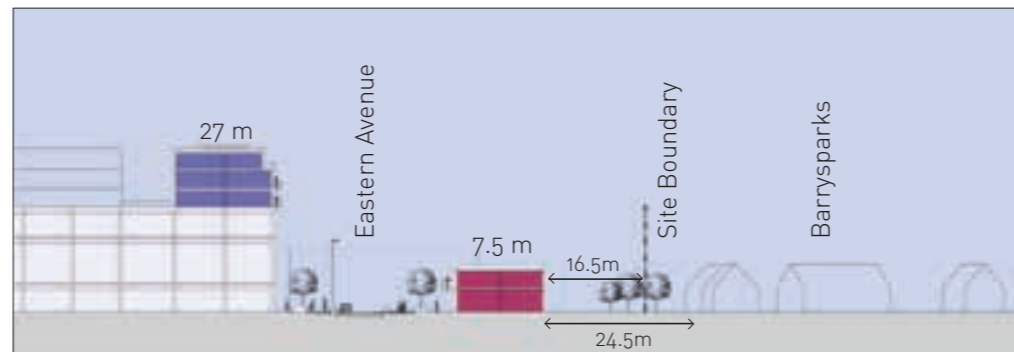
Key Plan



O: Section along Western Avenue showing proposed development and Airside Business Park



P: Section along Eastern Avenue showing proposed development and housing at Drynam Road and Foxwood



Q: Section along Eastern Avenue showing proposed development and housing at Barrysparks

8.5 Urban Form: Streets

Objectives:

- To provide for a clearly defined street hierarchy. Each street will be designed according to the core principle that the street is a space, a place of containment and activity rather than simply a movement and access route. The scale and character of each street, its width, and the height of buildings along it shall relate to this hierarchy. A high degree of permeability will be provided throughout the street network, with provision for 24 hour streets.
- To create a strong central 24 hour pedestrian street, 'Upper Street' through the Barrysparks development area, traversing the Metro Plaza and linking with the Cultural Quarter through the Pavilions development area.
- To provide for a network of high quality public spaces and urban squares/civic spaces.
- To provide for a number of 'landmark' buildings to give a 'sense of place' and punctuate key 'places' within the area.

The proposed network of streets will be distinguished in character via:

- Width to height proportions, which will reflect the primacy of the streets within the scheme. Urban design and daylight criteria will govern the width-height relationship.
- Primary use, for example retail, residential access, office use etc, will result in differing street widths and materials.
- All streets will be designed with passive security in mind, including the following features:
 - Overlooking by residential and other uses on the upper floors
 - Clear and unobstructed street lines without recessed or hidden areas for easy surveillance at ground level
 - Residential and office accommodation should have street addresses with overt street level entrance lobbies distributed along all 24 hour routes.
 - All routes should have alternative access points so cul-de-sacs are avoided
 - Clear street naming signage should be used throughout the area in a consistent manner to a Swords Town Centre standard design.
 - No element of the buildings adjacent the streets shall project over the street width, except intermittent bays less than 10% of the street width.

STREET COMPARISON/HIERARCHY



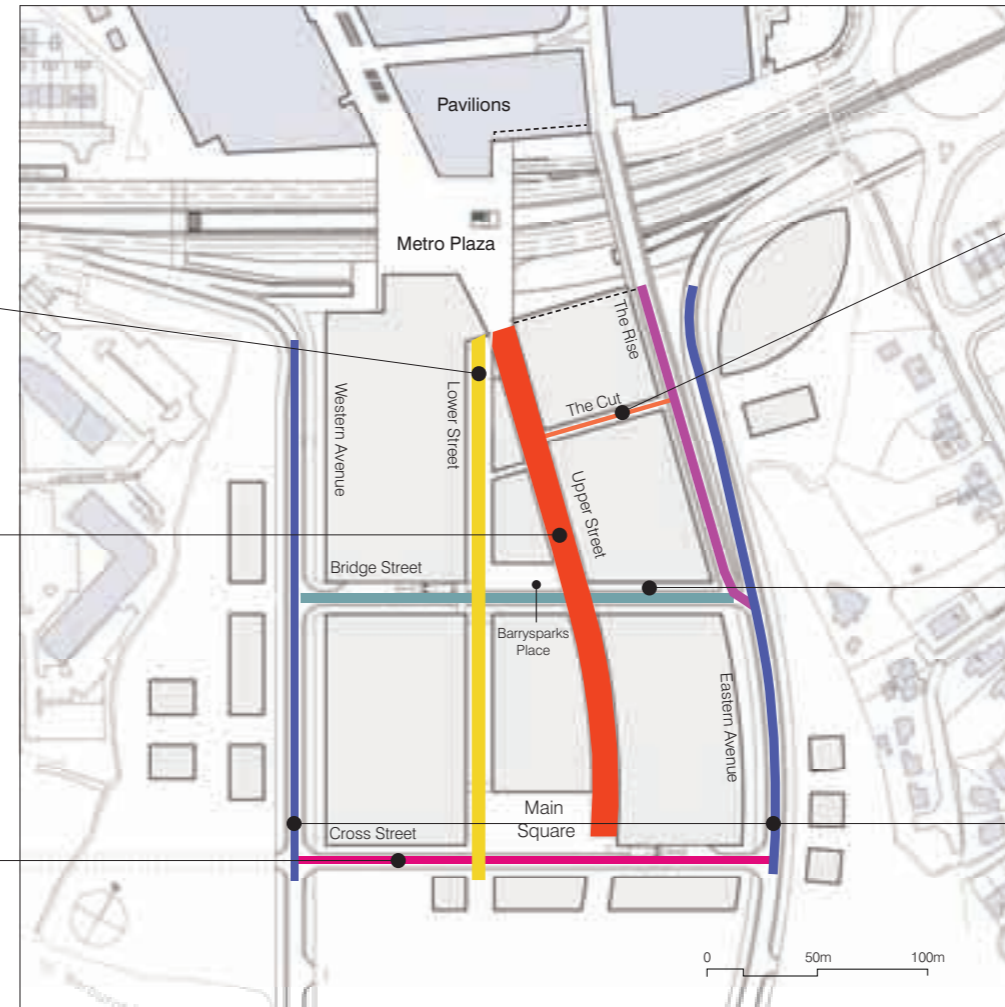
Stroget, Copenhagen



Henry Street, Dublin



Grafton Street, Dublin



LEGEND	STREET	LEGEND	STREET	LEGEND	STREET
	Upper Street		Lower Street		The Rise
	Bridge Street		The Cut		
	Cross Street		Western Avenue and Eastern Avenue		



Quartier Bloom, Dublin



Docklands, Dublin



Bus Routes along a tree-lined avenue, London

**STREET HIERARCHY –
URBAN DESIGN PARAMETERS AND OBJECTIVES:**

The primary routes at Barrysparks are described below. They have been given provisional names for ease of description. Additional streets may be developed to ensure break up of blocks ensuring greater levels of permeability and access.

Upper Street

CHARACTER

- Upper Street shall be the primary and central 24 hour pedestrian route traversing the Barrysparks development. This route shall be a sloping street, linking in with Metro Plaza at level +1, falling to +0 at the main public space within the scheme, 'Main Square'.
- Upper Street shall connect with Central Street within the Pavilions to the northwest. It is important to maintain 24 hour connection between the two sites via this street and to ensure the co-ordinated design of these streets in terms of material, finishes, street furniture etc.

DESIGN

- Upper Street shall be 10-12m wide.
- The buildings along this route shall vary in height from 20m to 43m with the taller buildings being located where the street widens around public spaces.
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing.
- Materials and finishes to the street shall be designed to reflect the character of an external street.
- Upper Street shall be co-ordinated with Central Street (within the Pavilions) in terms of materials, finishes and character, where appropriate.

USES

- Retail uses shall occupy maximum 3 floors, with residential and offices units overhead.
- Residential uses over the retail street shall form part of street facades creating points of focal interest and passive supervision of the street.
- The location of residential/office units above shall be co-ordinated to ensure daylight/sunlight to the street and minimise overshadowing. Shadow studies will be conducted as part of the planning process to ensure that the height and mass of the buildings are appropriate and do not have a negative impact within the site or on the surrounding areas.
- Landscaped courtyards serving the residential/office units may be visible providing opportunity for the landscaping to become part of the street character.

LEGEND

- Retail
- Residential
- Residential over Retail
- Offices/Business
- Offices/Business over Retail
- Hotel & Business Centre with Conferencing Facilities
- Landmark Building (Offices/Business and Residential over Retail)



Lower Street

CHARACTER

- Lower Street is an important 24 hour pedestrian route through the development linking in with Upper Street and the Metro Quarter to the northwest and the main public space within the scheme, 'Main Square' to the southeast. This street runs at grade parallel to Upper Street.

DESIGN

- Lower Street shall be 10-12m wide.
- Building Heights shall be 20-23m.
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing.
- Part of this street will have a double height covered arcaded walkway at first floor level. At the lower street level protection is provided by way of fabric or glass canopies to protect pedestrians.

USES

- Retail uses shall occupy maximum 3 floors, with residential and offices units overhead.
- Residential uses over the retail street shall form part of street facades creating points of focal interest and passive supervision of the street.
- The location of residential/office units above shall be co-ordinated to ensure daylight/sunlight to the street and minimise overshadowing. Shadow studies will be conducted as part of the planning process to ensure that the height and mass of the buildings are appropriate and do not have a negative impact within the site or on the surrounding areas.
- Landscaped courtyards serving the residential/office units may be visible providing opportunity for the landscaping to become part of the street character.

Eastern Avenue

CHARACTER

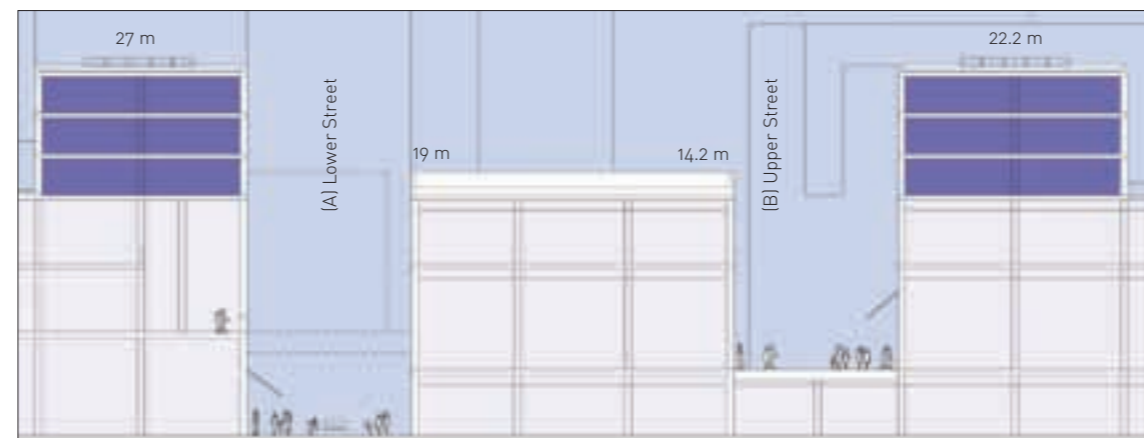
- Eastern Avenue shall be a tree lined avenue accommodating 24 hour access for vehicular and pedestrian/cyclist traffic. This avenue facilitates access from the R132 and shall connect into the proposed Airside/Drynam Distributor Road network. This avenue also connects to The Rise, which is an important part of the public transport system.

DESIGN

- Eastern Avenue shall be 28-30m wide from building to building. The carriageway width will be 7.3m with footpaths and cycleways separated from the carriageway on either side.
- Buildings adjoining this street shall range in height from 3 to 7 floors on either side.
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing.

USES

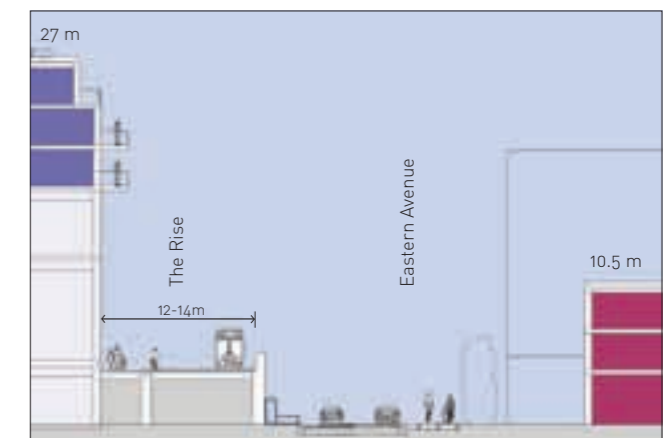
- Residential units and retail uses with residential units over shall be accommodated along Eastern Avenue.
- A Hotel Use may be accommodated at the junction of Eastern Avenue and the R132.
- Uses over the retail street shall form part of street facades creating points of focal interest and passive supervision of the street. The location of residential/office units above shall be co-ordinated to ensure daylight/sunlight to the street and minimise overshadowing. Shadow studies will be conducted as part of the planning process to ensure that the height and mass of the buildings are appropriate and do not have a negative impact within the site or on the surrounding areas.
- Landscaped courtyards serving the residential/office units may be visible providing opportunity for the landscaping to become part of the street character.



N: Street Section: Upper and Lower Streets

(A) Lower Street:
Street width 12m
Tallest building height 27m
Street width to building height ratio 1:2.25

(B) Upper Street:
Street width 12m
Tallest building height 22.2m
Street width to building height ratio 1:1.85



L: Street Section: Eastern Avenue (and The Rise - slope 1:20)
Street width 35m, Tallest building height 27m
Street width to building height ratio 1:0.77

The Rise

CHARACTER

- The Rise shall be the main 24 hour public thoroughfare linking Barrysparks lands over the R132 via the Metro Quarter and connecting to Pavilions area and Malahide Road. This street shall cater for cyclists, taxi and bus traffic.
- The Rise shall be designed as a sloping street (1:20 slope in line with building regulations) with a bridge over the R132.

DESIGN

- The width of The Rise street will be 12-14m and at a slope of less than 1:20. This shall be subject to confirmation of detailed engineering requirements for bus access.
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing.

USES

- The Rise shall facilitate pedestrian/cyclist/public transport access from Malahide Road to the R132 and into the Barrysparks development area.
- Entrances and frontages shall be organised along this street to ensure activity.

Western Avenue

CHARACTER

- Western Avenue is an "all vehicle" avenue, facilitating permeable access through the site. It will be the main access road for vehicles travelling from the north and shall connect with the Airside/Dynam Distributor Road network providing access to the southeast of Swords and the surrounding road network.

DESIGN

- The carriageway width shall be 7-8m with footpaths and cycleways separated from the carriageway on either side. The width from building to building shall be 28- 30m.
- This tree lined avenue shall have buildings ranging in height from 3 to 7 floors on either side reinforcing the street's importance as a major route.
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing.

USES

- Western Avenue shall be fronted by a mix of office and ground floor retail uses, with a focus on active frontages to this street.
- Uses over the retail street shall form part of street facades creating points of focal interest and passive supervision of the street.
- Landscaped courtyards serving the residential/office units may be visible providing opportunity for the landscaping to become part of the street character.

The Cut

CHARACTER

- The Cut shall be a secondary 24 hour pedestrian/cyclist route linking Upper Street and The Rise.

DESIGN

- The Cut shall be 6m wide.

- Building heights shall be 19m – 30m
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing. Shadow studies will be conducted as part of the planning process to ensure that the height and mass of the buildings are appropriate and do not have a negative impact within the site or on the surrounding areas.

USES

- Active retail uses shall be accommodated with offices and residential units over the retail units.
- Uses over the retail street shall form part of street facades creating points of focal interest and passive supervision of the street.
- Landscaped courtyards serving the residential/office units may be visible providing opportunity for the landscaping to become part of the street character.

Bridge Street

CHARACTER

- This 24 hour Cross Street shall link Western and Eastern Avenues and is predominately for the use of cyclists, pedestrians and restricted vehicular access/service vehicles.

DESIGN

- This street shall be 14-15m wide.
- Building heights will be 19-27m.
- A small public space is provided halfway along its length and is called Barrysparks Place.
- Building height, orientation and width of the street shall ensure daylight and sunlight within the street, while minimising overshadowing.

USES

- Retail uses with residential uses over shall be accommodate along Bridge Street.
- Leisure, amenity and active uses shall front onto Barrysparks.
- Landscaped courtyards serving the residential units may be visible providing opportunity for the landscaping to become part of the street character.

Cross Street

CHARACTER

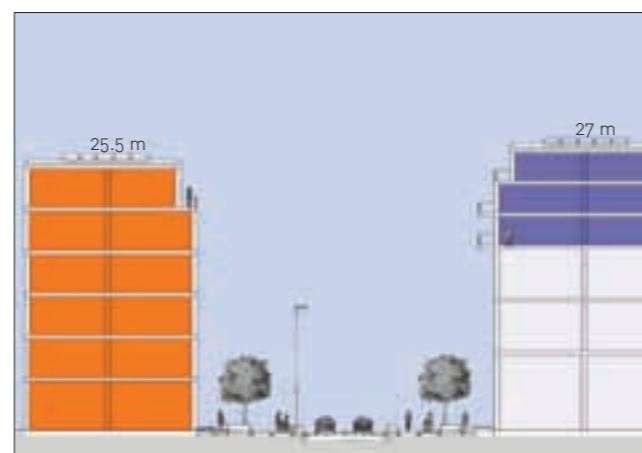
- Cross Street provides 24 hour access between Western and Eastern Avenues and is predominately for the use of cyclists, pedestrians and service vehicles.
- The primary public space within this area 'Main Square' adjoins Cross Street and shall be addressed by active frontages.

DESIGN

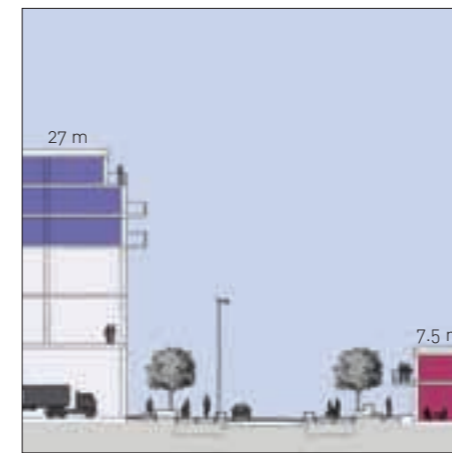
- This street shall be 14-15m wide. At limited times of the day it may also be used by cars. In the evenings its use will help bring activity to the central parts of the development.

USES

- Retail uses with residential and office uses over shall be accommodated along this street.
- Ground floor uses shall have active frontages to enliven the street.
- Main Square will support a mix of active frontages address this public space and the street.



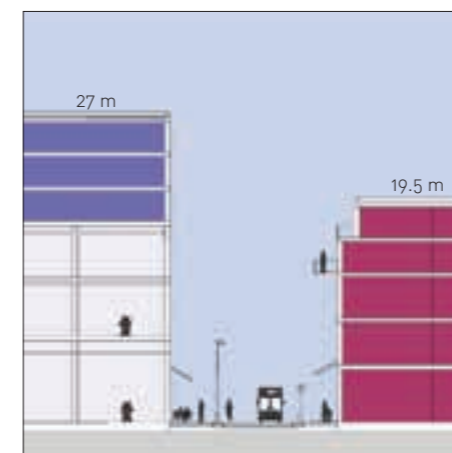
H: Street Section: Western Avenue
Street width 28.1m, Tallest building height 27m
Street width to building height ratio 1:0.96



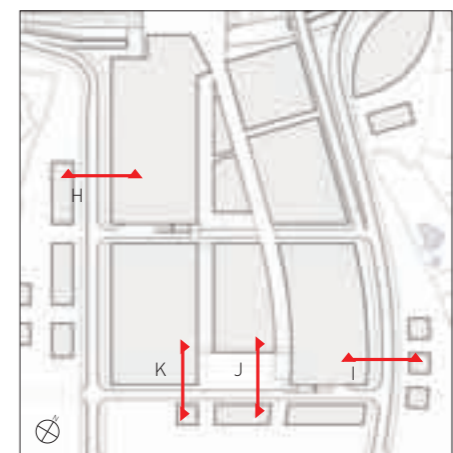
I: Street Section: Eastern Avenue
Street width 29m, Tallest building height 27m
Street width to building height ratio 1:0.93



J: Street Section: Main Square and Cross Street
Street width 38.5m, Tallest building height 30m
Street width to building height ratio 1:0.78



K: Street Section: Cross Street
Street width 14.5m, Tallest building height 27.0m
Street width to building height ratio 1:1.86



Key Plan

8.6 Open Space

8.6.1 PUBLIC SPACES/ACTIVITY NODES

- To ensure the provision of high quality public spaces and urban squares/civic spaces within the development, linking with the Metro Quarter.
- To ensure that the public realm is characterised by a high standard of design, quality finishes and treatments, including hard and soft landscaping.
- To provide for a number of 'landmark' buildings to give a 'sense of place' and punctuate key 'places' within the area.

Barrysparks will accommodate a mix of uses and a range of activities. The promotion of a longer day is important to ensure greater vibrancy and activity and to maintain a night time economy as an extension to the daytime economy. This longer day will be supported by a mix of uses that include offices, residential, retail, retail services, such as restaurants and bars, and a hotel. Active ground floor frontages onto the main 24 hour routes and onto the main public spaces will be supported.

Two main public squares are proposed within the Masterplan plan which will aid in the legibility and attraction of the area [see map *Public Realm Strategy*]. The primary square is called Main Square and is located at the southern edge of the scheme, with an additional smaller square within the scheme, Barrysparks Place. Proposed development onto the Metro Plaza public space will be required to support the urban design parameters set out in section 7.0 to support this key public node.

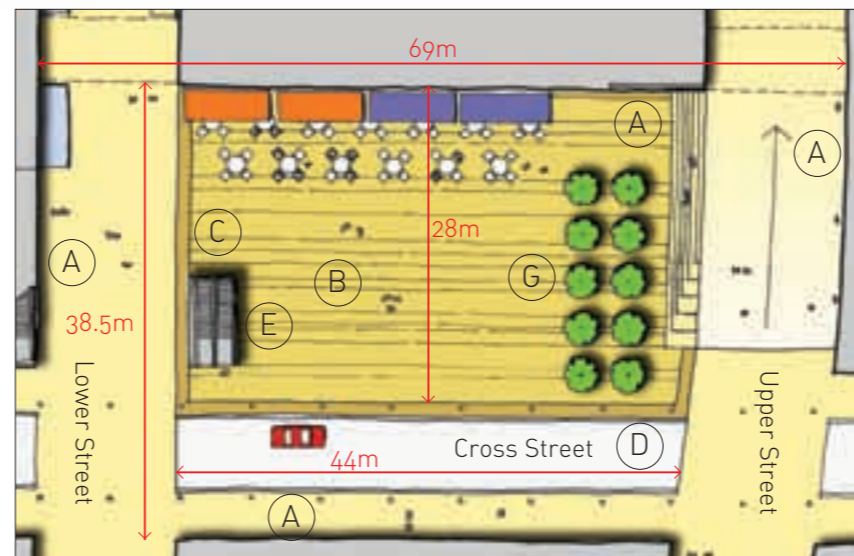
Main Square

CHARACTER

- Main Square shall be a major 24 hour public space within the Barrysparks development area, with direct pedestrian, cycle and bus links to the Metro Quarter. It is intended that this square will be one of the main urban spaces within Barrysparks. It is a place for gathering, eating and socialising. It will also provide a location for events such as farmers' markets, seasonal fairs, street theatre, concerts etc.

DESIGN

- The location and orientation of Main Square has been carefully considered to ensure that it will receive sunlight throughout most of the year.
- Main Square measures 38.5m by 69m (measured face to face of buildings). The buildings around the square may vary in height from 22m to 30m.
- Active ground floor frontages onto this urban space, eg cafes, restaurants, provision of outdoor seating etc, shall be supported to enliven this space and make it attractive.
- The careful selection of paving materials, street furniture, lighting and signage shall contribute to this public space becoming a quality meeting space.



Main Square, Barrysparks



Example of steps adjacent to Upper Street at Main Square

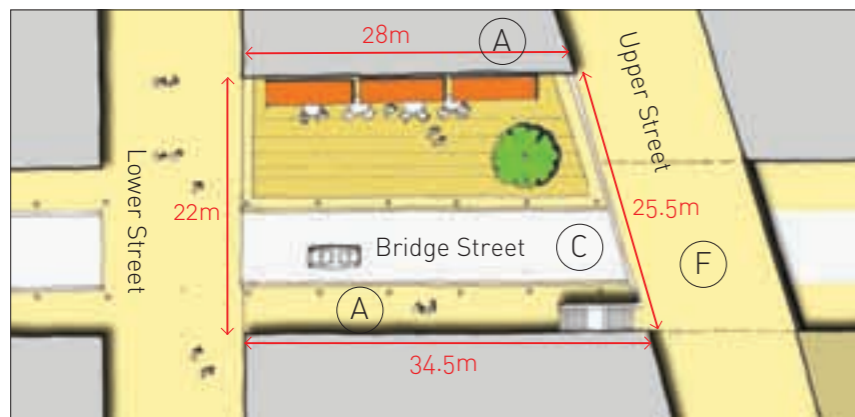
Barrysparks Place

CHARACTER

- This is a small public space formed at the junction of Bridge Street, Lower Street and Upper Street. This intimate space shall comprise active ground floor frontages.

DESIGN

- Barrysparks Place shall be approximately 28m x 28m. The height of buildings around the square shall be carefully considered within the context of daylight/sunlight levels within the space and impact of overshadowing.
- Active ground floor frontages onto this urban space, eg cafes, restaurants, provision of outdoor seating etc, shall be supported to enliven this space and make it attractive.
- The careful selection of paving materials, street furniture, lighting and signage shall contribute to this public space becoming a quality meeting space.



Barrysparks Place

Proposed Leisure Node

Within the south-eastern extent of the Barrysparks land there will be an additional activity node, which will link in with the network of nodes within the Town Centre, ie the Civic Quarter, Cultural Quarter, Metro Quarter and future proposed Balheary node to the north of North Street. The Barrysparks leisure node is considered important in the future expansion of the Town Centre, but will not be fully defined or designed pending the preparation of a Local Area Plan for this area.

PUBLIC REALM STRATEGY



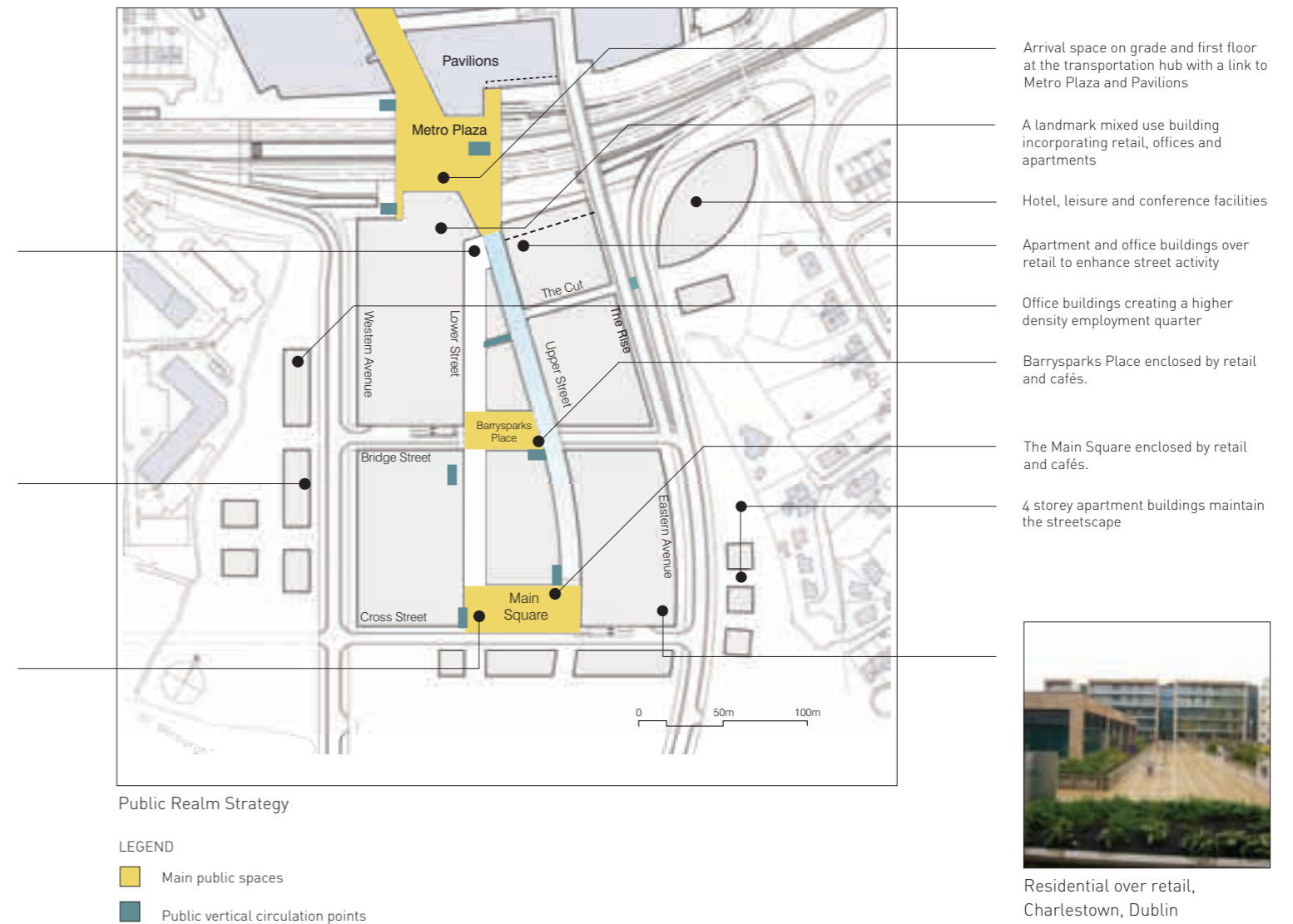
Street Café, Stockholm



Offices, Copenhagen



Temple Bar, Dublin



Residential over retail, Charlestown, Dublin

8.6.2 LANDSCAPE STRATEGY

The overall Landscape Strategy for the area is prescribed by several essential factors:

- **Site Context**
Proximity to the existing Town Centre and future Pavilions development
- **Route-ways and permeability**
Development of additional routes and access points through the proposed development and along the bounding areas
- **Strategic Transport Corridor (R132) and Metro North**
 - Response to engineering requirements; such as the development and alteration of the R132 and Metro North.
 - Response to the Metro Plaza.

- **Landscape Corridors bounding the edge of the site area**
 - R132 Parkway – Extension and development
 - Southern and Northern Boundary
- **Parkway Experience and Arrival**
 - Visual appearance and arrival sequence by cars along the R132.
 - Arrival sequence for Metro North users.
- **Pedestrian Comfort and Usability**
Where the road network comes in contact with the pedestrian realm, it is essential that the landscape treatment responds in such a way that the public space is adapted or treated in a manner that protects the users of the development, while avoiding an unnecessary interruption to the vehicular flow.

The aim is to create a landscape setting consistent with a high quality development and to provide a coherent new 'green' structure throughout the site providing a variety of landscape elements and open spaces. Linear elements

include the site boundary treatment, the new landscape edge to the R132, the avenues and streetscapes with arcades and walkways traversing and linking the indoor and outdoor spaces of the development. Urban squares, roof gardens and a civic park provides open spaces of varying sizes along the pedestrian axis connecting the southern end of the site with the new Metro Stop.

The landscape strategy in relation to the Barrysparks *boundary* areas is as follows:

All existing vegetation is to be retained where appropriate unless it is assessed and shown to be unsafe, diseased or of no long term amenity value. This established vegetation forms an important source of food and shelter for fauna and is indispensable for maintaining flora and fauna density.

- **Existing Vegetation**

The existing vegetation pattern is to be enhanced where suitable and new planting to consist of predominantly existing/native species to retain the local landscape identity and sense of place, its character and distinctiveness wherever possible. It will thus remain as a predominantly linear landscape, with elements of varying width.

- **Views and Vistas**

Opportunities for views in and out of the site may be desirable locally and the boundary planting will be either low or non-existent to allow interesting or attractive views of the surrounding area. Conversely, the width may increase and a high content of evergreen species be introduced to maximise all year screening and minimise nuisances such as views to loading or storage areas, car parking etc.

- **New planting**

The aspect of new planting facing the development gives the opportunity to improve the environment by increasing soft landscaping areas, species diversity and improve the reproductive capacity of an ecosystem.

The quantity of soft landscaping will decrease when moving from the site boundary towards the more urban areas. The civic park will be an exception. At roof level the landscape impact is high due to the extent of proposed green roofs.

The landscape strategy in relation to the Barrysparks development area is as follows:

- **Avenues (north-south)**

The north-south aligned avenues are lined with rows of trees on both sides creating visually strong, formal and linear elements of a simple and striking architectural appearance appropriate for the speed of traffic. The tree species is limited to one or maximum two species for coherency and continuity creating a strong identity throughout the site. The row of trees will be interrupted by site accesses and visibility splays etc., and where unobstructed views are desired.

A band of low ground cover in the verge separates the combined foot/cycle path from vehicular traffic thus providing a safer and more attractive experience when using the road network. An intermittent band of low ornamental planting or low hedging beneath the avenue trees will allow pedestrian movement between footpaths and pedestrian access points along building façades etc.

- **Streets (mainly east-west and including the north-south central pedestrian axis)**

These streets experience a higher volume of pedestrians and give fewer opportunities for soft landscaping. Therefore a carefully selected palette of high quality paving materials and street furniture will be chosen to ensure continuity, visual calmness and coherency throughout. Preference will be given to sustainable materials.

Selected streets allow limited access for cars and/or buses facilitating access to these parts and spaces at all times.

The streets form an essential part of the extensive pedestrian and cyclist network which will help to successfully integrate the development with the surroundings and encourage an influx of people to the outdoor and indoor amenities.

- **Main Square**

Main Square is the largest open hard landscaped space and will be used as a central meeting and resting place. It may also be used for seasonal events like Christmas fairs, skating rink, farmers market or similar.

- **Civic Park**

As part of the infrastructure contract a large open public space will be constructed. It is located south of the MC Zone Lands around the central axis of the development.

The existing streams are to be retained wherever possible as open watercourses in accordance with SUDS principles and form part of the over ground attenuation ponds. They will add character and diversity to the character of the civic park.

An extensive pedestrian and cyclist network links the civic park to surrounding developments, residential areas, Metro North and Swords city centre.

- **Green Roofs**

The proposal for large scale units adjacent to Metro North maximise the opportunity for green roof technology promoting long-term sustainability. They form an essential part of the SUDS designs and add ecological, aesthetic and amenity advantages in addition to reducing the volume of runoff and attenuating peak flows.

Consideration will be given to the climate and potentially harsh exposed growing conditions. Hardy plant species will be selected.

The types of green roofs range from intensive to extensive with no access to extensive areas.

ELEMENTS, MATERIALS AND EXPRESSION

STREET: SPACE, APPEARANCE AND USAGE

- **Artwork** – A series of artwork and bespoke urban elements, such as seating, lighting and paving patterns will be used at appropriate locations.
- **Lighting** – Lighting shall be used as an integral design element while also acting as a functional safety installation.
- **Uncluttered and Uninterrupted** – Provision of an unfussy floor plane that responds to the street type requirements, while at the same time providing seating and relaxation/ outdoor dining functions. In this manner, patterns of usage are considered and catered for.
- **Vista and Termination** – Consideration will be given to the provision of a green terminus in the form of tree nodes and views to green areas to the boundary.

MATERIAL CONTINUITY

- The continuation of a uniform quality paving material shall provide an apparent extension and integration of the future development into the existing urban fabric. A key factor for the integration of the development shall be the similarity/matching of the external and internal landscape element.

A selection of lighting shall form a continuous appearance around the boundary of the site, while also marking the primary street connections.

Specimen Tree selection: The continuity of tree form and appearance shall provide a legible boundary along the R132. Tree selection at the Metro Quarter will be more urban in character (*see section 7.0 Metro Quarter*).

UPPER REALM RESIDENTIAL LANDSCAPE STRATEGY

- To achieve the appearance of a balanced and proportional landscape treatment to the roof terrace levels, it is critical that there is a sufficient layering of planted material and build-up to allow for the installation of large tree species and associated planting.
- To achieve the build-up and planting depth for the upper realm, landscape is carefully coordinated with the engineering requirements to take into consideration the loading, drainage and irrigation issues. The availability of an interstitial layer at below the residential levels allows for the development of a much more dynamic and proportioned landscape treatment than a standard 'loaded' roof terrace.
- The residential roof plan will allow the opportunity for a sequence of interconnected spaces. It is intended that these landscaped areas are visible from street level and add to the character and animation of the streetscape.
- A selection of spatial requirements is included within the confines of the upper realm levels. Semi public play area, barbeque and outdoor eating areas, green space and shelter/screen planting are combined to create an animated living environment.

9.0 Public Realm and Landscaping Strategy

9.1 Introduction

Successful Town Centres have a multi-functional role to play. The modern competitive urban landscape is one of vibrancy, diversity, amenity and sustainability and uses are mixed rather than segregated.

The Landscape & Public Realm structure is a key element of a successful Town Centre, with the quality of design reinforcing and enhancing the urban setting. This is particularly relevant to Swords Town Centre where a strong natural landscape and spread of trees throughout the town environs is further enriched by the Ward River Valley Park and water course, as well as the mature tree lines along Main Street and R132 parkway/landscaped condition [see map 'Green Spaces, Pedestrian Links, Views'].

The effective screening of the current disparate set of uses and built forms by dense tree planting has permitted Swords to retain its identity. The protection and extension of this historic use of planted and retained natural tree planting as structured landscape elements is a key component of the urban design framework and a defining element of Swords as a whole.

In conjunction with the strengthening and consolidation of landscaping, a Public Realm Strategy for the Main Street Area, linking to North Street and Town Centre extension areas (Pavilions and Barrysparks) is required.



9.2 Characteristics of the Public Realm

Public realm is the space between and within buildings that are publicly accessible, including streets, squares, forecourts, parks and open spaces. It also includes landscape elements, street furniture and public art. The Masterplan seeks to improve and expand the public realm in Swords Town Centre by addressing existing deficiencies and promoting proposals for public realm enhancements.

A strategy for the Public Realm should address:

- **Amenity** – creating a safe, attractive and accessible Town Centre environment
- **Coherence** – establishing a clear set of design principles which run throughout the Town Centre
- **Identity** – the principles should reinforce the character and identity of Swords.

The public realm derives from the urban strategy of connecting the evolving Swords with the Metro. To this end, the Masterplan proposes a network of new and enhanced streets and spaces in order to deliver these connections.

The principal streets to be enhanced include Main Street and associated transport corridors leading to and from Main Street, connecting Main Street to Metro and surrounding amenities and services. In addition, the importance of the R132 as a main arterial route through Swords cannot be underestimated. It will create a first impression of the city of Swords for many people, and is examined in detail under Section 7.0, Metro Quarter.

The major urban spaces are:

- Civic Quarter.
- Cultural Square.
- Pavilions Place.
- Malahide Place.
- Metro Plaza.
- Main Square, Barrysparks.
- Barrysparks Place.

Swords already has the benefit of three existing major public green spaces:

- Town Park
- Rivervalley Park
- Broadmeadow Valley Park

While the parks are an important and integral part of the public realm they are not considered in detail in the Town Centre Masterplan which focuses predominately on hard landscaping and urban nodes within the Town Centre



area. The development of the parks will require a site-specific Landscape Strategy and associated Appropriate Assessment. This shall be completed within 12-24 months of the preparation of this Masterplan.

While the treatment the public realm of the major urban spaces is discussed in detail elsewhere in this document, this section of the Masterplan highlights key issues of the public realm strategy for the Town Centre as a whole and consists of:

- Streetscape (the 'streetscape' includes facades addressing the public areas as well as the highway, cycle lane, pavements and associated street furniture and planting).
- Street furniture
- Signage
- Lighting
- Public Art

The following are key public realm criteria to be considered in relation to Swords Town Centre:

1. The design and layout of buildings must incorporate the principles of passive surveillance to encourage a community atmosphere while discouraging anti-social behaviour.
2. Active frontages are to be provided around the main public spaces, especially those of the major activity nodes.
3. Pavements and walkways must be wide enough to accommodate wheelchairs and pushchairs and have dropped/sloping kerbs to ensure safe and easy crossing points.
4. All crossing points should be located on pedestrian and cyclist desire lines.
5. Guard rails and other street furniture should be kept to a minimum consistent with maintaining safety standards while avoiding visual clutter.
6. Cycle paths should be clearly defined but changes of colour within the highway should be minimised.
7. Paving materials should reflect the character of the area, whether it be the historic core of the civic quarter or the modern retail of Pavilions.
8. The use of an agreed palette of materials will define the hierarchy of streets and spaces in the Town Centre, enhance the setting of civic buildings and identify pedestrian priority areas whilst reinforcing local distinctiveness.
9. Signage and public art will be incorporated into the paving to add additional visual interest and minimize clutter within the streetscape. Detailing of railings and street furniture, such as planters, can also function as elements of public art within the streetscape.



9.3 Street Furniture

Street furniture should help to create a distinctive and consistent identity for the Town Centre whilst enabling individual character areas to be recognized. The co-ordination of street furniture, public art and streetscape can reduce visual clutter and create a 'coherent' Town Centre.

1. The use of local and environmentally friendly materials can assist in minimising the visual intrusion of street furniture in the streetscene.
2. The design and positioning of street furniture should also be co-ordinated to minimize clutter. Street furniture should be placed to avoid conflict with pedestrian movement. It should not reduce the width of the pavement to such a degree that it impacts on accessibility for users of wheelchairs and pushchairs.
3. Trees will be used to soften the landscape in key locations, for example on the Metro Plaza they may be used to provide acoustic and visual barrier to the R132.
4. Coordinating street furniture with planting will assist in creating attractive environments for pedestrians and in minimising visual intrusion.
5. The positioning of seating and other street furniture will take account of the micro-climate. Ensuring protection from the wind and maximising sunlight will enhance use of spaces whilst also assisting with maintenance of street furniture.

9.4 Signage

Signage will be designed to convey directional and orientation information (particularly for visitors). Signage should be consistent and co-ordinated across the Town Centre. A Town Centre 'identity' can be achieved by consideration of:

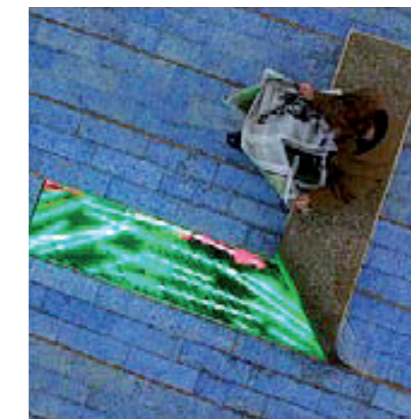
- Typeface;
 - Colour and materials used;
 - Shape, proportions and even heights of signs;
 - Use of logos.
1. Some signage will be integrated with the design of buildings, and reflect the character of the area. Illumination of signage should be controlled, particularly in historic parts of the Town Centre.
 2. Where possible, signage should also be co-ordinated with street furniture and with information boards.
 3. High quality information boards/orientation signage will be provided at priority orientation points such as the Metro Quarter.

9.5 Lighting

Lighting is a fundamental element of any strategy for the public realm. The design of the public realm must be able to support activity in the evening as well as during the day. Streets and spaces must encourage activity and provide safe and welcoming environments.

On a practical level, lighting can improve the attractiveness of an area and make it feel safe for pedestrians at night. Lighting can also create interesting environments by illuminating buildings or key routes and enhancing the setting of civic buildings and spaces.

1. Lighting needs to be sensitively designed and positioned to avoid clutter and light pollution. Lighting fixtures, fittings and columns should, wherever practicable, be incorporated into existing or proposed columns, street furniture and features to reduce the impact of lighting in the streetscape.
2. The approach to lighting, particularly of important buildings, should be co-ordinated across the Town. The distinctive character of individual areas can however be emphasised by subtle changes in the lighting strategies, and coloured lighting can add a further dimension to this strategy. Low level lighting is likely to be required, and energy efficient lighting should be integrated into the strategy across the Town.
3. Lighting strategies for special (whether civic or seasonal) events should be considered and the infrastructure incorporated into the design of new spaces and buildings.



9.6 Public Art

Public art needs to be considered as an integral element of the design of buildings and public spaces. It can enhance buildings and spaces, creating visual interest and encouraging commercial and social activity. Public art can be used as a means of highlighting the significance of a building, public space or gateway to the Town Centre. It can also give greater character and identity, creating a 'sense of place'.

The new activity nodes of the Civic, Cultural and Metro Quarters present ideal locations for public art.

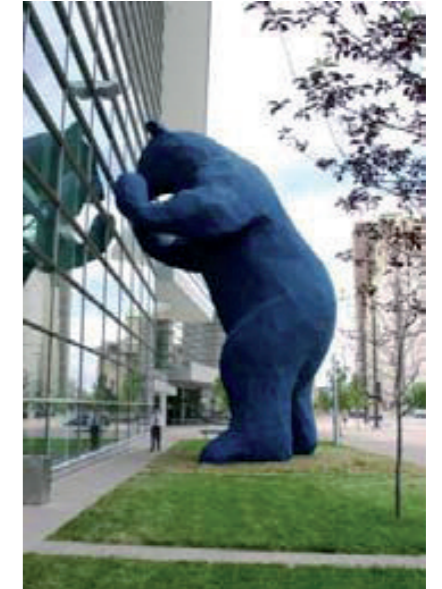
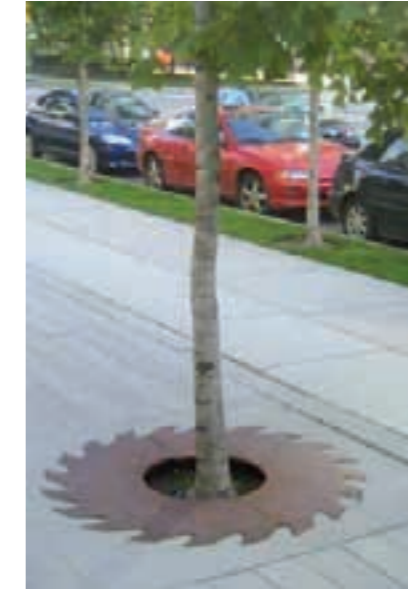
1. There will be strong co-ordination between lighting, signage and public art within the Town Centre to avoid clutter and to present a consistent and distinctive identity.
2. A wide range of techniques should be employed including free-standing artworks, artist designed landscapes, digital technologies, artist's lightworks and signage and temporary projects which may be especially appropriate in the Metro Quarter.
3. The approach to public art should stimulate debate, encourage participation, be informative, locally relevant and responsive to community aspirations.

9.7 Conclusion and Implementation

Quality of the public realm is a priority of the Masterplan. A user-friendly public realm is well connected and attractive, making walking and cycling easy, pleasant and convenient, and improving the area's attractiveness as a place to live, work and visit.

A public realm strategy for Swords Town Centre shall be completed within 12-24 months of the preparation of the Masterplan.

Development proposals will be required to demonstrate a positive contribution to achieving well designed public spaces, creating active ground floor uses, contributing to passive surveillance of public areas, improving the micro-climate in and around new buildings and incorporating high quality surface treatments, street lighting, furniture and public art.



10.0 Swords Town Centre Heights Strategy

10.1 Introduction

The urban design framework for the Swords Town Centre Masterplan provides a guide to suitable building heights for new sustainable development, whilst protecting the established residential and visual amenity of the area.

Context is paramount and an appropriate response is fundamental in respect of five key criteria:

1. **Respect of the Amenity of Existing Development:** Where the heights of proposed buildings require set back distances and landscape screening elements to protect the privacy of local residents adjoining development areas.
2. **A Positive Contribution to the Public Realm:** Buildings exceeding the general context heights and unscreened by natural features will have to demonstrate their urban design merits both in terms of outward appearance and benefits accruing in terms of open space and additional amenities provided.
3. **Heritage Valued:** Protection of key vistas, of local protected structures and the social and historic heritage of the area is important. Heights and massing should not adversely impact on the innate and valued historic character of the area.
4. **Landmark Role:** Areas of particular significance, higher density and increased activity can be identified via taller buildings acting as landmarks. The location of taller buildings can be part of the process of creating an identity for the town. It requires both a rationale for locating these taller elements and an understanding of their desirability and viability in those locations.
5. **Sustainable Compact Development:** New urban development must follow the logic for compact form at higher densities to be sustainable. Urban sites, especially in the vicinity of mass transit systems such as the Metro, can support higher densities than suburban or rural locations. In order to maximise such limited and valuable sites it is appropriate to consider a scale change in the interests of creating mixed urban developments with lower environmental impacts.

10.2 Overview

The heights strategy for each of the character areas is explained in detail in their respective sections throughout the Masterplan document, however the overriding features are as follows:

1. Swords Town Setting

There are four main elements defining the town setting.

● Topography and Landscape

The gently rolling topography and natural tree cover of the Swords environs provides few long distance or local views in and around the town. The skyline has no pronounced built forms or profile defining the Town Centre and only glimpses of fragments of buildings are visible within the general tree canopy. This enveloping nature of the natural tree cover is further enhanced by the verge planting of the R132 which effectively channels views along the roadway. St. Columba's Church and Round Tower, the only historic monument of any height, is hardly noticeable as it barely emerges from the tree line except in a few locations within the Town Centre itself. Views of St. Columba's Church and Round Tower from within the Town Centre shall be protected.

The town is endowed with extensive open space in the form, predominately, of natural landscape centred around the Ward and Broadmeadow River Valleys. These landscape assets are a key defining quality of the town. As such both their amenity value and potential to inform the identity of Swords should be considered with respect to development proposals and appropriate height or massing. Views into and from these public open spaces should be both protected and enhanced. The heights strategy identifies these open spaces and key views through them.

● M1 Motorway Presence

The motorway is relatively distant from and offers little prospect to the Town Centre. Prominent forms visible from the motorway are the industrial sheds of the Swords Business Park area to the east and recent housing developments further south. Much of the motorway is a cutting, except to the North where it rises to cross the Broadmeadow Estuary. Opportunities to announce the identity of Swords to passing traffic on this significant transport corridor exist on development lands adjacent the motorway in the Seatown and Lissenhall areas and these locations are identified on the heights diagram.

● R132 Approach

The strong tree lined parkway condition to the R132 limits the appreciation of height in the adjacent developed areas. Little if any buildings are visible from the R132 as it passes through Swords to the east of the current Town Centre.

There is no overt identification of the location of the Town Centre as the parkway condition of the R132 is nearly constant and there are no significant buildings (other than the Pavilions Shopping Centre). The parkway planting does an effective job in masking the disparate heights and forms of the differing uses along the R132. The height and density of the parkway trees play a positive role in projecting the image of Swords as a 'green' location and should be protected and enhanced as a key element of the vision of Swords as 'an emerging green city'.

● Residential Areas

Much of the existing residential areas of Swords are made up of 1 to 2 storey houses. The heights strategy for areas located outside of the Town Centre areas will respond to this existing context as well as the need to create a hierarchy of public spaces and a local centre focus where the heights increase to up to 4 floors to create a stronger sense of enclosure and intensity of activity.

● Metro North

The arrival of Metro North will change the current highway orientated R132 route through Swords by attracting development around the Stop locations. Some of these developments are subject to future LAPs, but higher buildings will logically be centred on the areas around the future Metro Stops of Fosterstown, Swords, Seatown, Estuary and Lissenhall. Heights will decrease towards the existing residential areas, but will inevitably create a distinctive scale change at the stops and articulate the route along the R132. Significant areas of undeveloped land, or areas requiring rezoning to reflect their current and future status within the Swords Town environs, lie adjacent most of the Metro Stop locations. Heights in these locations are set to reflect the local context, the opportunity to create a distinctive identity for the area and best practise on sustainable development densities/mix of uses.

10.3 Town Centre Key Areas

The Town Centre is divided into the following six key areas:

- **Metro Quarter**

Main public spaces, such as that at the Metro Quarter, can accommodate greater heights depending on their location and hierarchy within the spatial order of the town. The greatest heights will be located around Metro Quarter in keeping with sustainable development best practice policy to locate higher densities around public transport interchanges. As the effective gateway to Swords, the Metro Quarter has a rationale for being a significant orientating landmark within Swords Town and visible on the skyline and approach routes. A pair of taller buildings is proposed as a marker of the importance of the Metro Quarter to Swords Town and within County Fingal as a whole. These buildings will be subject to assessment of their overall architectural and urban design merit when considering appropriate heights and appearance.

- **Pavilions and Barrysparks Development Areas**

Building heights within the Pavilions and Barrysparks development areas are driven by their proximity to the Swords Stop and the need for a compact urban form with a mix of uses over the core retail levels. Heights range from 3-10 storeys depending on the use within each building and the need to create a modulated building profile to minimise any potential adverse impact on adjacent development and respect the historic character of the existing Town Centre core area. A heavily tree planted southern edge mitigates the scale change between the extended Pavilions area and Carlton Court.

- **Cultural Quarter**

A precedent for the main street area south of the Malahide Road has been set by the Penneys development set at 5-6 storeys. This height is to be respected by all buildings around the Cultural quarter. The exception to this is on the western side of Main Street where the heights have been limited to 2-4 storeys such that the views and amenity of the Ward River Valley Park will not be affected.

- **Historic Town Core and Civic Quarter**

The design of all new developments along the existing main street are to respect the existing parapet heights of 1-3 storeys, however this height may graduate to 3-5 storeys away from Main Street with frontage onto Forster's Way/New Street and the Ward River Walk, where topographical changes allow increase in height.

The Council offices currently present a high quality landmark building with an important and dominant function to perform in the Civic Quarter. New buildings which may be accommodated in this area should respect the landmark nature of Civic Offices, as well as the historic importance of Swords Castle.

- **Existing Residential Areas**

The height of buildings located at the edge conditions between existing communities and development areas will respond sensitivity to their locations. Landscaped buffer zones will be provided as well as decreased heights.

- **Industrial Lands**

Town Centre industrial lands are largely screened by existing trees on the site perimeters. The heights strategy assumes that such landscaping elements can be retained and enhanced to permit 3 to 5 storey developments in due course.

10.4 Landmark Buildings

The building height strategy sets overall recommendations designed to offer guidance on new developments in the plan area. Nevertheless, the plan recognises the importance of landmark buildings in making urban design statements which can enhance the skyline and the character of Swords generally. The plan provides opportunities for landmark buildings at several locations within Swords Town such as:

- Cultural Quarter
- Metro Quarter (east and west of the R132)
- West side of R132 at Fosterstown station
- East side of R132 at Seatown station
- West of M1 motorway in Seatown development area.
- End of North Street, opposite Balheary park

An increase in building height, at the discretion of the planning authority, may be accepted where it can be demonstrated that there is a strong urban design rationale for doing so, although it should be re-iterated that landmark buildings do not necessarily have to be high to achieve their objectives in urban design terms.

10.5 Tall Building Design

In all cases, the architectural quality of any building taller than its surrounding context, will be a primary consideration and the design, proportion and materials must be demonstrably of the highest quality. Tall buildings should also have a slenderness (height to width ratio) of 3:1. It should also be demonstrated that the scale of development proposed will not impact negatively on the amenity or microclimate of the open spaces in the area.

10.6 Heights Strategy Diagram

A diagram has been produced that sets out appropriate height ranges for the various character areas in the town. The heights on the diagram are shown in storey height ranges. This unit of measure has been adopted for ease of comprehension rather than using dimensions in metres. Storey heights are taken as residential floor to floor levels of around 3m with commercial buildings at an assumed 5m approximate floor to floor level. Allowance should be made for roof forms rising and set back above these storey heights: Typically these heights are in the region of 3m for housing and 4-5m for non residential uses, dependant on the building type and form.

Objectives

- To ensure development sites adjoining established residential areas provide adequate set backs, landscape tree screening barriers and building height limits to protect existing residential amenity.
- To ensure that the height and massing of any new development does not adversely affect any conservation area or protected monument/structure including proscribed vistas and views.
- To ensure that all development does not have detrimental effects on local microclimate, within or adjoining the development site, by either inhibiting sunlight/daylight penetration or down draughts off taller elements.
- To provide a limited number of strategically sited taller buildings on selected key development sites that will contribute positively to the current and future identity of Swords as a town.
- To provide higher density and increased height buildings around the proposed Metro stops to take advantage of the arrival of Metro North and the need to create a compact and sustainable form to an expanded Swords.
- To plan height in relation to a cohesive urban structure and respecting typical street width to height ratios of 1:1
- To retain and enhance the advantages that the height of the existing landscape of mature trees has in mitigating scale changes between different sites and building forms.
- To use height sensitively in building a distinctive skyline for the town, so forming a key component of the identity of the 'emerging green city of Swords'.

10.7 Summary

In summary the Masterplan's building heights strategy is based on established urban design principles: Respect for the identity and character of the receiving context, achieving sustainable densities in new developments, locating taller buildings in appropriate locations within the Masterplan area and creating attractive and pleasant open spaces and a quality public realm.



11.0 Sustainability Agenda

11.1 Sustainability and Transport

The Council will seek to promote more sustainable forms of mixed use development within the town, to increase the use of public transport and encourage the use of walking and cycling modes for trips at local level. Any future LAP /s for Swords will address all mobility needs and put in place a strategy to promote the development and use of public transport, as well as walking and cycling so as to reduce car traffic demands within the town, and particularly within the Town Centre. The development of town bus services which are fully integrated with Metro North will maximise the benefits of and the efficient use of the Metro for the benefit of those living and working in Swords.

11.2 Sustainability and Building Design

Buildings have considerable environmental impact, for example through operational energy, transportation of people between them, raw materials consumed by the building and use of land. Building design will reflect national sustainability and energy conservation policies and contribute to the creation of appropriate urban form within the development.

In their life cycle, energy is used in buildings in a number of different ways:

- In construction
- In operation, for lighting, heating and power
- For demolition, recycling and disposal

The importance of implementing more sustainable design in construction has been identified as a key part of supporting sustainable development overall. Increasing levels of building construction creates increased greenhouse gas emission which has in turn been linked to changes in climate and global warming. By far the biggest contribution to greenhouse gases is in emissions of carbon dioxide. The building sector is a major energy consumer, accounting for approximately 40% of total energy consumption, of which residential development accounts for 25% (SEI 2005).

There are many examples of buildings and technologies that demonstrate the feasibility and viability of achieving substantial reductions in CO₂ emissions from developments via good design, choice of construction method and location close to existing public transport interchanges.

Fingal County Council is committed as a priority to encouraging more sustainable development through energy-end-use-efficiency, and increasing the use of renewable energy in all new building projects in the designated area within Swords This will be achieved by improving the requirements as set out in Part L of the Building Regulations 2007 i.e.:

All dwellings to achieve a minimum BER (Building Energy Rating) of A3, all non domestic buildings to achieve a minimum BER (Building Energy Rating) of B2. These targets will be reviewed and revised by Fingal County Council in 2010.

It is an objective of Fingal County Council to promote sustainability as part of future developments within the Masterplan area and throughout Swords. The Council encourages superior energy design and technological practices in all developments. It is the policy of the Council to improve qualitative standards of sustainable design in proposed developments.



11.2.8 USE OF LOW ENERGY/ CARBON REDUCING TECHNOLOGIES

Once a development's energy consumption has been minimised, carbon reducing technologies should be investigated to further reduce the demands for fossil fuel based energies and in turn reduce the buildings' carbon emissions

11.2.9 PROVIDING REQUIRED ENERGY IN MOST EFFICIENT WAY

Efficiency measures and demand reduction will not reduce CO₂ emissions to the required national and international targets. Once energy use has been minimised the energy required should be provided in the most efficient way. Alternative means of generating energy that produce far less CO₂ need to be considered, such as local combined heat and power which is considerably more efficient than power supplied remotely with their attendant transmission and heat losses. The feasibility for local power generation on town scale, once the preserve of national utilities, is now both economically viable and well tested at an international level. Major European cities, such as Helsinki and London, and smaller towns are in the process of adopting this strategy as a cornerstone of their energy strategy. The proposed scale and mix of development in Swords Town Centre indicates that it would be appropriate to invest in a local power and heat generation infrastructure.

The design of the building services presents opportunities for energy saving and reduced carbon emissions through the careful selection of plant, controls and management of the services.

Non fossil fuels such as biomass of woodchips offers an alternative which is in effect carbon neutral but supply is currently too limited and is unlikely to be able to scaled up to supply power and large town scale.

Buildings can generate their own power via building mounted solar or wind energy collectors, but due to limited available surface area this can only supply typically fewer than 20% of the required power.

The following measures should be considered to increase the efficiency of buildings within a development:

- **Heat Recovery:** Incorporation of heat recovery into ventilation systems to allow the reclaim of energy.
- **Free cooling:** Plant and equipment optimised for use of free cooling for air and water systems when suitable external conditions exist.
- **Lighting:** The lighting in the development will incorporate low energy lamps with lighting controls incorporating daylight saving.
- **Variable speed pumping and fans:** The air and water systems will be designed using variable speed drives.

- **Condensing Boilers:** The boiler plant providing low temperature hot water will include for flue gas reclaim by condensing the 'latent' heat from the flue gas for use in these systems for space heating and air preconditioning.
- **CO monitoring:** The car park ventilation will incorporate CO monitoring to control the ventilation rate. The system uses less energy as the fans are set to operate at a lower extract rate using variable speed drives and only increase to full extract when the CO levels exceed certain values.
- **Building Management System:** BMS will be used to efficiently control and monitor the services within the development. The systems will incorporate: optimal routines, night set back and weather compensation.

11.2.10 WATER CONSERVATION

Water conservation measures should be incorporated into future developments. The amount of water used in the development should be limited through the use of water conserving devices such as low flush, dual flush systems and automatic cut off devices, in order to prevent continuous unattended drawing of water. Other water conservation measures include:

- **Grey Water Recycling**
Grey water recycling, which involves the reuse of waste water from sinks and showers, should be considered.
- **Rain water harvesting**
Rain water harvesting, which involves the collection of rainwater for reuse in WC and urinal flushing, should be considered.

11.2.11 WASTE MANAGEMENT

An integrated waste management strategy is required to ensure that waste strategy makes a positive contribution to the overall sustainability of any development. The waste strategy should form an integral part of the overall sustainable strategy for the site. Furthermore, all new developments will have to comply with Fingal County Council Waste Management Plan with regard to bin storage and bottle bank/textile sites etc.

A waste strategy shall address:

- Maximising the recovery of valuable resources, for reuse or recycling, before further treatment or disposal,
- Design and development of an integrated process with the objective of significantly reducing total direct and indirect emissions, particularly CO₂, methane and dioxins.

11.2.12 SUSTAINABLE URBAN DRAINAGE SYSTEMS/ STORM WATER MANAGEMENT

Fingal County Council has adopted the Greater Dublin Strategic Drainage Study (GSDSDS) Storm water Drainage Policy. In compliance with the GSDSDS, all new developments must commit to an integrated urban water management strategy that represents the best management practices in sustainable drainage. The primary objective to be addressed by any new development with regard to storm water run-off is to regulate, control and minimise storm water discharge from the proposed development site. The SuDS principle will require all new developments within the Swords area to provide flood control and runoff treatment.

The proposal for large scale units adjacent to Metro North maximise the opportunity for green roof technology promoting long-term sustainability. They form an essential part of the SUDS designs and add ecological, aesthetic and amenity advantages in addition to reducing the volume of runoff and attenuating peak flows.



12.2 Historic Town Centre

The first phases of development permissible immediately, subject to normal planning considerations, will be those relating to the on-going development and regeneration of the Historic Town Centre of Swords. It is essential that this area be permitted to grow and develop within this first phase so that it keeps pace with and complements the development which will be taking place in the Town Centre expansion areas (namely in Penneys, Pavilions and ultimately Barrysparks development areas).

Development which contributes or fast tracks the Council's objective of delivering Cultural and Community uses and public places, such as those proposed at the Cultural Quarter and Civic Quarter, will also be given priority as the creation of these areas is an integral element of maintaining and enhancing the vibrancy of the historic Town Centre and creating a balanced and well serviced emerging city. To encourage and complement development within the Town Centre, a Public Realm and Traffic Management Strategy for the Town Centre will be completed within 12-24 months of the preparation of this Masterplan.

12.3 Town Centre Expansion Areas

The new mixed-use and retail expansion areas can be divided into several phases, namely:

1. Phase 3 of Pavilions Extension (retail, office, and residential development, integrating with the existing Shopping Centre, the Metro and R132)
2. Phase 1 of Barrysparks Development (which comprises a first phase of development on the MC zoned land immediately east of the R132, such as would be sufficient to provide a commercial/retail interface with the Metro on the eastern side of R132).
3. Phase 4 of Pavilions Development, which comprises the development of South Street, the interface between existing Pavilions Shopping Centre and Dublin Road, and the first phases of the proposed Cultural Quarter (in particular the library, theatre and Cultural Square).
4. Phase 2 of Barrysparks Development (development of the remainder of MC zoned land east of R132).

The ancillary social and transport infrastructure which will be required to complement and serve these retail developments includes:

1. Metro Box* and associated ramp structures.
2. Metro Plaza and pedestrian connections to development east and west of R132.
3. R132 Bus Bridge and connections east and west of R132 (The Rise Street).
4. Metro North.
5. Library.
6. Theatre.
7. Cultural Square.
8. Ward River Place.
9. Drynam Link Road

12.4 Indicative Phasing for Town Centre Expansion Areas

It is acknowledged that availability of Water Services (in particular Waste Water Treatment capacity) and market forces may influence phasing, as too may changes in national or local government policy, however, at this time it is anticipated that phasing will broadly follow that outlined hereunder.

Phase 3 of Pavilions will be initially accommodated with modest and minor augmentation of the local road network, in particular along the R132 and Malahide Road Roundabout. The design and development of Pavilions Phase 3 will be required to facilitate its ultimate integration with Metro North and/or Barrysparks development, as and when these developments arrive in Swords.

Pavilions & Barrysparks Interface. It is envisaged that the proposed Metro Box* will be required to serve the anticipated vehicular traffic movements associated with the ultimate development of Swords east and west of R132. The timing of delivery of the Metro Box* and its integration with the development of the town expansion areas will be considered within development management context.

Upon arrival of Metro North and specifically the opening of the Swords Stop, the Plaza/Podium and pedestrian link through the proposed Pavilions Central Street will be required.

Independent of Metro North arriving, it is envisaged that the Metro Box*, Podium/Level 1 pedestrian linkage and Bus Bridge will be required in order to integrate the existing and expanding retail area west of the R132 to the new retail expansion area identified east of the R132 (i.e. Barrysparks MC zoned lands).

Phase 1 of Barrysparks will require the completed Metro Box*, the second element of the pedestrian link, connecting Central Street west of the R132 to that east of the R132, proposed R132 Bus Bridge (linking Malahide Road, The Rise, and Barrysparks development area) and completion of proposed Drynam Link Road.

Having regard to its current Land Use Zoning, the development of Barrysparks 'MC' zoned lands requires the adoption of a Local Area Plan.

Phase 4 of the Pavilions will deliver South Street (residential street), the interface between existing Pavilions Shopping Centre and Dublin Road, and the first phases of the proposed Cultural Quarter (at minimum the proposed library, theatre and Cultural Square). The remainder of the Cultural Quarter, i.e. Ward Place and enhancement of the Park, will be delivered as part of the commercial development of lands on the west side of the Dublin Road.

Phase 2 of Barrysparks will follow the completion of the Community and Cultural aspects of the Cultural Quarter. Assessment of the economic strength and vitality of the historic Town Centre area will be required in advance of extending the retail area further eastwards in order to ensure that Historic Town Centre Area, in particular Swords Main Street, can survive commercially as the commercial and retail draw moves eastwards. It is envisaged that the creation of a successful Cultural Quarter at the southern end of Main Street, as well as ensuring a mixed-use Town Centre, will substantially redress some of the Council's concerns in this respect.

* Metro Box is an underground vehicular connection across the R132, associated ramps, and necessary realignment of the R132 locally, in order to provide for access to lands zoned 'Swords Town Centre' east and west of the R132, as shown in the Fingal Development Plan 2005 to 2011. This Metro Box plan was adopted under Part 8.

Appendix 1 Urban Design Analysis

This appendix identifies the existing key characteristics of the historic Town Centre area in terms of its structure, function, built form and landscape qualities. This urban design appraisal was undertaken to help identify those assets and features of Swords Town Centre that should be retained and enhanced, as set out in Section 2 Urban Design Guidelines.

The appraisal of the town is presented under the following headings:

- Land Uses
- Topography
- Views
- Public Open Space/Public Realm
- Legibility
- Connections
- Parking
- Safety and Security
- Protected Structures
- Record of Monuments and Places
- Development Block Size
- Building Orientation
- Quality of Building Frontages
- Urban Grain
- Building Heights and Massing
- Architectural Features



Western side of Main Street



Western side of North Street

Land Uses

THE RETAIL/COMMERCIAL CORE

Swords' retail and commercial core has a typically wide mix of uses for a town of its size. It is possible to identify clusters of activity which contribute significantly to the character of the town.

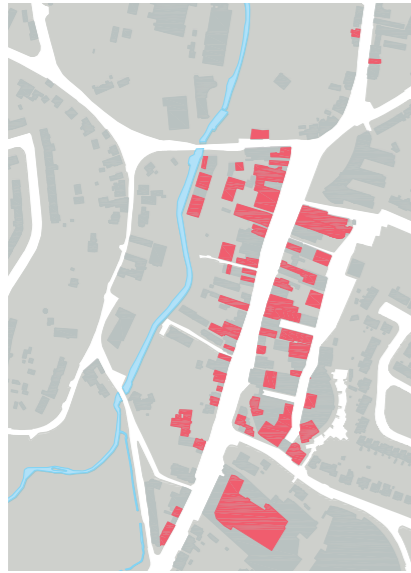
Retail at ground floor levels is mainly concentrated within the Main Street area and at the junction of the Malahide Road and Dublin Street. The Main Street is dominated by small scale comparison goods retailers and small scale convenience stores such as Centra and Londis. Large convenience goods retailing are limited to the key anchor of Superquinn and Dunnes Stores at the "Pavilions Shopping Centre" to the southeast of the town. Small offices are distributed around the Town Centre at Main Street predominantly at first floor levels. These uses are predominantly daytime uses.

In terms of evening uses the retail and commercial core is the best used area. However the number of these evening uses are few with approximately three public houses and a few take-aways located along the Main Street. The desire for Swords town centre is to ensure that the vitality of the evening economy is promoted further and sustained in a managed way, taking into consideration the impacts on neighbouring land uses.

Land uses in the retail and commercial core zone are inextricably linked to the convenience and comparison retail offered by the Pavillions shopping centre located to the southern edge of the town centre. This centre draws a large number of visitors to Swords however, these do not necessarily generate many linked visits to the Main Street area.

Key urban design interventions such as improved connectivity and a number of public realm enhancements from the Pavilions to the Main Street could address this.

Swords retail and commercial core is unlikely to be attractive to large format retailers due to its tight urban grain and plot, but could accommodate small scale specialist retailers. A pavement culture with spill out areas from restaurants and cafés could be fostered along the Main Street aided by the town's unique scale and quality of architecture which will also aim to diversify the current evening economy offer.



Ground Floor Commercial



Ground Floor Office



Ground Floor Public & Institution



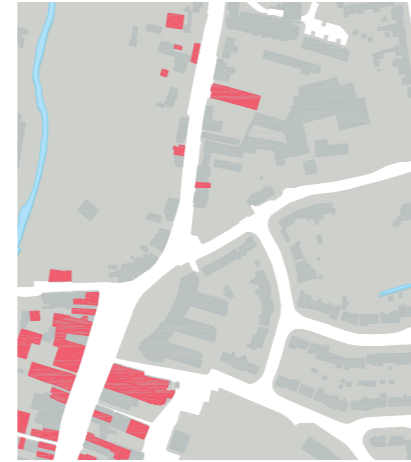
Ground Floor Residential



Ground Floor Vacancy

CIVIC/COMMUNITY – Seatown Road

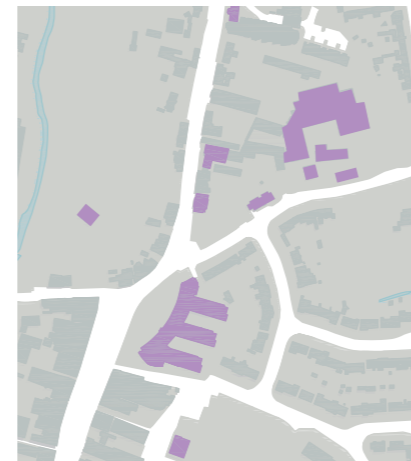
Seatown Road is the community and civic centre of the town. The character area lies east between North Street and Main Street. School, community and civic buildings are located along this route that includes Fingal County Hall as well as Swords Castle. The Seatown Road has the potential to become a strong and attractive pedestrian route into the Town Centre in light of the proposals from Metro North to locate a stop at Seatown.



Ground Floor Commercial



Ground Floor Office



Ground Floor Public & Institution



Ground Floor Residential



Ground Floor Vacancy

COMMERCIAL/RESIDENTIAL

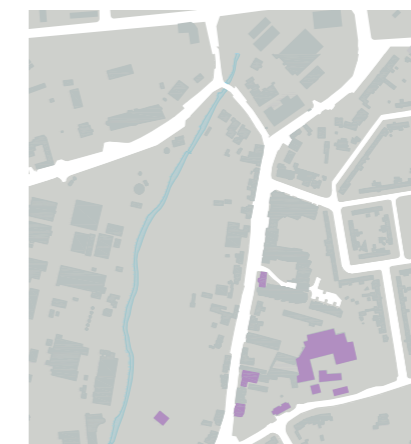
The North Street area is characterised by retail with a small mix of established terraced housing, some apartments as well as a hotel, offices and public houses. North Street also contains a number of independent specialist shops including a bicycle repair shop, garage repairs, motor sales, a carpet specialist and a photographers gallery. The area lacks a sense of enclosure, partly because of weak building lines and exposed surface carparking areas. There is also a high rate of vacant buildings (up to 27% of existing building stock). This provides opportunities for redevelopment of buildings and sites within this character area.



Ground Floor Commercial



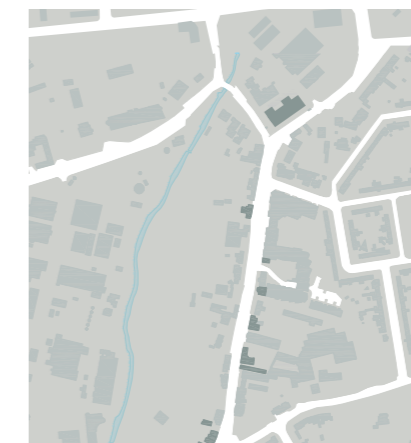
Ground Floor Office



Ground Floor Public & Institution



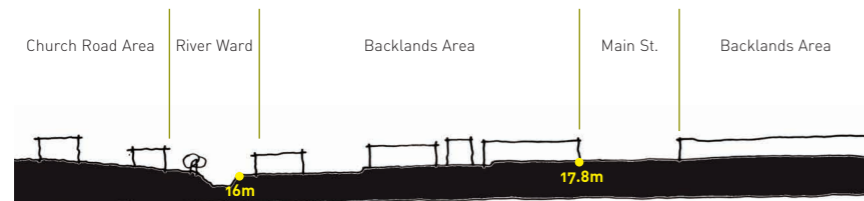
Ground Floor Residential



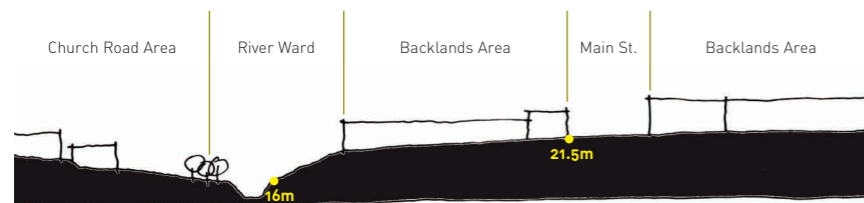
Ground Floor Vacancy

Topography

Building footprints and streets should follow the line of contours, with buildings 'stepping up' slopes where they cross contours. The Main Street area, North Street area and Seatown Road area are set on relatively flat ground, with a sharp drop in gradient to the west towards the River Ward. Changes in levels and slopes here can provide a distinctive character and sense of place. This offers an opportunity for the roofscape to emphasise the natural landform.



Section AA showing existing levels and indicative building heights



Section BB showing existing levels and indicative building heights



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Views

The composition of views and vistas adds character, and reinforces legibility and an understanding of the place. A varied sequence of connecting views, which lead or draw the eye from one feature to another, creates interest. This should be a key consideration when designing the new blocks and streets.

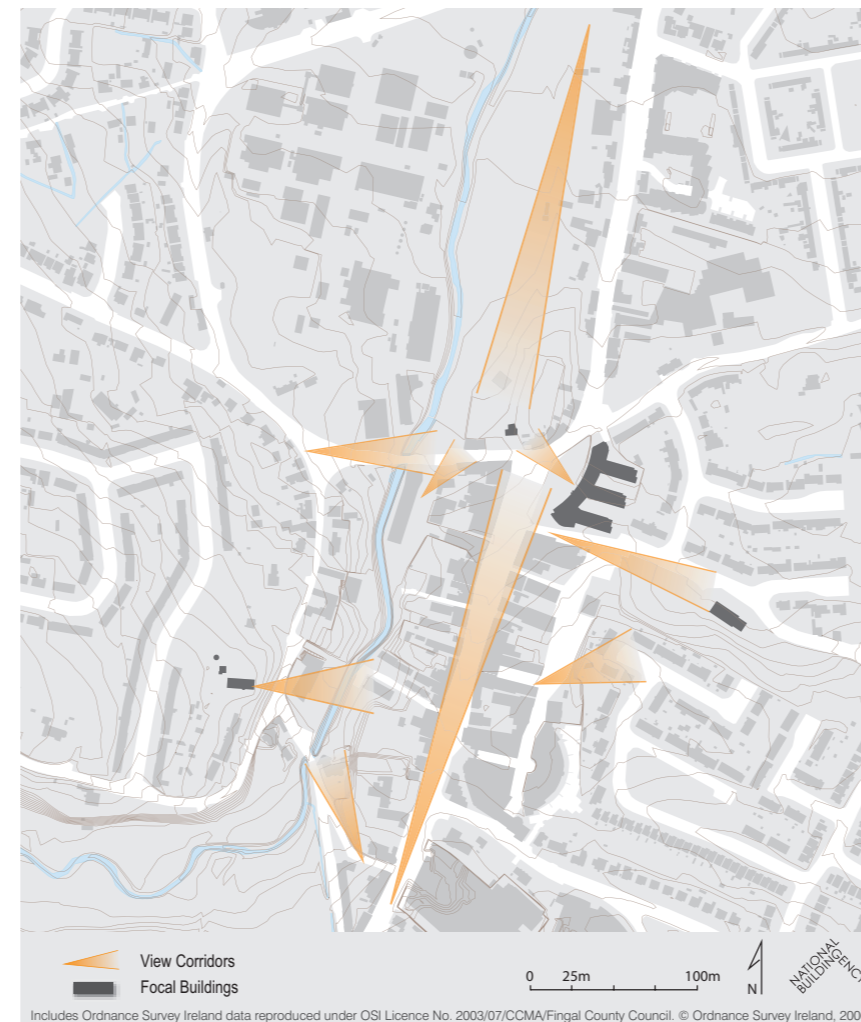
Swords' flat topography makes street-long vistas particularly significant. Views of Swords Castle from the Main Street and Dublin Street are important, creating a visual connection to the North Street area. It is the intention of Fingal County Council to open views to and develop a positive urban space around Swords Castle as part of a separate project.

The adjacent figure highlights the key view corridors to existing focal buildings within Swords.

The views to the historic core of Swords at St Columba's Church and round tower from the Main Street, Church Road and Well Road are also an important reference point.

There are views to the spire of St Columille's Roman Catholic Church from Main Street and New Street providing a prominent landmark from a distance over Swords consistent low building height.

There are also strong views of the County Hall, which is a new landmark building from Bridge Street, Main Street and North Street.



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Public Open Space/Public Realm

THE RETAIL/COMMERCIAL CORE

The quality of the public realm in the Town Centre needs to be improved. In recognition of the importance of the public realm to the vibrancy, vitality and quality of the Town Centre, Fingal County Council will prepare a detailed public realm strategy. A key element of this strategy will be developing a civic space at Swords Castle.

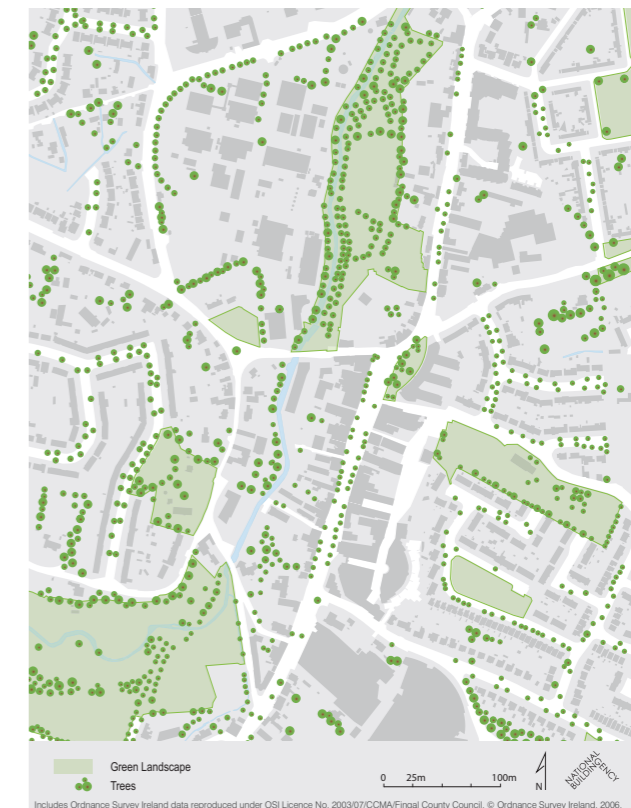
Planting and greenery is a distinctive feature of the retail/commercial core. The wide pavements of the Main Street accommodates large mature treestands (Plane variety). To supplement this there is a small parcel of green open space and mature tree stands of Holmes Oak (Evergreen Oak) north of the Main Street associated with the entrance to the Fingal County Hall. Moving westwards from the Main Street towards the river there is extensive treestands and greenery. There is also evidence of a historic stone cut wall along Well Road which contributes to a positive character. The River Ward currently is an underutilised asset which the Town Centre effectively turns its back on. There is potential to front buildings onto the river and other green open spaces to provide surveillance, an active edge, and an attractive visual aspect.

CIVIC/COMMUNITY CORE

There is little soft landscaping or street trees in this character area except for the greenery associated with the Parochial House and Swords Castle.

COMMERCIAL/RESIDENTIAL

In this character area streets are planted with few street trees and a hard landscape predominates. The area also enjoys access to open space within Swords Town Park to the west of North Street.



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Legibility

Landmark features allow visitors and residents to orientate themselves, creating a legible and easily navigable environment. This can include focal buildings, intimate spaces, street trees etc. Within the proposed framework for Swords developing landmark features are critical to maintain a sense of legibility and an understanding of the place.

Existing aids to legibility within Swords include views from within the Main Street northwards to Swords Castle, St Colmcille's and the Fingal County Hall's building and trees which greatly enhances the street's legibility. To the south of the Main Street glimpse views to Columba's Church and round tower further enhances orientation.

East-west connections to the Main Street, particularly along the Malahide Road lack landmark features that could assist in the legibility of the area. Opportunity site no.6 outlined in chapter 3.0 specifically addresses this issue.

Swords Town Centre is also surrounded by barriers to legibility. The river creates an edge to the west along which the settlement spreads to the north and the R132 acts as a barrier to the east.

Connections & Routes

THE RETAIL/COMMERCIAL CORE

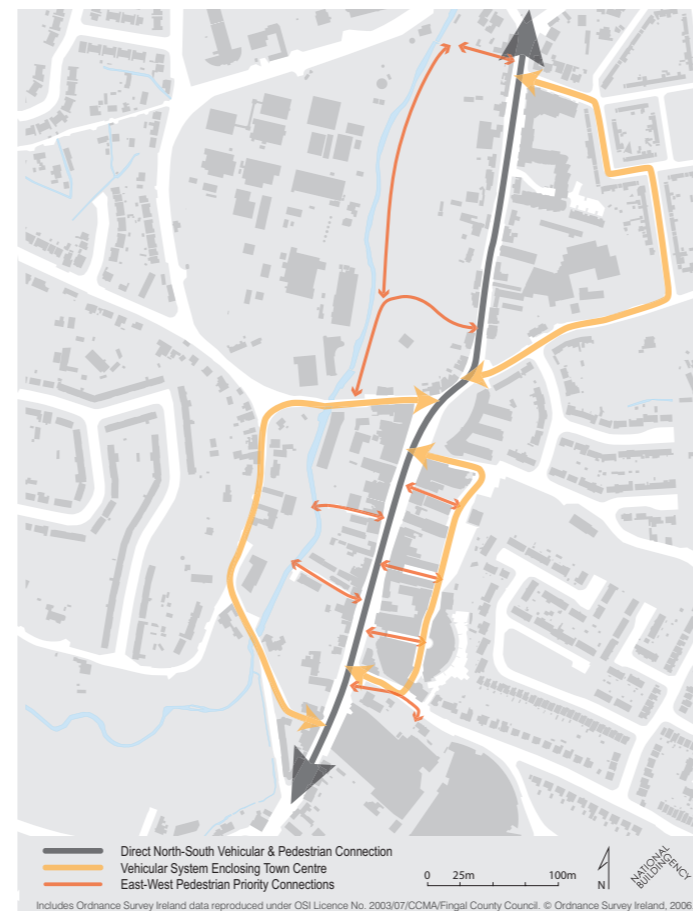
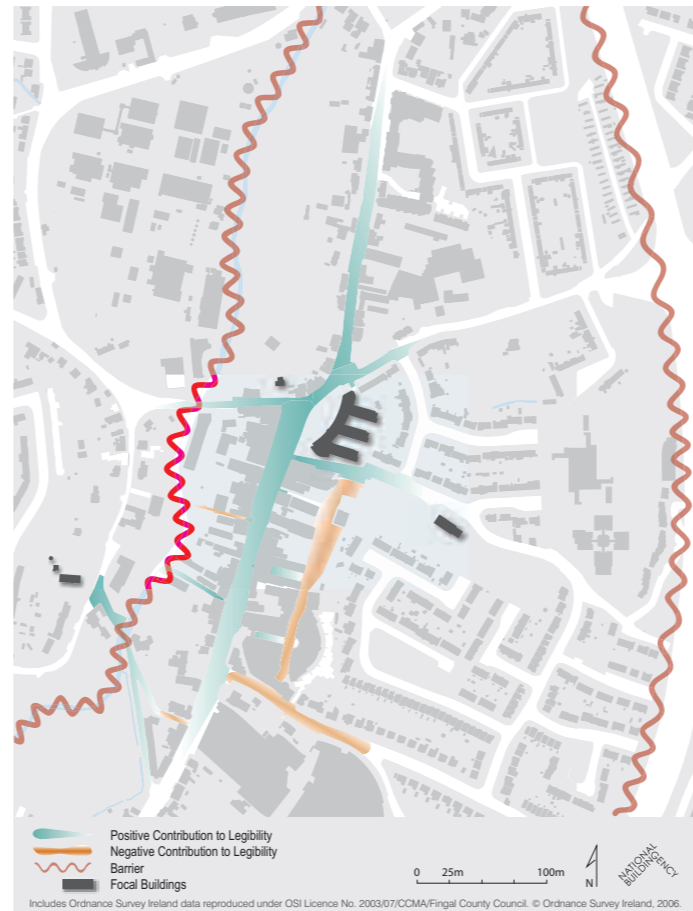
The Main Street's historic layout includes a number of east-west pedestrian links. These include River Mall, Colmcille Court to the west of Main Street and Swords Town Centre Mall, Mainscourt and The Plaza to the east of Main Street. To the west of the Main Street these connections offer good access to the River Ward's amenity. The Main Street has evolved to accommodate vehicles at the expense of pedestrian use. There are also connectivity problems for pedestrians travelling between the Pavilions Shopping Centre and the Main Street, causing a loss of potential visitors to the Town Centre. To the west of the Main Street pedestrian and vehicular circulation is constrained by the river with only two crossing points at Bridge Street and Well Road. The pedestrian walk along the river terminates at the boundary of Bridge Street thereby restricting connectivity from the walk to Bridge Street and Swords Town Park beyond. There is also restricted pedestrian access to the east of the Main Street from 'the Crescent' development to New Street and beyond.

CIVIC/COMMUNITY CORE

Seatown Road provides direct access eastwards to the R132 and the M1 motorway. With the proposed Metro North extension to Swords and the delivery of key road links it will be possible to give more priority to pedestrians on the Main Street and increase its role as a pedestrian friendly urban centre.

COMMERCIAL/RESIDENTIAL

North Street's traditional north south route remains a busy through route southwards to the Main Street. A segregated pedestrian route through Swords Town Park is provided from the northern end of North Street southwards down to Main Street.



Parking

THE RETAIL/COMMERCIAL CORE

The Town Centre offers extensive on-street parking along the wide Main Street as well as around the smaller backland streets at New Street and along Bridge Street. Short term car parking will be provided for to serve the retail and commercial core along Main Street.

At present, surface car parking areas that are open to the public are highlighted in the figure adjacent in red. The estimated car parking capacity of these is in the order of 170 spaces in total. One basement car parking facility at New Street has a car parking capacity of approximately 50 spaces. Private pocket surface car parking is predominantly located within areas to the rear of the Main Street. These areas occupy prominent areas in the Town Centre and some may be potential redevelopment sites. The Town Centre lacks a coherent parking strategy, with an array of car parks and informal parking areas. Providing for adequate levels of short term parking will be critical if Main Street is to thrive in the future.

CIVIC/COMMUNITY CORE

Within the civic community character area there exists one surface car park for employees of Fingal County Council. This car park has a capacity of approximately 90 car parking spaces. On street parking can constrict narrow streets residential areas (Seatown Villas) adjacent to Seatown Road.

COMMERCIAL/RESIDENTIAL

On-street parking is provided in bays along North Street. The bays are reasonably well defined through the use of road markings. Double yellow lines restrict parking on certain areas of the street.



Safety and Security

The principles of 'Safer Places: the planning system and crime prevention' by the ODPM should be incorporated as part of any new development. The better practice guidance sets out the seven design principles that are sought in new and existing developments:

- Access and movement
- Structure
- Surveillance
- Ownership
- Physical protection
- Activity
- Management and maintenance

Please refer to Appendix 2 of this document for a summary of the document principles.

THE RETAIL/COMMERCIAL CORE

Whilst the Main Street experiences good safety and security measures, the backland areas and in particular the area fronting onto the river does not benefit from natural surveillance. Orientating building fronts to the river, CCTV and good lighting can help reduce the feeling of insecurity in this particular hot spot.

CIVIC/COMMUNITY CORE

The Seatown Road is popular with pedestrians travelling from the centre eastwards to the R132. However differing front boundary treatments and building setbacks make for differing levels of street security. Some are open plan, some have low boundaries whilst others have high walls and dense hedging.

COMMERCIAL/RESIDENTIAL

While buildings along North Street front onto the street they do not overlook the Town Park. The entrance laneway to Swords Town Park and the back lane associated with the terraced housing at the northwest of North Street are poorly lit and lacking in natural surveillance.



Inactive frontage to the River Ward

Protected Structures

THE RETAIL/COMMERCIAL CORE

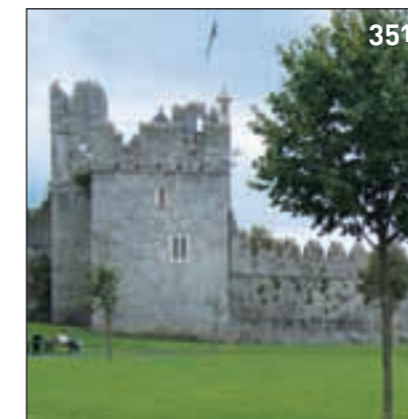
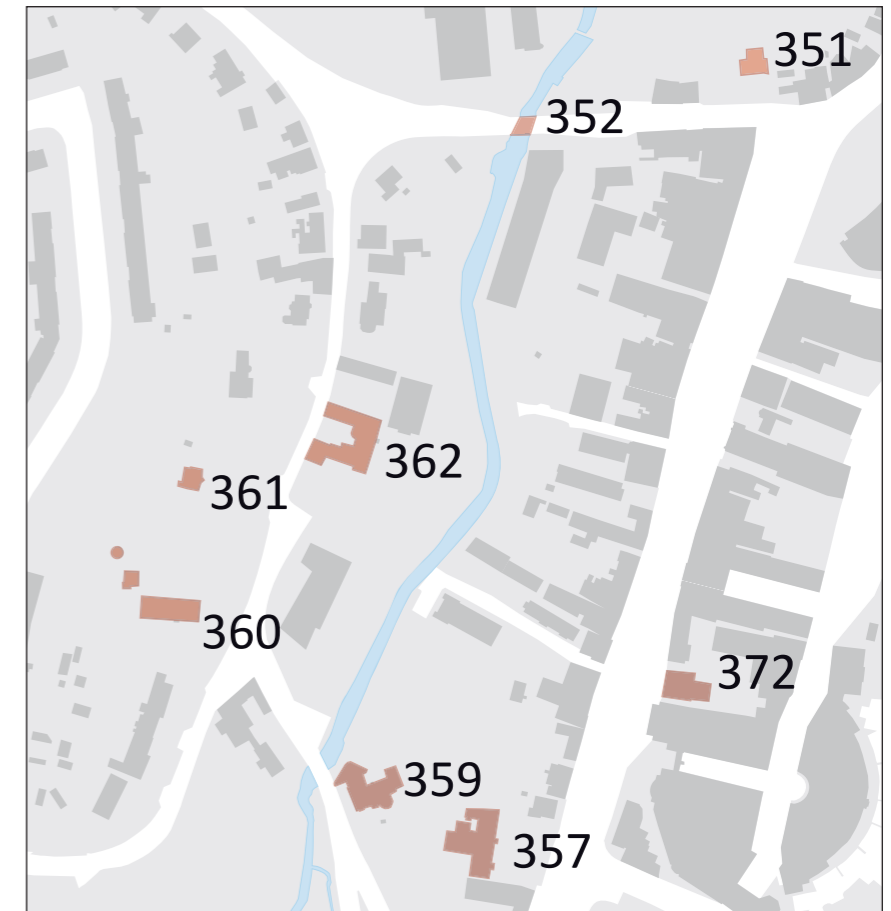
The Main Street has a strong and distinctive streetscape, which should be protected if the town is to retain its unique character. Swords origins as a market town are evident in its wide Main Street, and its medieval importance is made clear in the surviving deep and narrow plots. There are several protected structures within the area of the Main Street.

These include the following Protected Structures:

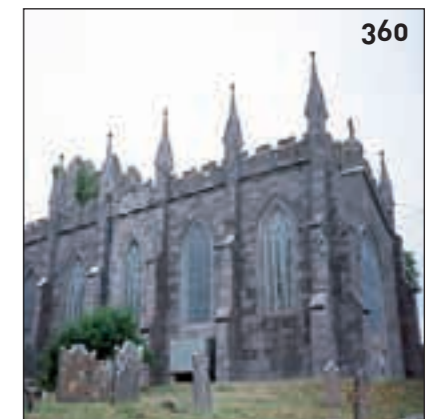
Record No.	Description
351	Swords Castle
352	Mill Bridge
357	Old Borough School
359	The Old School House Restaurant
360	St. Columba's Church (C of I)
361	Sexton's House
362	Old Vicarage
372	National Irish Bank

Structures of note include:

- 351 Swords Castle**
Swords Castle was built as the manorial residence of the Archbishops of Dublin around 1200. The castle covers a large pentagonal walled area of nearly 1.5 acres with a tower on the north and an impressive gateway complex on the south.
- 352 Mill Bridge**
A single-arch rock faced limestone road bridge over the river Ward. Its construction dates from around 1870. The bridge consists of an ashlar limestone coping to parapet as well as a plaque inscribed with 'Erected in the memory of Peter Wilson who gave his life for Irish freedom in the rising of Easter Week 1916, Ar dheis De go raibh a anam'.
- 357 Old Borough School**
The old school is a detached nine-bay two-storey former national school house built around 1810 on a T-shaped plan. The roof is double pitched consisting of slate, concrete ridge tiles, granite coping. The openings are square headed with granite sills, timber sash windows, timber panelled door and overlight.
- 359 The Old School House Restaurant**
A detached nine-bay two-storey former national school house built around 1810, on a T-shaped plan. The building is attributed to architect Francis Johnston. The roof is double pitched with slate, and rendered chimney stacks. The walls are smooth rendered. The openings are square headed with 6/6 timber sash windows. There is a round headed door opening to right porch with overlight.



351



360

360 St Columba's Church (19th century church plus ecclesiastical remains, Round Tower, Graveyard, Whetstone, Cross, Sheela-na-gig)

St. Columba's Church of Ireland is a detached gable-fronted ashlar limestone Gothic style single-cell Church of Ireland church dating from c.1815. It was designed by Francis Johnston. It has six-bay side elevations with stepped buttresses, pinnacles and crenellated roof parapet. The adjoining round tower is an instantly recognisable landmark from the town centre of Swords. The tower is 26m high with a stone cross at its apex, an unusual feature in Irish round towers.

361 Sexton's House

A detached three-bay single-storey ashlar limestone Tudor style sexton's house built around 1870 with a central single-bay projecting porch on a triangular plan. The roof is double pitched and hipped with slate, concrete ridge tiles, a cut-limestone chimney stack, timber eaves, cast-iron rainwater goods and decorative timber barge boards to left gable end. The walls consist of coursed cut-limestone and cut-limestone quoins. The openings consist of pointed-arch window openings with cut-stone chamfered sills and surrounds.

362 Old Vicarage

A detached three-bay two-storey former vicarage built around 1675, with projecting two-bay two-storey block attached to right-hand side. The building has been extensively renovated and around 1990 the building was developed into apartments. The roof is gable fronted, double-pitched and hipped, slated with some replacement fibre-cement slate with rendered chimney stacks, yellow clay pots, timber eaves and uPVC rainwater goods. The walls are random coursed rubble stone with traces of roughcast/nap render to right side elevation. The openings are square-headed with granite sills with replacement uPVC casement windows and replacement tongue-and-groove timber panelled doors.

372 National Irish Bank

This is a five-bay single-storey ashlar granite bank built around 1920, with a hipped roof behind a parapet wall. The walls are granite ashlar to the front. The openings are square-headed with granite sills.



362



372

CIVIC/COMMUNITY – Seatown Road

This character area around Seatown Road has a different character, and primarily consists of historic community and civic buildings with a loose urban grain. St Columcille's Church is located within the area and represents the town's most readily identifiable landmark. Both the church and its setting should be protected, with particular attention given to key views of the steeple.

The following Protected Structures include:

Record No.	Description
350	Courthouse
353	Former Teacher's Residence – Accord
354	Swords Youth & Resource Centre
356	St. Columcille's Church (RC)
358	Parochial House

Protected Structures of note include:

350 Swords Courthouse

This detached three-bay single-storey courthouse was built around 1843, with a double-height central block and flanking single-storey recessed wings. The walls are coursed limestone with limestone quoinings with square headed window openings.

353 Former Teacher's Residence – Accord

This building is a detached three-bay two-storey roughly dressed rubble stone house built around 1890 a with gable over its central bay.

354 Swords Youth & Resource Centre

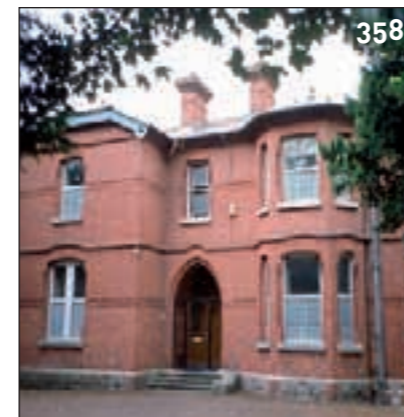
A detached six-bay two-storey former school built around 1865, with recessed end bays. The building has recently been sensitively refurbished. The roof is hipped and slated. The openings are square headed with rendered architraves. The door entrance is square headed and flanked by rendered Tuscan pilasters and a timber boarded doors with overlight above.

356 St Colmcille's Roman Catholic Church

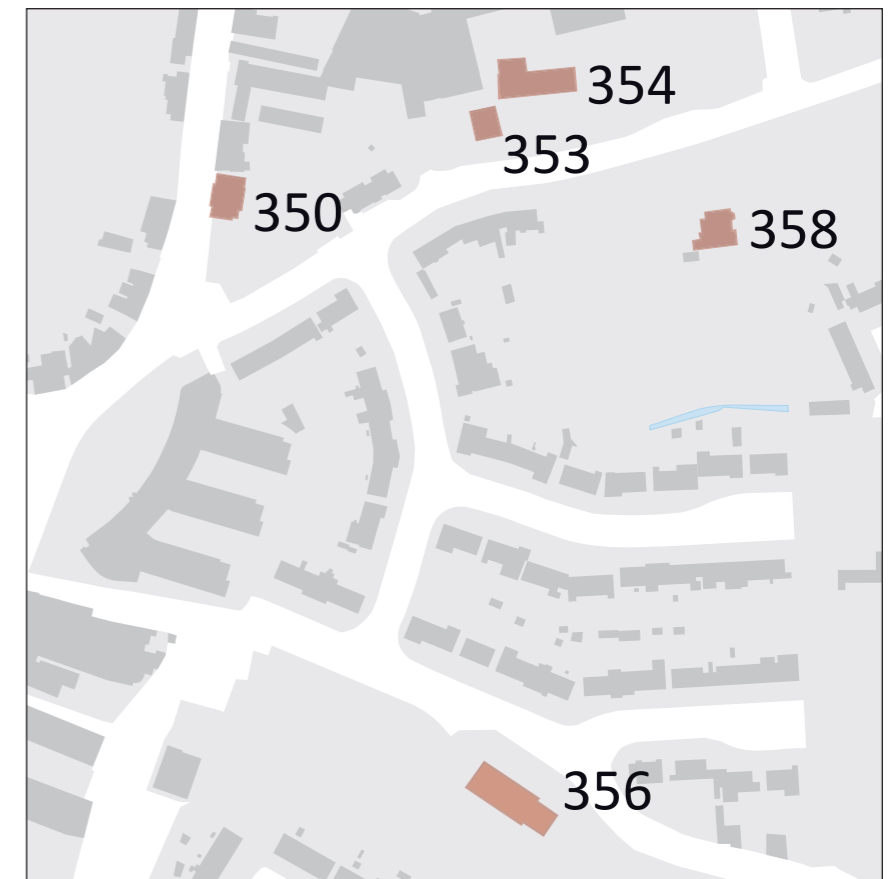
The Church is a detached three-bay gable-fronted single-cell Roman Catholic church built around 1830 a with single central bay having a tower and spire. The walls are nap rendered, ruled and lined with a granite course and coping.

358 Swords Parochial House

This is a detached three-bay two-storey red brick house built around 1895, with recessed entrance porch flanked by projecting full height bays. The walls are red bricked with a flemish bond, rubble stone plinth course, with chamfered yellow brick coping, red brick chevron string courses and moulded brick string courses.



358



350



353



354



356

COMMERCIAL/RESIDENTIAL

The North Street area has a strong and distinctive streetscape of protected buildings dating from the late 19c to the early 20c. The buildings consist of fine frontages with distinctive limestone and red brick detailing. The following Protected Structures include:

Record No. Description

- 346** Carnegie Library
- 347/348** 42-44 North Street
- 349** Former New Borough Female School

Structures of note include:

346 Swords Carnegie Free Library

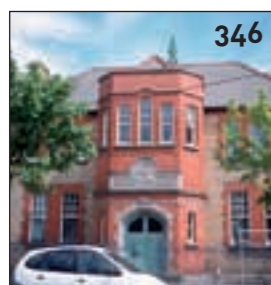
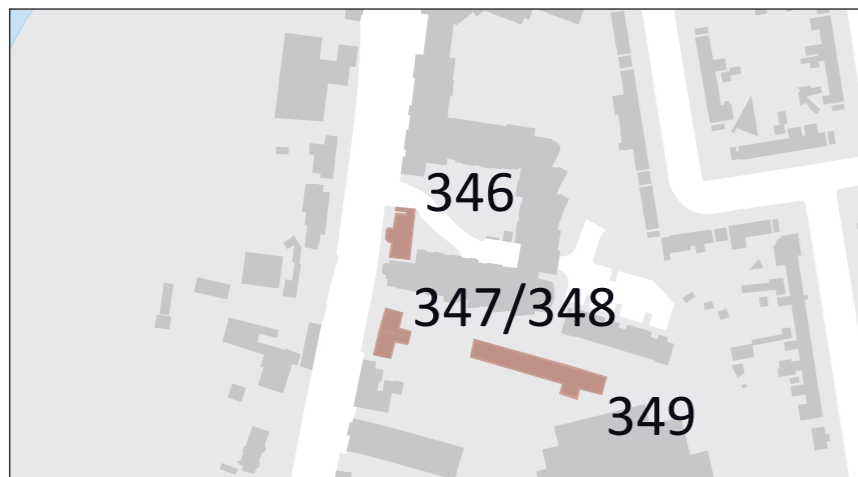
The library is a detached three-bay two-storey yellow and red brick building built around 1908 with a projecting canted central bay. The walls consist of a limestone plinth course with a yellow brick laid in an English garden wall bond.

349 Former New Borough Female School, Swords, Fingal

The training centre is a detached thirteen-bay single-storey former school built around 1890. The roof is double pitched roof with overhanging timber eaves supported by timber brackets with a natural slate roof and rendered chimney stacks. The openings are square headed windows with granite cills.

347/348 42-44 North Street

This pair of semi-detached three-bay two-storey former school residences were built around 1890 in the same fashion as the former teachers residence on Seatown Road. The roof is double pitched slate with projecting eaves and timber barge boards. The walls are coursed limestone rubble walls with a red brick chimney breast to gable wall.



Schedule of Monuments and Records

There are a number of monuments and places within Swords that are included in the Record of Monuments and Places (RMP) produced by the DoEHLG. These monuments and places are afforded statutory protection under Section 12 of the National Monuments (Amendment) Act 1994 and Policy 133 and 137 of the GCDP 2003-2009. Section 12 (3) of the above Act requires that any interference/work to a known archaeological site should be notified in writing to the Minister two months prior to the commencement of work.

The following provides a list of the Record of Monuments and Places within Swords Town Centre:

SMR No.	Classification
DU011-034002	Ecclesiastical Enclosure
DU011-034003	Graveyard
DU011-034004	Church
DU011-034005	Round Tower
DU011-034006	Cross
DU011-034007	Cross-Slab
DU011-034008	Sheela-Na-Gig
DU011-034009	Graveslab
DU011-034010	Graveslab
DU011-034014	Architectural Fragment
DU011-034017	Burial
DU011-034018	Burial Ground
DU011-034001	Castle – Anglo-Norman
DU011-034012	Mill – Unclassified
DU011-070	Font



Development Block Size

THE RETAIL/COMMERCIAL CORE

Swords Town Centre block layout is typically organic in form and layout, with a variety of block sizes. As a result they exhibit a human scale and finer pedestrian grain, with a choice of interconnecting streets and routes.

Example of block sizes that exist within the Town Centre vary in dimension east and west of the Main Street. To the west they are typically 80m in depth and 140m wide. In contrast the east of the Main Street displays proportions in the range of 40m in depth by 50m wide.

New block structures should include some flexibility and variation in size and shape, to promote the organic form of the Town Centre, whilst generally maintaining prevailing block dimensions.

CIVIC/COMMUNITY CORE

Example of block sizes that exist within the civic/community character area are similar in dimension to those west of the Main Street.

COMMERCIAL/RESIDENTIAL

Block sizes that exist within the commercial character are quite large in comparison to the character areas found at Main Street and Seatown Road.



Urban Grain

THE RETAIL/COMMERCIAL CORE

The arrangement of buildings within the block is defined principally by their individual plan form, height and scale. Building widths range on average from 8m to 10m in width on the western side of the Main Street. The historic narrow frontage approach establishes a building line of linked dwellings and continuous building frontage. This provides a very efficient use of land with a high building density. On the eastern side of the Main Street there has been a loss of grain over the years where plots have been amalgamated. This has detracted from the overall character of the street. As such, the building widths are much wider with an average dimension of 15m to 20m.

Building depths vary considerably between both sides of the Main Street also. On average building depths are 26m on the western side of the Main Street and 22m on the eastern side.

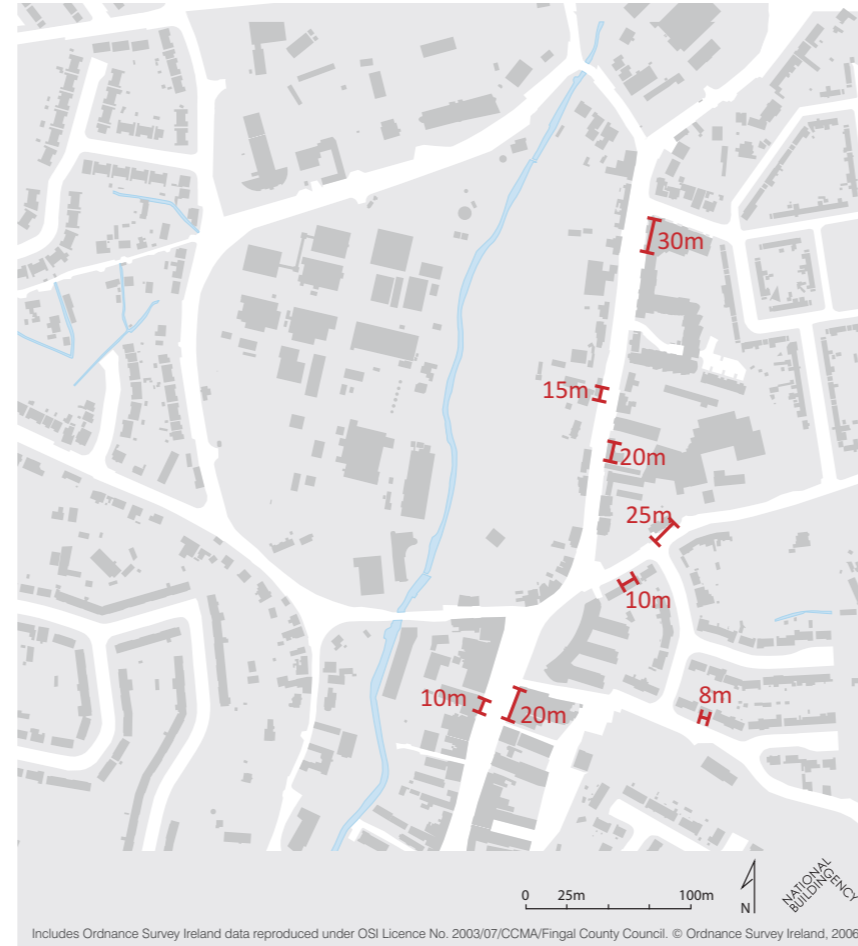
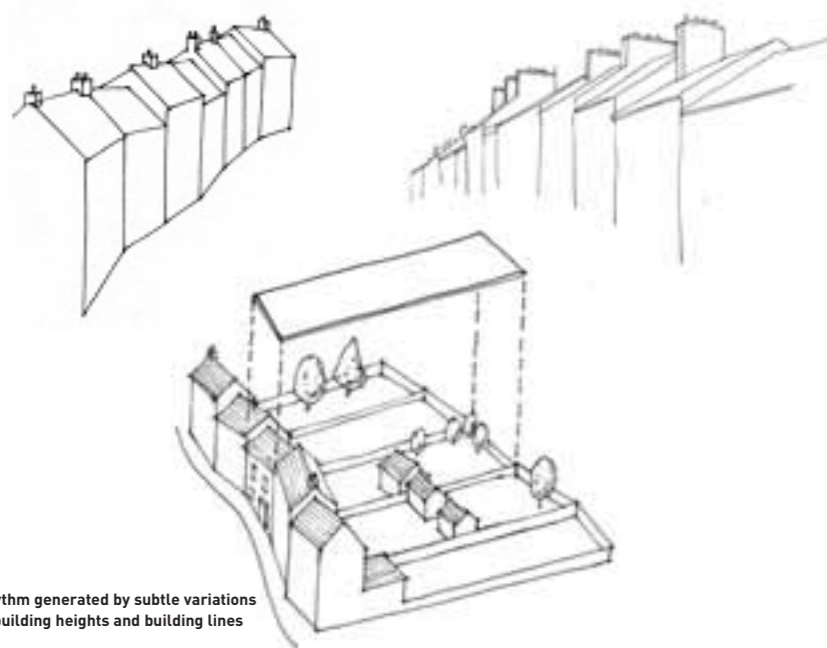
CIVIC/COMMUNITY CORE

Within this character area the building plots are a mixture of detached and semi-detached buildings. On the northern side of Seatown Road the building widths are at least 25m wide. To the southern edge of Seatown Road at the junction of Dublin Road and North Street, the widths are significantly less at 10 wide.

Building depths vary considerably between both sides of Seatown Road also. On the northern side the depth is on average 25m. On the southern side the depths significantly more at 40m in depth.

COMMERCIAL/RESIDENTIAL

North Street, to the east has a very distinctive tight-knit urban grain, which breaks away to a looser grain towards the northern end of North Street. North Street to the west has a poorly defined consistent urban grain with large setbacks which fails to provide a legible layout. The building width of between 10m and 15m approximately is prevalent on the western side of North Street and on the eastern side of the Main Street a building width of 20m-30m predominates.



Quality of Building Frontages

THE RETAIL/COMMERCIAL CORE

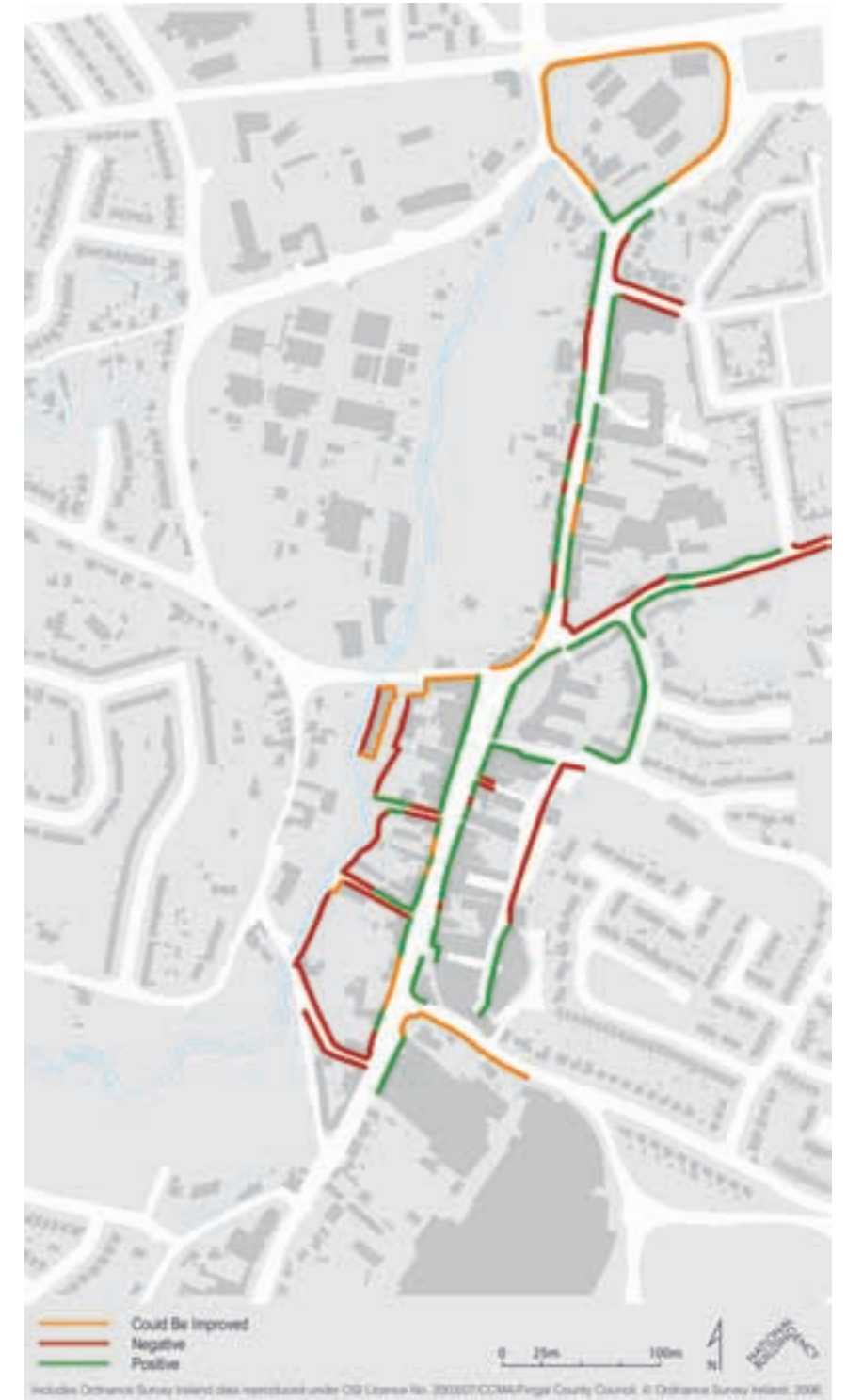
The quality of building frontages with a positive contribution to the public realm are mainly orientated towards the Main Street. There is a significant amount of negative frontage (frontages that display blank walls or setbacks to the public realm) particularly to the rear of the Main Street to the west overlooking the river.

CIVIC/COMMUNITY CORE

The quality of building frontages within this area are generally positive. While there is a series of vacant terraced buildings at the junction of North Street and the Main Street which present poor frontages to the public realm, Fingal County Council have the intention to improve the physical quality of the area through a public realm strategy.

COMMERCIAL/RESIDENTIAL

The quality of building frontages with a negative contribution to the public realm are limited within this character area. The majority of buildings to the west and east of North Street present a positive frontage. However at the northern end of North Street the quality of building frontages diminishes and could be improved upon greatly.



Building Orientation

THE RETAIL/COMMERCIAL CORE

The buildings within Main Street offer many active frontages to the public realm with most accommodating shops and commercial activity at ground floor level. Gaps in the building line offer access and views into laneways which are a distinctive local feature. Again these are frequently fronted by ground floor active uses. These include River Mall, Colmcille Court to the west of Main Street and Swords Town Centre Mall, Mainscourt and The Plaza to the east of Main Street.

To the backlands, west of the Main Street the rear gardens, yards surface car parks and high walls that front the river walk contributes to an inactive frontage with poor natural surveillance.



Active frontages to be promoted



Inactive frontages to be avoided

CIVIC/COMMUNITY CORE

Within this character area buildings are generally set in rows facing the street. Some properties have medium-large front gardens with high walls creating sections of blank frontages. In other areas open plan front gardens ensure active frontages.

COMMERCIAL/RESIDENTIAL

The formal street layout of this character area with buildings perpendicular to the street creates a strong frontage and building line to North Street. In some areas the use of high fences and walls contributes to an inactive street frontage. This is particularly evident on the side street immediately north of the Carnegie Court Hotel approaching Seatown Villas.



Seatown Road: Buildings front the street with open setbacks



North Street: The use of high fences and walls contributes to an inactive street frontage

Building Heights and Massing

THE RETAIL/COMMERCIAL CORE

The majority of the western edge of the Main Street has developed at a low-rise scale, predominantly featuring two-storey terraced buildings. The individual buildings contain a variation in their heights from ground to eaves. This is realised by changes in the angle of the roof pitch and the internal floor to ceiling heights. The roofscape and variation in the scale and massing of buildings provides a richness of character along the western edge of the Main street.

To the eastern edge of the Main Street there are numerous recent modern infill buildings of 3 to 5 storeys, which create a denser feel to this side of the town. The taller buildings here such as the County Hall relate well to the wider Main Street.



The western edge of the Main Street predominantly featuring two-storey terraced buildings



The eastern edge of the Main Street featuring buildings from three to five storeys

CIVIC/COMMUNITY

Buildings within this character area are predominantly two storeys with varying floor to ceiling heights. Most of the later infill in the character area respects this.

COMMERCIAL/RESIDENTIAL

Buildings within this area are characteristically two-storey and of a domestic scale. There are some taller buildings to the northern end of North Street (hotel and commercial block) which reflects more recent denser infill development.



Seatown Road predominantly featuring two-storey detached buildings



Seatown Road predominantly featuring two-storey detached buildings



North Street to the northern edge features recent development blocks of three to four storeys



North Street to the western and eastern edge features domestic buildings of two storeys



Appendix 2 Summary of Checklists

Safer Places: The Planning System and Crime, ODPM, April 2004

ACCESS AND MOVEMENT

- 1 Have the consequences of the number and nature of all connections been considered?
- 2 Do all routes lead to somewhere people want to go? Are all routes necessary?
- 3 Do routes provide potential offenders with ready and unnoticed access to potential targets?
- 4 Are routes for different users segregated when they could be integrated?
- 5 Will pedestrians, cyclists and drivers be able to understand which routes they should use?
- 6 Is it easy to understand how to travel through an area?

STRUCTURE

- 1 Have the types of building been selected and designed with security in mind?
- 2 Is the layout of the development appropriate for the identified crime risk, as well as to meet wider planning objectives?
- 3 Will all uses in an area be compatible and have potential conflicts been thoroughly thought through?
- 4 Does all public space serve a purpose and support an appropriate level of legitimate activity?
- 5 Has the remodelling, removal or re-use of buildings and spaces that are vulnerable to crime been considered?
- 6 Have the potential benefits for crime prevention of restoring historic environments been considered?

SURVEILLANCE

- 1 Are opportunities for surveillance from the subject and adjacent buildings maximised?
- 2 Will those most likely to observe any criminal or anti-social behaviour respond appropriately?
- 3 Are both of the above true at all times of the day, week and year?
- 4 Have efforts been made to eliminate 'inactive' frontages and corners?
- 5 Where appropriate, such as in public buildings, does the design allow for high visibility into the building or site?
- 6 Are parked cars highly visible but secure?
- 7 Has lighting been a primary consideration in planning out crime?
- 8 Is the standard of lighting and its maintenance regime adequate and is it resistant to vandalism and damage? Is it well-designed and well-sited?
- 9 Is CCTV the best way to solve the particular problem and is it the most effective use of resources?
- 10 Is the CCTV part of a wider package of crime prevention measures?
- 11 Will the resources be in place to maintain the CCTV system, including staff to monitor and respond to the pictures, in future years?

OWNERSHIP

- 1 Will it be clear to users – including potential offenders and capable guardians – which space is public, communal, semi-private and private?
- 2 Are the boundaries between public, communal and private space signified in the most appropriate manner, be it a physical barrier or a psychological barriers such as changes in paving, surface texture/colour, landscaping and signage?
- 3 Will the place have an identity of its own?
- 4 Are all those who should feel ownership involved in defining the place's identity?
- 5 Are barriers of a high quality of design in their detailing and appropriate to their local context?

PHYSICAL PROTECTION

- 1 Have the 'target hardening' principles of Secured by Design been addressed?
- 2 Has the potentially negative visual impact of crime prevention measures been addressed and, where these cannot be ameliorated by good design, have the advantages been weighed against their adverse impacts?

ACTIVITY

- 1 Will as many law abiding people as possible be attracted to use the public realm?
- 2 Is there a strategy for encouraging residential population in town centres?
- 3 Should the evening economy be nurtured, and, if so, is it diverse and inclusive?
- 4 Are mixed uses successfully integrated with one another?
- 5 Are all uses in an area compatible and have potential conflicts been thoroughly addressed?
- 6 Will what attracts people to the public realm uphold its attractiveness?

Appendix 3 Glossary

Active frontage

Provided by a building or other feature whose use is directly accessible from the street or space which it faces; the opposite effect to a blank wall.

Architectural features

Doors, windows, cornices and other features which contribute to the overall design and architecture of a building.

Backland Development

Development which takes place to the rear of existing structures fronting a street or roadway.

Building types

The building typologies in the area. For example terrace, detached, block etc.

Building heights and massing

The combined effect of the height and bulk of a building or group of buildings.

Context

The setting of a site or area, including factors such as traffic, activities and land uses as well as landscape and built form.

Elevation

The façade of a building.

Enclosure

The use of buildings to create a sense of defined space.

Landmark

A building or structure that stands out from its background by virtue of height, size or some other aspect of design.

Legibility

The degree to which a place can be easily understood and traversed.

Permeability

The degree to which an area has a variety of pleasant, convenient and safe routes through it.

Protected structures

A building, feature, site, or structure identified in the Development Plan as worthy of protection or preservation in accordance with the Planning and Development Act 2000.

Public open space

The areas created and operated for benefit of the public and which are open to public (i.e., park, square, playground, etc.).

Safety and security

The discouragement to wrong-doing by the presence of passers-by or the ability of people to be seen from surrounding windows.

Topography

A description or representation of artificial or natural features on or of the ground.

Threshold Zone

An area of land that separates private space and buildings from public space, such as a small front garden.

Typology

A study of building types. Examples include bungalows, terraced housing, semi-detached housing, detached housing etc.

Urban grain

The pattern of the arrangement and size of buildings and their plots in a settlement; and the degree to which an area's pattern of street-blocks and street junctions is respectively small and frequent, or large and infrequent.

Views

What is visible from a particular point.

Vista

An enclosed view, usually a long and narrow one.



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